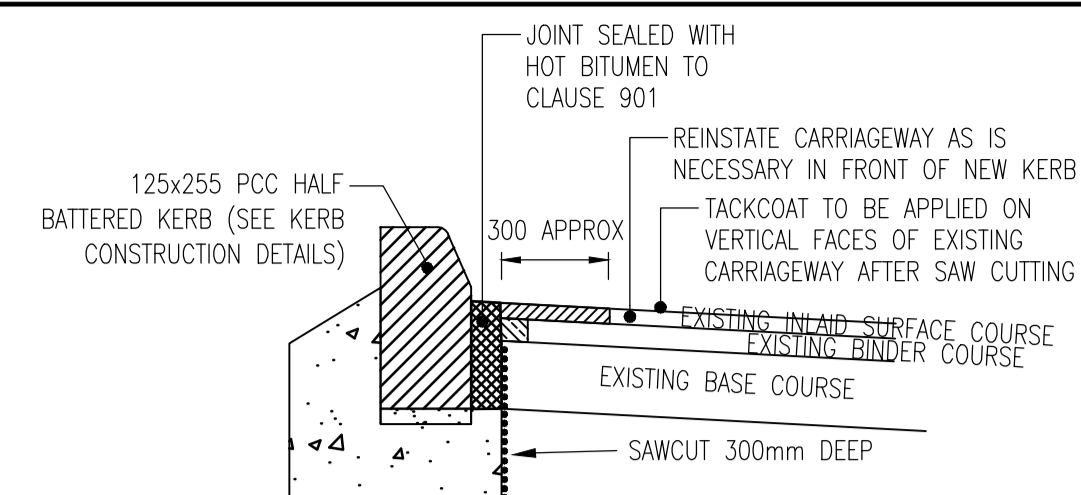
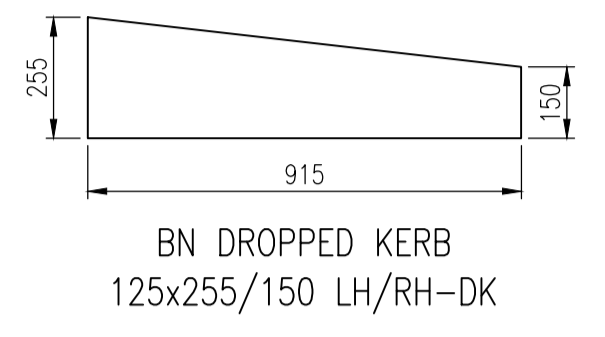


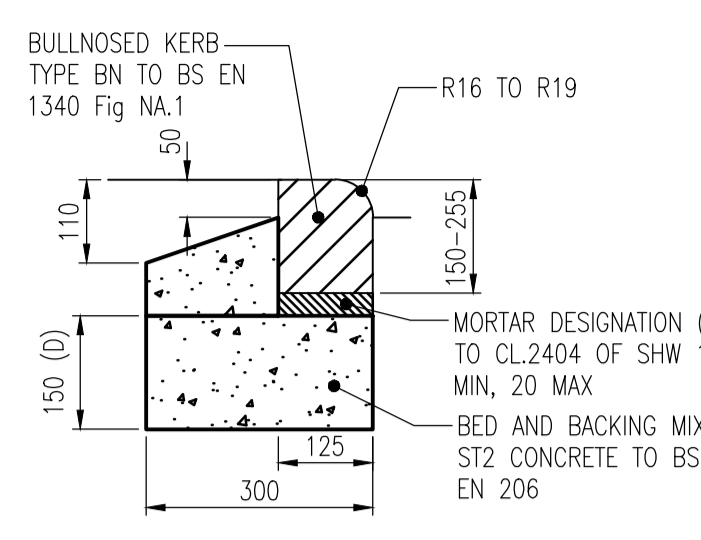
HALF BATTERED KERB TYPE HB2 (125mm UPSTAND)



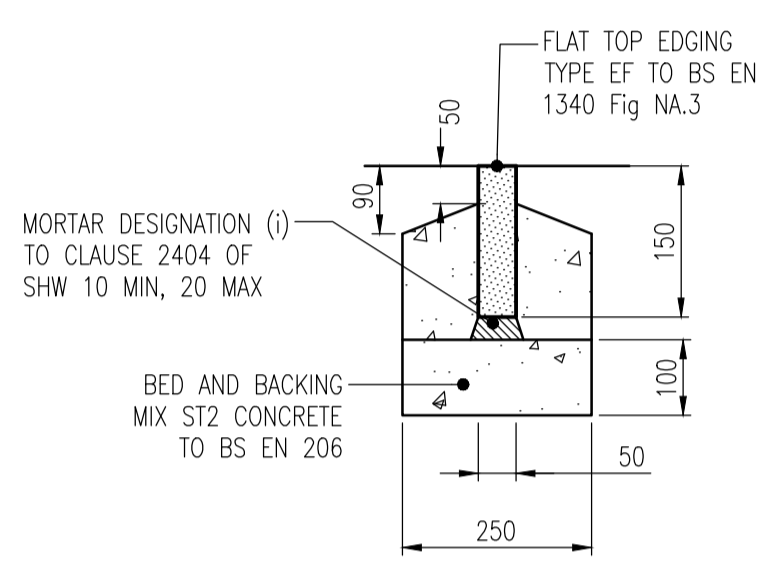
KERB IN CHASE DETAIL (125mm UPSTAND)



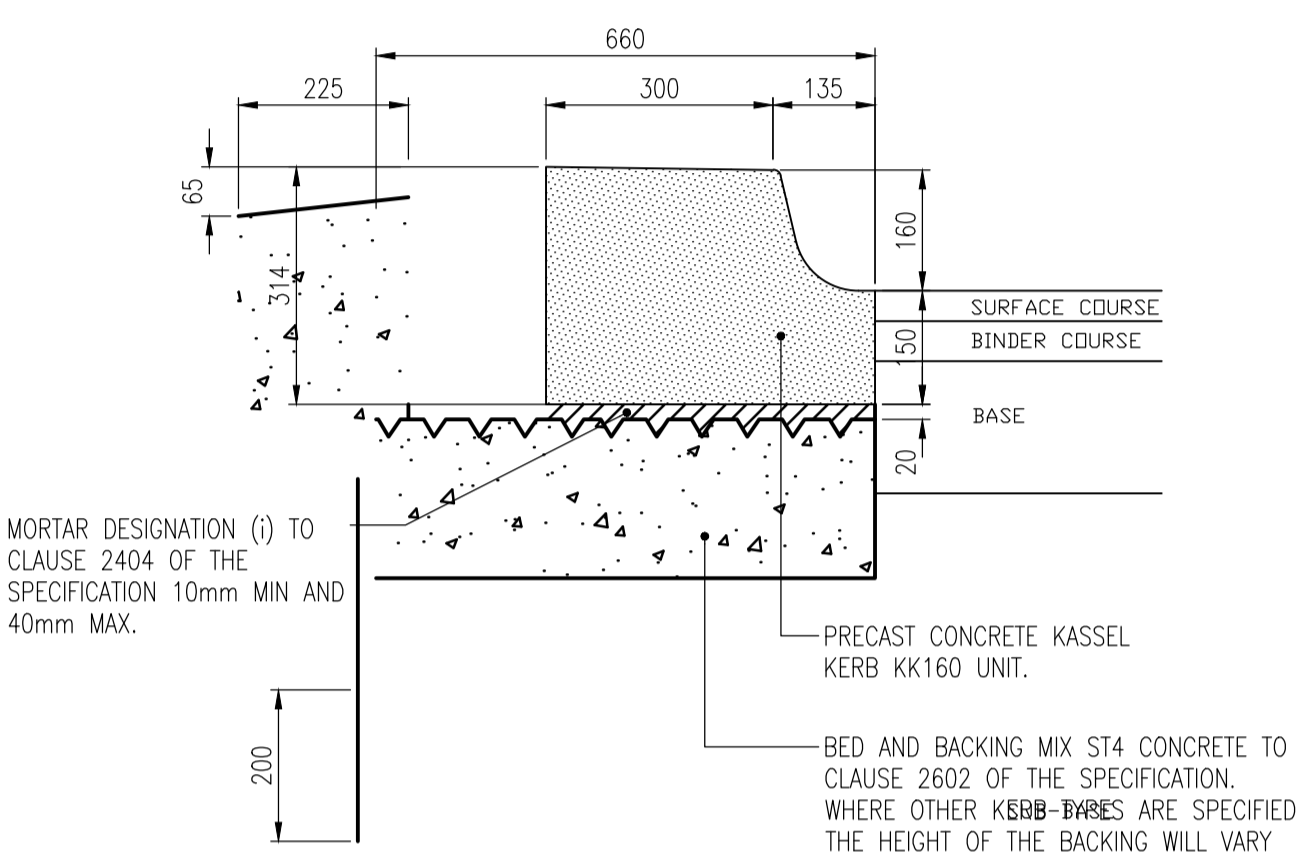
BN DROPPED KERB 125x255/150 LH/RH-DK



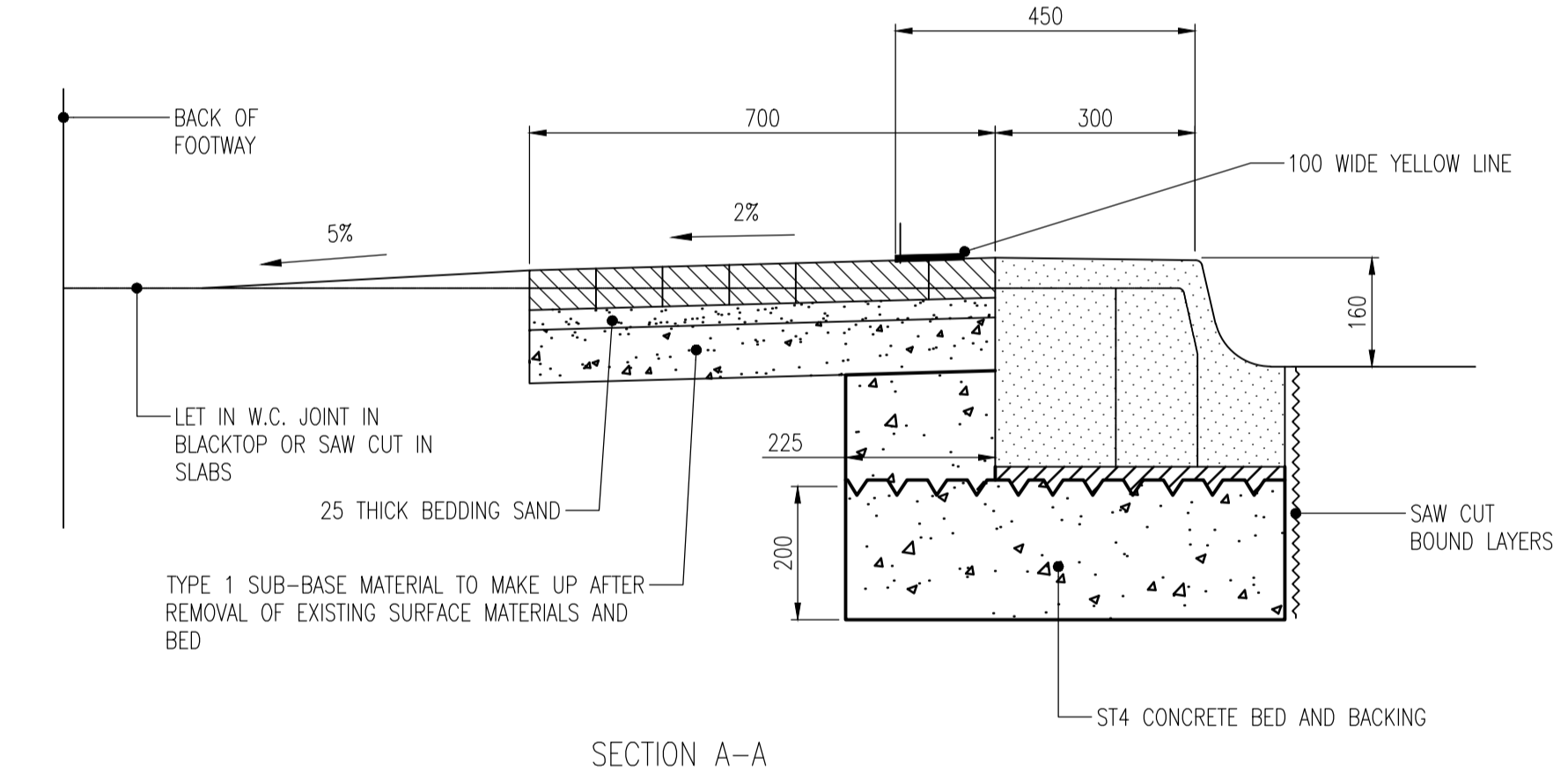
BULLNOSED KERB TYPE BN (PEDESTRIAN CROSSINGS UPSTAND 0-6mm UNIT MAY BE INVERTED TO PROVIDE SQUARE FLUSH EDGE AT PEDESTRIAN CROSSING) (VEHICLE CROSSOVERS UPSTAND 12-25mm)



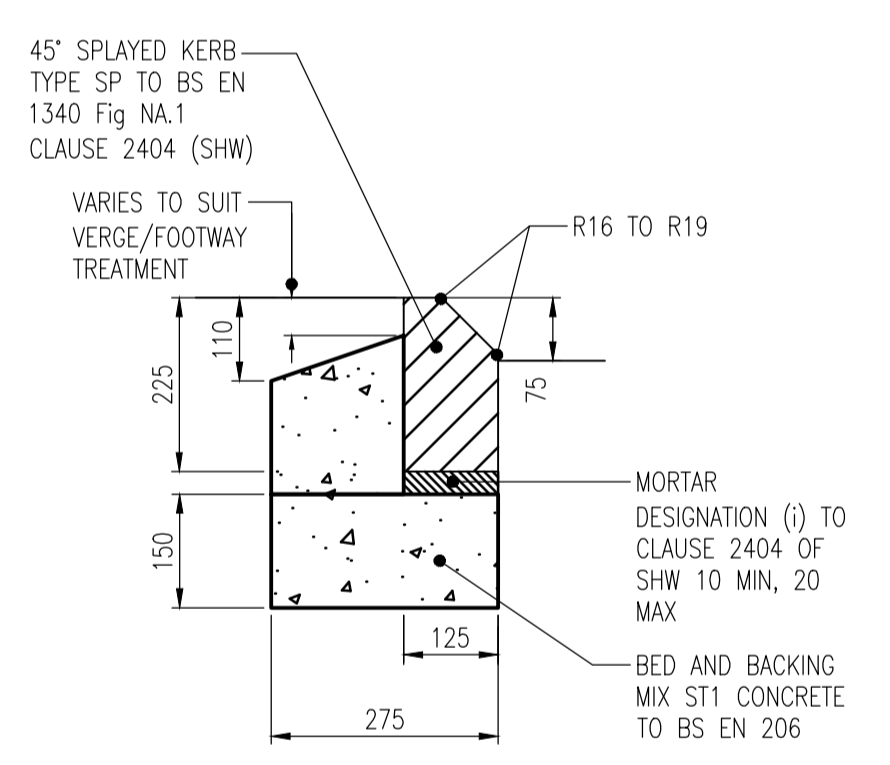
FLAT TOP EDGING TYPE EF (FLUSH)



KASSEL KERB DETAIL



SECTION A-A



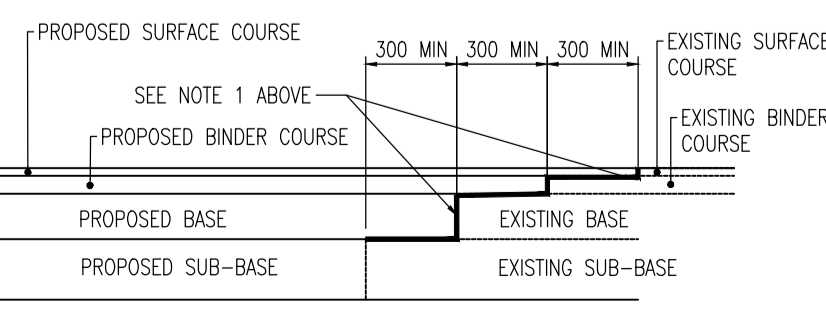
45° SPAYED KERB TYPE SP

RESTRICTED DESIGN OPTIONS - CLASS 2 SUBBASE ON CAPPING
REF: INTERIM ADVICE NOTE 73/06 (FIGURE 3.2)

CBR (%)	TYPE 1 SUBBASE THICKNESS (MM)	CAPPING THICKNESS (MM)
3*	275	240
4	240	225
5	220	210
6	205	200
7	195	195
8	180	185

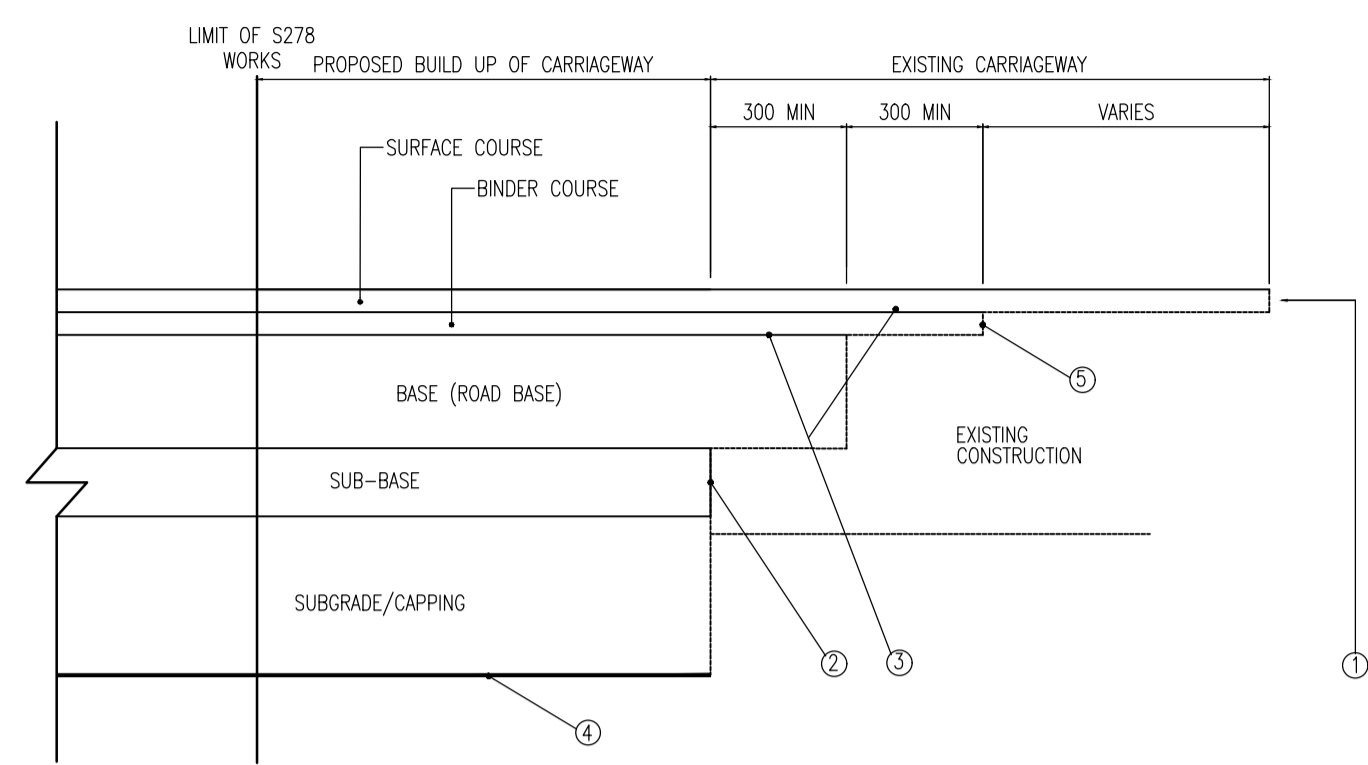
* WHERE THIS INFORMATION IS IN THE SPECIFICATION, CONTRACTORS SHOULD CHECK WITH THE LOCAL AUTHORITY TO DETERMINE THE CAPPING THICKNESS TO BE USED. WHERE THIS INFORMATION IS NOT IN THE SPECIFICATION, CONTRACTORS SHOULD CHECK WITH THE LOCAL AUTHORITY TO DETERMINE THE CAPPING THICKNESS TO BE USED.

- NOTES:
- CBR'S TO BE CONFIRMED BY INSITU TESTS CARRIED OUT IN ACCORDANCE WITH LOCAL AUTHORITY SPECIFICATION.
 - WORKS TO BE CARRIED OUT IN ACCORDANCE WITH THE SPECIFICATION FOR HIGHWAY WORKS (SHW), LATEST EDITION AND THE CURRENT BRITISH STANDARDS.
 - SHW CLAUSE NUMBERS SHALL APPLY IN THE EVENT OF CONFLICTING INFORMATION.
 - THESE DETAILS INCLUDE THE USE OF HEAVY KERB/PAVING PRODUCTS WHICH THE CONTRACTOR IS EXPECTED TO HANDLE USING APPROPRIATE MECHANICAL HANDLING AIDS IN ACCORDANCE WITH HSE GUIDANCE.

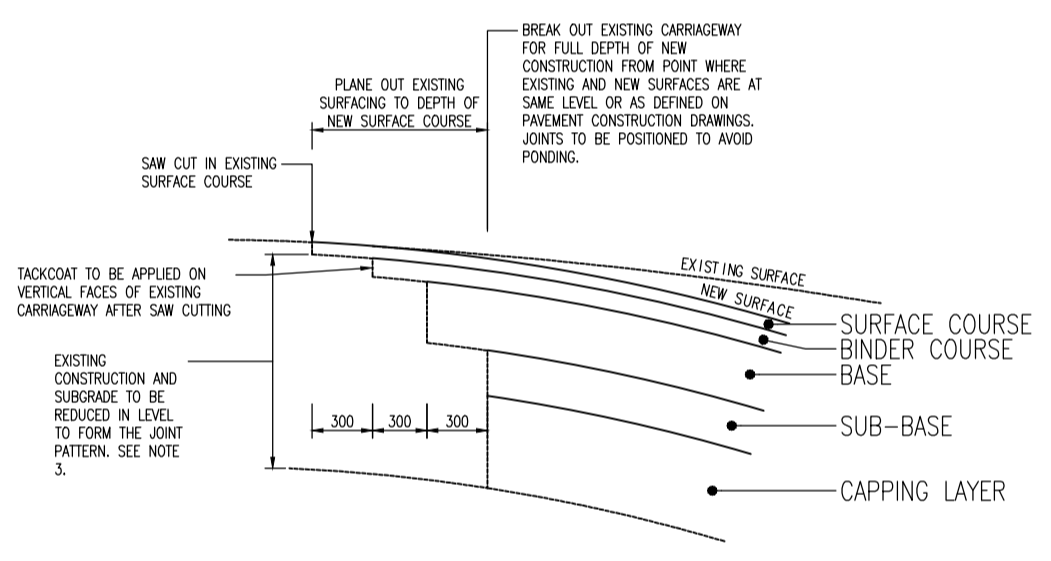


FOOTWAY/CYCLEWAY TIE-IN DETAIL

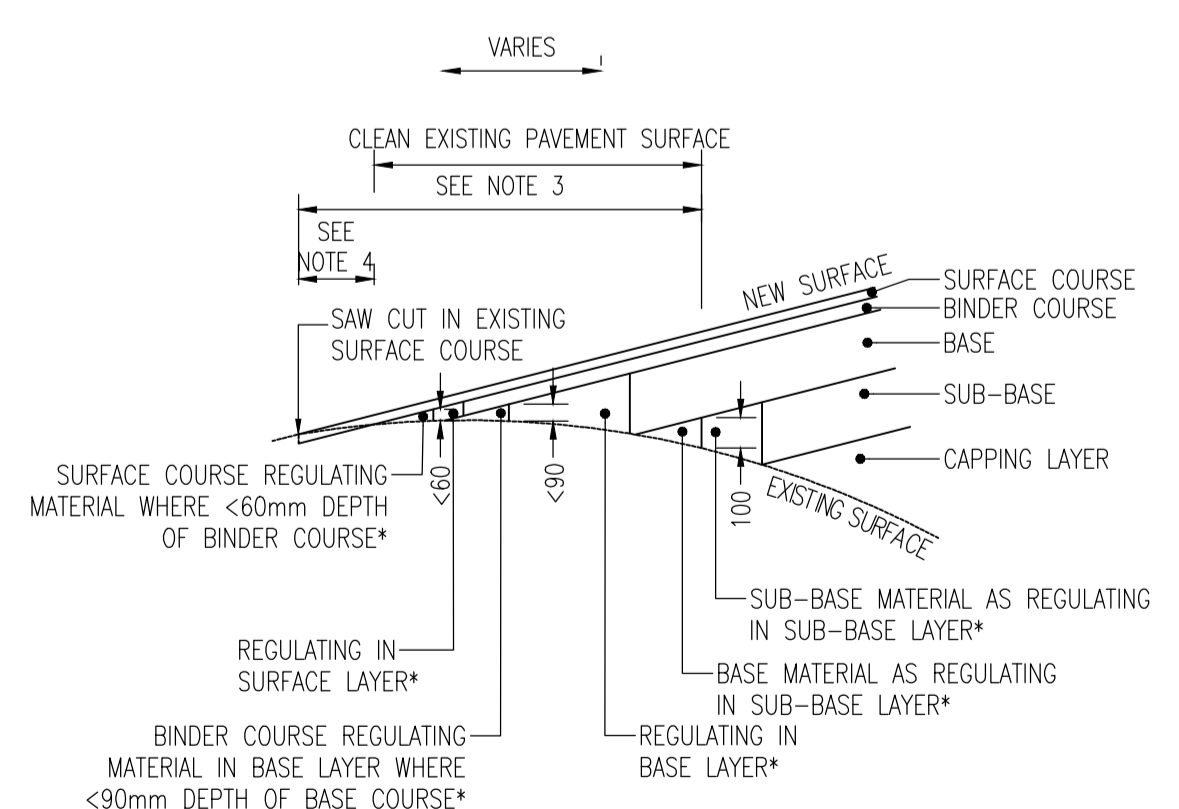
DIAGRAMATIC SECTION OF LONGITUDINAL PAVEMENT JOINT



- A STRAIGHT SAW CUT JOINT SHALL BE CUT PRIOR TO PLANING OFF EXISTING FLEXIBLE PAVEMENT IN ACCORDANCE WITH CL.903.23 AND BS 5949:87. DEPTH OF SAW CUT TO SUIT PROPOSED SURFACE COURSE THICKNESS. VERTICAL FACE OF THE EXISTING SURFACE COURSE IS TO BE PAINTED WITH ASPHALTIC CEMENT IN ACCORDANCE WITH CL.901.22 (iii).
- THE EXISTING FACE SHALL BE BROKEN BACK TO SOUND MATERIAL, CUT CLEAN AND VERTICAL.
- EXISTING SURFACING TO BE PLANED OFF TO PRODUCE SOUND LEVEL STEPS TO SUIT PROPOSED SURFACING LAYERS.
- TERRAM GEOTEXTILE SEPARATOR LAYER.
- TACKCOAT TO BE APPLIED ON VERTICAL FACES OF EXISTING CARRIAGEWAY AFTER SAW CUTTING IN ACCORDANCE WITH CL.920.3



INTERFACE A
NEW SURFACE AT SAME LEVEL AS OR BELOW EXISTING SURFACE



INTERFACE B
NEW SURFACE ABOVE EXISTING SURFACE (Refer to Drg. No. 700-3)

PAVEMENT TIE-IN DETAILS

- NOTES:
- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
 - DETAILS OF INTERFACE A & B APPLY TO BOTH LONGITUDINAL & TRANSVERSE SECTIONS.
 - JOINTS TO BE FORMED IN ACCORDANCE WITH CL. 901.22 (iii) VERTICAL FACES OF EXISTING BITUMINOUS MATERIALS TO BE PAINTED WITH BITUMEN. ALL HORIZONTAL OR INCLINED EXISTING SURFACES TO ACCEPT BITUMINOUS MATERIALS TO BE SPRAYED WITH TACK COAT IN ACCORDANCE WITH CL.920.3.
 - FOR TRANSVERSE DETAILS ONLY, EDGE OF EXISTING CARRIAGEWAY TO BE CUT BACK TO SOUND CONSTRUCTION (MIN 300mm).
 - FOR MEASUREMENT PURPOSES ONLY THIS VOLUME IS CONSIDERED WHOLLY AS REGULATING MATERIAL. IN PRACTICE IT SHALL BE A COMBINATION OF MATERIALS TO SUIT THE PERMISSIBLE MAX & MIN THICKNESSES FOR EACH MATERIAL & THE CONTRACTORS METHOD OF WORKING.
 - FOR DETAILS OF PAVEMENT MATERIALS REFER TO WSP DWG 1549-700-1.
 - ALL CLAUSE NUMBERS REFER TO THE SPECIFICATION FOR HIGHWAY WORKS.

DO NOT SCALE

- NOTES
- ALL DIMENSIONS IN MILLIMETRES UNLESS STATED OTHERWISE.
 - THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT SCHEME DRAWINGS.
 - ALL CONCRETE AND CONCRETE PRODUCTS BELOW GROUND LEVEL ARE TO BE CLASS 3 SULPHATE RESISTING TO BRE SPECIAL DIGEST 1:2001.
 - PRECAST UNITS TO BE HYDRAULICALLY PRESSED AND TO BS EN 1340:2003. WEATHERING RESISTANCE CLASS 3, BENDING STRENGTH CLASS 2, ABRASION RESISTANCE CLASS 3, SLIP/SKID RESISTANCE MIN P.P.T.V. 45 TO BS 7932.
 - BED AND BACKING TO BE MIX CONCRETE CLASS ST1 UNLESS OTHERWISE STATED TO CLAUSE 2602SR OF THE SPECIFICATION FOR HIGHWAY WORKS AND SHALL BE A MINIMUM THICKNESS OF 150mm.
 - WHERE NECESSARY A CHASE SHALL BE FORMED IN THE SUB-BASE TO ALLOW 150 MIN KERB BED.
 - ALL MATERIALS USED IN PAVEMENT CONSTRUCTION WITHIN 450 OF PAVEMENT FINISHED LEVEL SHALL BE NON FROST SUSCEPTIBLE.
 - ALL PRECAST CONCRETE UNITS ARE TO BE LAID IN ACCORDANCE WITH CL.1101 OF THE SPECIFICATION FOR HIGHWAY WORKS NOV 2005.
 - CONCRETE FOR KERB BED AND INFILL SHALL COMPLY WITH BS EN 206-1
 - WHERE A SPECIFIC PRODUCT OR SUPPLIER IS SPECIFIED, AN APPROVED ALTERNATIVE CAN BE OFFERED.
 - THE WORKS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SPECIFICATION AND THE DEPARTMENT FOR TRANSPORT'S SPECIFICATION FOR HIGHWAY WORKS NOV 2005.
 - WHERE EXCAVATION INTO EXISTING CARRIAGEWAY IS REQUIRED A TRENCH SHALL BE EXCAVATED OF SUFFICIENT DEPTH AND WIDTH TO ACCOMMODATE THE COMPLETE BED AND BACKING OF THE KERB TYPE SCHEDULED.
 - ALL MATERIALS USED IN PAVEMENT CONSTRUCTION WITHIN 450 OF PAVEMENT FINISHED LEVEL SHALL BE NON FROST SUSCEPTIBLE.
 - ALL SETTING OUT IS TO BE AGREED WITH THE ENGINEER PRIOR TO THE COMMENCEMENT OF WORKS ON SITE.

KEY TO HEALTH AND SAFETY SYMBOLS

	INDICATES A RESIDUAL RISK REQUIRING A COMPULSORY ACTION.
	INDICATES A RESIDUAL RISK FOR INFORMATION.
	INDICATES A RESIDUAL RISK REQUIRING A PROHIBITIVE ACTION.
	INDICATES A RESIDUAL RISK AS A WARNING.

CONTRACTOR SHOULD PROVIDE MECHANICAL LIFTING AIDS FOR ITEMS HEAVIER THAN 20 KGS. IF MECHANICAL LIFTING AIDS ARE IMPRACTICAL A RISK ASSESSMENT SHOULD BE UNDERTAKEN WHICH IDENTIFIES THE APPROPRIATE CONTROL MEASURES REQUIRED TO AVOID RISK OF INJURY FROM MANUAL HANDLING.

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REV	DATE	BY	DESCRIPTION		CHK	APP

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Unit 9 The Chase, John Tate Road, Foxholes Business Park, Hertford, SG13 7NN, UK
T+ 44 (0) 1992 526 000, F+ 44 (0) 1992 526 001
wsp.com

CLIENT: **ATLAS HOTELS GROUP**

ARCHITECT: **NORR**

SITE/PROJECT: **HOLIDAY INN EXPRESS BICESTER GATEWAY**

TITLE: **HIGHWAY CONSTRUCTION DETAILS SHEET 1 OF 2**

SCALE @ A1:	NTS	CHECKED:	MPB	APPROVED:	MPB
PROJECT NO:	70033775	DESIGNED:	MM	DRAWN:	MM
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