

4.6 APPROACH TO HERITAGE ASSETS

- 4.6.1 In designing the proposals, careful consideration has been given to the existing heritage assets in close proximity to the site.
- 4.6.2 The proposed development layout will be sensitive to the settings of the local heritage assets of the Grade II Listed Home Farmhouse and the Grade II* Listed Church of St Laurence.
- 4.6.3 The provision of open space to the eastern and southern parts of the site will ensure that Home Farmhouse and the church are provided with an appropriate setting, set back from residential development and the main vehicular point of access to the site. This also provides an attractive outlook for residential properties.
- 4.6.4 The vista of the church has informed the disposition of the residential avenue within the site, with dwellings along the boulevard designed to frame this vista.

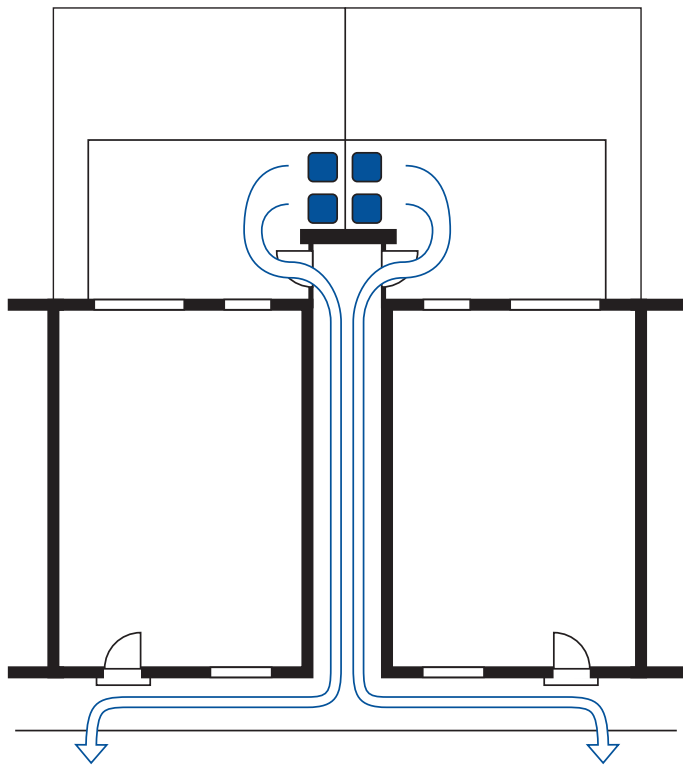
4.7 CRIME PREVENTION

- 4.7.1 The promotion of a well-designed development should in itself deter crime. The development will reflect best practice crime prevention principles. Streets and spaces would be designed to be overlooked by development to promote a sense of safety and reduce the fear of crime.
- 4.7.2 Vehicular access from Charlotte Avenue and the adjacent open space would be overlooked by residential properties resulting in a high level of natural surveillance. Residential streets and green spaces would also have a high level of natural surveillance from active residential frontages.
- 4.7.3 The effective long-term management and maintenance of any new development is key to deterring anti-social behaviour as identified in the principles above. Roads will be designed to be adopted and the management and maintenance of infrastructure, open spaces and sustainable drainage systems will allow for future adoption. Alternatively, the governance of open space may be transferred to a management trust(s) which will be responsible for owning and managing these spaces in perpetuity. These options will be discussed further with the relevant stakeholders as the Planning process advances.

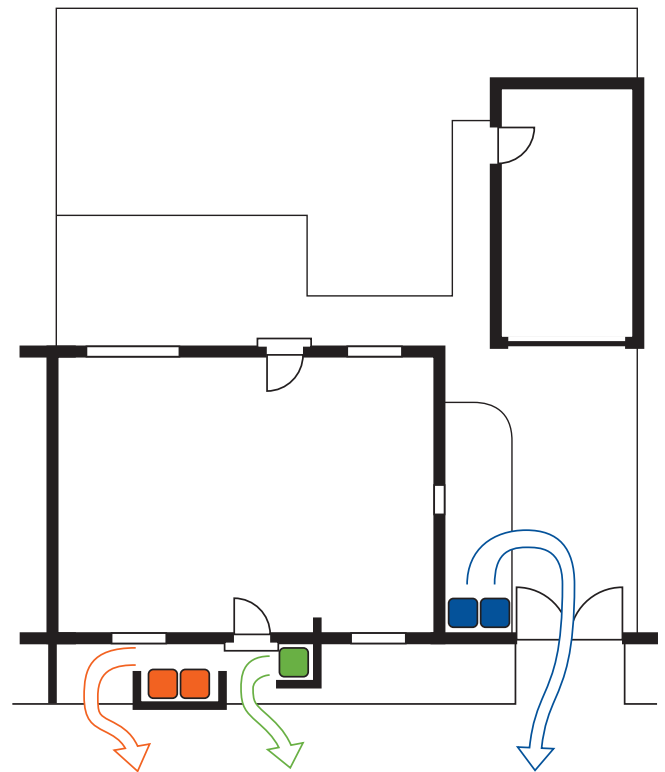


4.8 BIN STORAGE

4.8.1 Bin storage will be provided on-plot for dwellings in accordance with the requirements of Cherwell District Council. Detached and semi-detached homes will store bins in their rear gardens. Terraced homes within courtyards may have designated areas for small groups of homes to store bins; these areas will be screened within the courtyards to maintain an attractive and tidy environment. In all cases it will be possible to achieve direct access between the bin storage area and the kerbside without going through the house.



Rear outdoor storage with ginnel access to property front



Methods for shielding bin storage

5.0 ACCESS

5.1 ACCESS AND MOVEMENT PRINCIPLES

5.1.1 Policy Bicester 1 of the Cherwell Local Plan (Part 1) (2015; re-adopted in 2016) sets out the following development requirements for North West Bicester:

- Prioritisation of non-car modes to encourage a modal shift from car use to other forms of travel.
- Pedestrian and cycle routes that enable integration and connectivity between new and existing communities, including the town centre.

5.1.2 Development Principle 6 of the North West Bicester SPD (2016) requires development to have a network of well-designed, connected spaces and routes that prioritise the movement of pedestrians, cyclists and public transport. Development proposals must integrate fully with existing communities in Bicester by improving/removing barriers to movement both on and off-site.

5.1.3 Development Principle 9 requires proposals to create a network of interconnected green cycle and walking routes with direct links between green areas and key destinations.

5.1.4 The draft Cherwell Design Guide SPD (2017) requires new development to promote a movement network and street design which encourages walking and cycling over vehicle movements and connect into the existing network of streets and footpaths.

5.1.5 The Oxfordshire Residential Road Design Guide (2003; amended in 2015) priorities pedestrians over vehicular movement to encourage more sustainable travel.

5.1.6 The Design Guide promotes developments that are accessible, legible and convenient to all users, including the mobility impaired. Developments should be designed to emphasise a sense of place and community, with movement networks to enhance these qualities, with links with adjacent areas to ensure permeability.

5.1.7 Oxfordshire's Cycling Design Standards: A guide for developers, planners and engineers (2017) encourages new development to:

- Establish a well connected street network which provides cycle users with the opportunity to make direct journeys with distance minimised; and
- Create cycle links through open, overlooked green corridors where a longer form of traffic free path is appropriate.

5.1.8 Oxfordshire's Walking Design Standards: A guide for developers, planners and engineers (2017) encourages new development to design streets with the needs of pedestrians as a priority.



- 5.1.9 The proposals will provide for an integrated network of routes and spaces allowing for use by pedestrians, cyclists and vehicles, with that order of priority. This movement network will incorporate pedestrian and cycle routes which connect into the surrounding North West Bicester area.
- 5.1.10 The Transport Assessment submitted in support of this planning application sets out in further detail the transport issues and demonstrates how these are being addressed in respect of sustainable development and transport objectives.
- 5.1.11 The traffic assessment, through its extensive evidence and survey base data, demonstrates that the application proposals will not have a significant impact on the local road network.
- 5.1.12 All properties within the development are served by the internal access arrangements of the site. It is proposed to provide one principal vehicular access to the site from Charlotte Avenue. A secondary vehicular access will be provided from the existing Home Farm access off Banbury Road to access the allotments and informal parking area.
- 5.1.13 The road layout will facilitate access to all parts of the site and turning movements as required. The illustrative Master Plan demonstrates how this could be achieved.
- 5.1.14 New walking and cycling routes within the proposal provide greater accessibility to open spaces and connectivity with the surrounding North West Bicester site.
- 5.1.15 In addition to these improvements to the pedestrian/cycle network, the site is accessible to local facilities and well served by means of bus service.
- 5.1.16 The scheme will be designed to be inclusive of disabled users and consideration has been given to the access to the proposed properties and the ease of use taking account of advice set out in "Planning Practice Guidance", "The Principles of Inclusive Design (Cabe 2006), and "Inclusive Mobility: A Guide to Best Practice" (2002).
- 5.1.17 The highway elements of the development will mainly be adopted and maintained by the Local Highway Authority.





Fig 5.1 - Access and Movement Parameter Plan

5.2 CONNECTIONS WITH THE WIDER NORTH WEST BICESTER ECO-TOWN

- 5.2.1 The proposed development provides opportunities for this site and the wider North West Bicester area to be brought forward as a comprehensively planned sustainable eco-town.
- 5.2.2 In preparing this application careful consideration has been given to how the site facilitates physical and visual connections with the adjacent areas within the North West Bicester area (particularly the neighbouring exemplar site) for existing and future residents to access wider services and facilities.
- 5.2.3 The Access and Movement Parameter Plan set out within this DAS has been prepared within the context of enabling ease of access to the scheme and to the wider area. Connectivity and integration with the wider North West Bicester eco-town will be secured as follows:
- The proposed development will provide for access by all modes to be served to and from Charlotte Avenue.
 - New public transport provision along Charlotte Avenue will be within a walkable distance (400 metres) of the site and all residential dwellings.
 - The internal street network has been designed to enable pedestrian and cycle connections with the adjoining development.
 - A combined footway/cycleway will be provided within the area of open space on-site to provide a leisure route which will connect with the pedestrian and cycle network within the adjacent development parcels of the exemplar site, to the north-west and to the south-west of the site.

5.3 PARKING

Car parking

- 5.3.1 Oxfordshire's Residential Road Design Guide (2003; amended in 2015; Appendix 6) states that one allocated car parking space per dwelling will be acceptable at North West Bicester Ecotown. This may be on plot or off plot. Off plot provision may be grouped in a parking court provided the courts are small, close by, secure and conveniently accessed. Additional unallocated off plot car parking may also be provided up to a maximum of one space per dwelling.
- 5.3.2 Provision for car parking on site will be made in accordance with the standards and policy set out by Cherwell District Council and Oxfordshire County Council. Resident car parking will be provided on-plot in garages or on driveways. The following principles will govern parking provision:
- Parking to be provided as close to each property as possible, and is safe and easy to use;
 - Parking is generally expected to be provided in a combination of on plot, off plot and on street spaces;
 - Adopting a flexible approach to parking design and provision, focusing on optimum design and layout to meet the needs of residents, pedestrians and cyclists; and
 - Reducing the visibility of the car in the streetscene through careful design, robust boundary treatments, and unobtrusive garaging and use of car ports.
- 5.3.3 Informal parking will also be provided in walking distance of the Church, for use by parishioners and users of the allotments. This proposal has been discussed with local stakeholders and improves the existing parking arrangements for this important local community facility.

Cycle parking

5.3.4 The Cherwell Design Guide SPD (2017) sets out the following cycle parking standards:
Cycle parking requirement

	Cycle Parking Requirement
Resident	1 bed - 1 space 2+ beds - 2 spaces
Visitor	1 stand per 2 units, where more than 4 units



5.3.5 Provision for cycle parking as part of the development will also be made in accordance with Council policy to encourage use of sustainable modes of transport. This can be secured by planning condition.