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## **HOLIDAY INN EXPRESS. BICESTER**

DESIGN STATEMENT

DECEMBER 2017

**LR** **NORR**

# CONTENTS

## 1.0 TERMS OF REFERENCE

## 2.0 INTRODUCTION

2.1 DESIGN STATEMENT OVERVIEW

2.1 PROPOSED DEVELOPMENT

## 3.0 DESIGN STRATEGY

3.1 SUMMARY OF DESIGN DEVELOPMENT

3.2 DESIGN LAYOUT

3.3 DESIGN QUALITY & MATERIALITY

3.4 SCALE: HEIGHT & MASS

3.5 ACCESS

3.6 LANDSCAPING

## 4.0 CONCLUSION

# 1.0 | TERMS OF REFERENCE

## 1.0 TERMS OF REFERENCE

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- 1.1 NORR Consultants have been instructed by London and Regional Properties (L+R) to prepare developed Architectural designs for the Reserved Matters application site at the Bicester Gateway development, Bicester, Oxfordshire.
- 1.2 The client's brief is to prepare designs for a high quality contemporary hotel.
- 1.3 The brief is to deliver a suitable approach to the existing site that will provide a high quality, flexible hotel response that will meet the requirements of the expanding local market and the aspirations of key stakeholders.
- 1.4 Following on initial analysis of the site and its context together with an assessment of other statutory and environmental requirements, full consultation with the Cherwell District Council through the outline planning process and pre-application discussions, the initial design proposals have been developed forward into a Reserved Matters application.
- 1.5 This Design Statement describes the rationale behind the design, its detailed development, the internal and external access arrangements and why the proposals are considered appropriate for this site and location.
- 1.6 To assist the proposals in achieving a positive physical integration, the area surrounding the building has also been fully considered. This analysis has focused on the following components: the physical context, the landscaping of the site and planning policy context.
- 1.7 The building is also informed by Planning Policy Context – an assessment of policies relevant to the development of the site, including an evaluation of the local plan policies and their impact on the proposals. Policy context will form part of a Reserved Matters report prepared by Quod.
- 1.9 Section 3.0, the Design section, outlines the application proposals including an explanation of the evolution of the design, layout, scale, appearance, access & circulation and other considerations.
- 1.10 An experienced team of professional consultants has been engaged by the client to assist in the preparation of all supporting documentation, including but not limited to:-
  - Architectural drawings and supporting information prepared by NORR;
  - Planning covering letter prepared by QUOD;
  - Landscaping proposals prepared by Turkington Martin
  - Energy statement prepared by RED Engineering Design
  - BREEAM pre-assessment prepared by RED Engineering Design
  - Structural & Civil drawings prepared by WSP
  - Ecological Implications Appraisal & Lighting report prepared by WYG
- 1.11 This Design Statement should be considered in conjunction with all other material comprising the application pack, with particular regard to the submission drawings, associated reports and illustrative material.

### 2.0 DESIGN STATEMENT OVERVIEW

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This document has been prepared as part of the supporting documentation for the Reserved Matters application which is to be submitted to Cherwell District Council for the development of vacant land east of A41 / Oxford Road and west of Wenderbury Road, Bicester.

This Design Statement has been prepared by NORR Consultants Ltd on behalf of London and Regional Properties (L+R), in support of the submission for;

Bicester Gateway **PHASE 1A** - the 149 hotel element of the previously consented Bicester Gateway development.

This statement will consider the detailed design of the scheme and demonstrate the scheme is of quality design, has had due regard to the prevailing character of the site and its surrounds, and accords with the principles set out in the DAS which accompanied the outline application. The statement highlights the schemes design development and identifies the proposed design responses in respect of access, appearance, landscaping, layout and scale.

### 2.1 THE PROPOSED DEVELOPMENT

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The development proposals will involve the erection of a 149 bedroom hotel with ancillary co-working space to the ground floor, to be operated under the Holiday Inn Express brand (part of the Intercontinental Hotel Group ("IHG")).

The hotel will offer a range of services and facilities which are complementary to the main office use at Bicester Gateway and the aspiration to bring forward a high quality business environment, which will attract high quality (knowledge based) jobs to the area, including:

- A restaurant and bar area, incorporating a "co-working" space;
- A range of meeting spaces (a total of 4 meeting rooms, one of which will be a large meeting room);
- 2 no. flexible conference rooms.

Our design proposals for the new hotel development promise to provide a high design quality which will perfectly compliment the Holiday Inn Express brand - aiming to produce a "smart" business & leisure hotel that does not compromise on quality, efficiency and style.



## 3.0 | DESIGN STRATEGY



FIG.01 The Consented Scheme



FIG.02 The developed Scheme

### 3.0 DESIGN STRATEGY

The outline planning application was accompanied by a suite of indicative plans and a DAS. Together, these indicated that Phase 1A of the development would the hotel would comprise a 5 storey L-shaped building adjacent to the north-western boundary of the site, providing 149 guest bedrooms, with car parking to the north-east. Cherwell District Council Development Control Committee had regard to and accepted the design principles suggested in the DAS submitted as part of the outline planning application.

Having gained ownership of the site L+R, have worked closely alongside the original material submitted whilst developing the detailed proposals for the Site. In recognition of the “gateway” location of the Site, L+R have sought to develop a high quality scheme which will provide a strong sense of arrival to the Site and Bicester itself. This is wholly consistent with the principles of the DAS and the Development Plan.

### 3.1 SUMMARY OF DESIGN DEVELOPMENT

Though the outline consent drawings have been used as the base for the schemes design development upon review with L+R, the building area and footprint has been rationalised to better suit the building’s proposed function & operation and furthermore, appear less bulky on the site and more linear in form.

Rather than L-shaped in form as per the consented scheme, the developed linear proposal is to be arranged over 5 stories and appear more slimline on the site. The building will be located within the original consented schemes ‘building line’ along the north-western boundary of the Site, with car parking to the north-east and the additional creation of additional parking & a ‘drop off’ area to the main entrance of the building as per the developed plans.

The rationalised ground floor layout still promises to provide high quality business and co-working facilities within the designated ‘Great Room’ and suite of business rooms. To further emphasise the hotels business and co-working ethos, we have removed all hotel accommodation previously located on the ground floor and have relocated it neatly over the four storeys of hotel accommodation.

Throughout this rationalisation and development, there has been a focus on developing a modern but sensitive building envelope that works with the natural surroundings. We have proposed a clean but soft palette of materials which will encourage the scheme to grow into the site and context.

Overall, we feel like we have developed a more efficient and in tune scheme which promises to be both a first class asset to both client and the growing bustling hub, which is Bicester.





### 3.2 DESIGN LAYOUT

Our developed proposal directly follows on from the key principles established by the outline planning application. The proposed site layout maintains the tree lined entrance avenue along Vendee Drive with direct vehicular access to the hotel site located off the existing Wendlebury Rd.

The principles of the building from outline planning are also largely maintained and the general principles of an open plan business & co-working facilities located at ground floor with hotel accommodation stacked from 1st-4th floors. However, upon review we have rationalised and improved the general principles of the initial building footprint by working closely alongside the future hotel operator & refocusing on market demands as well as improving the viability of the scheme. The have removed the excessive extruded wing at ground floor to create a more lightweight & linear footprint. The new efficient arrangement at ground floor is arranged around the central 'Great Room' which will be designed to hold both 149 covers for hotel guests and transform into a fully serviced co-working space for potential visitors. Off this area will be a bank of flexible private meeting rooms, WC's, serviced kitchen & back of house Staff areas. At upper levels, the hotel accommodation has been efficiently arranged over 4 floors to meet the proposed 149 bedroom total. The following key areas with regard to the re-configured floorplate have been fully considered:-

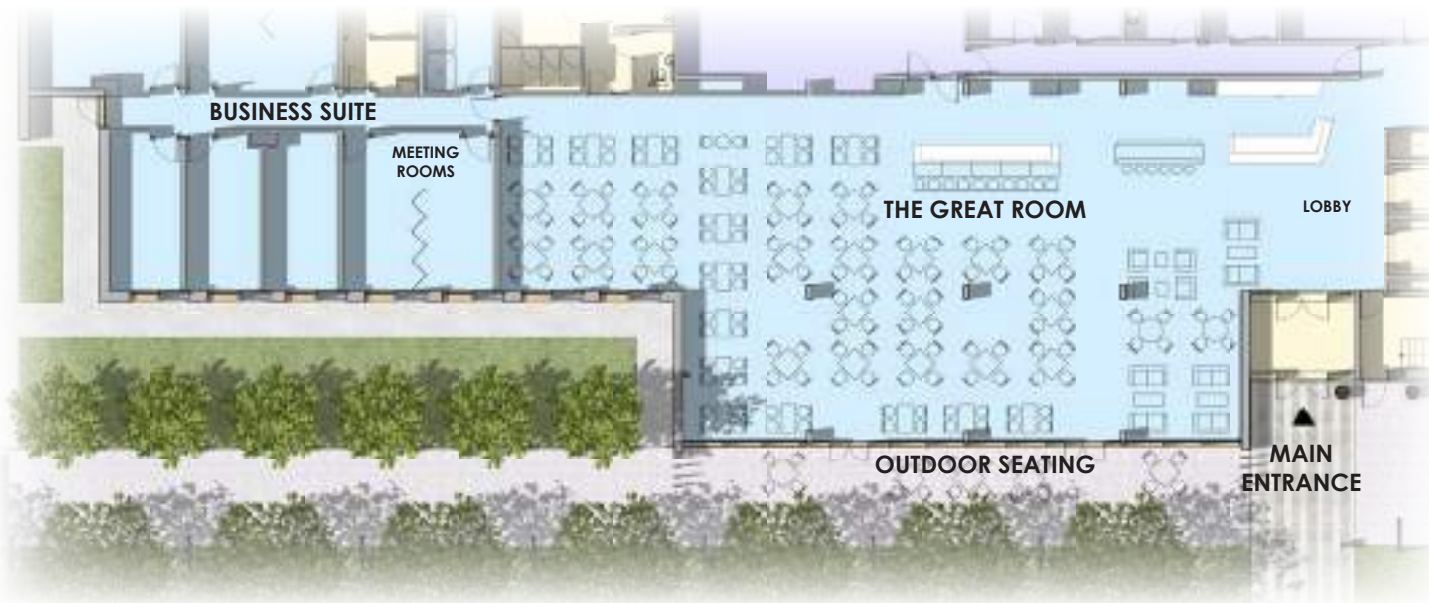
- New entrance and central reception / circulation;
  - State of the art 'Great Room';
  - Well organised floorplates;
  - New bank of central lifts; circulation, and WCs;
  - Flexible divisibility;
  - Clean cut & integrated Escape stairs within floorplates;
  - Rationalisation of building grid;
  - Proposed Floor-to-Floor heights of 4m at Ground and 3m 1st-4th.
- Primary circulation for the building is via the main core/entrance lobby . The core contains all the primary lifts.
- We are proposing a well designed external entrance increase to greatly improve the drop off and arrival area and create a true sense of arrival.
- Secondary staircases are provided to the perimeter of the building which provide for emergency escape and help to articulate and break up the side elevations of the building.
- The building design aims to provide a clear sense of arrival, the opportunity for good natural daylight to organised floor plates whilst being fully compliant with Approved Document M and the Equality Act 2010.

#### BUILDING AREAS - GEA

Ground	- 1223 m2
1st	- 1097 m2
Typical Floor [2nd-4th]	- 1097 m2
Roof	- 30 m2 (Stair 01 + Lobby)

**TOTAL - 5641 m2**





## 3.2 DESIGN LAYOUT

### THE GREAT ROOM

The Great Room is to be the a welcoming central hub of the hotel and will be designed to maximise flexibility of use. The Great Room is located within a single space which enables the hotel functions to expand or contract depending on demand at different times during the day. In this instance the The Great Room is to neighbour a full business suite of meeting rooms and by also to having a frontage to the main entrance area of the building to further help draw users in throughout the working day.

This designed space will have the potential to house guests for breakfast, hold business events and additionally provide a relaxing bar/lounge during the evening. The interior design will provide definition to the primary function of the Great Room and by the use of design elements break the volume into smaller, more intimate and informal spaces.

PLAN. THE GREAT ROOM



CGI OF THE PROPOSED GREAT ROOM

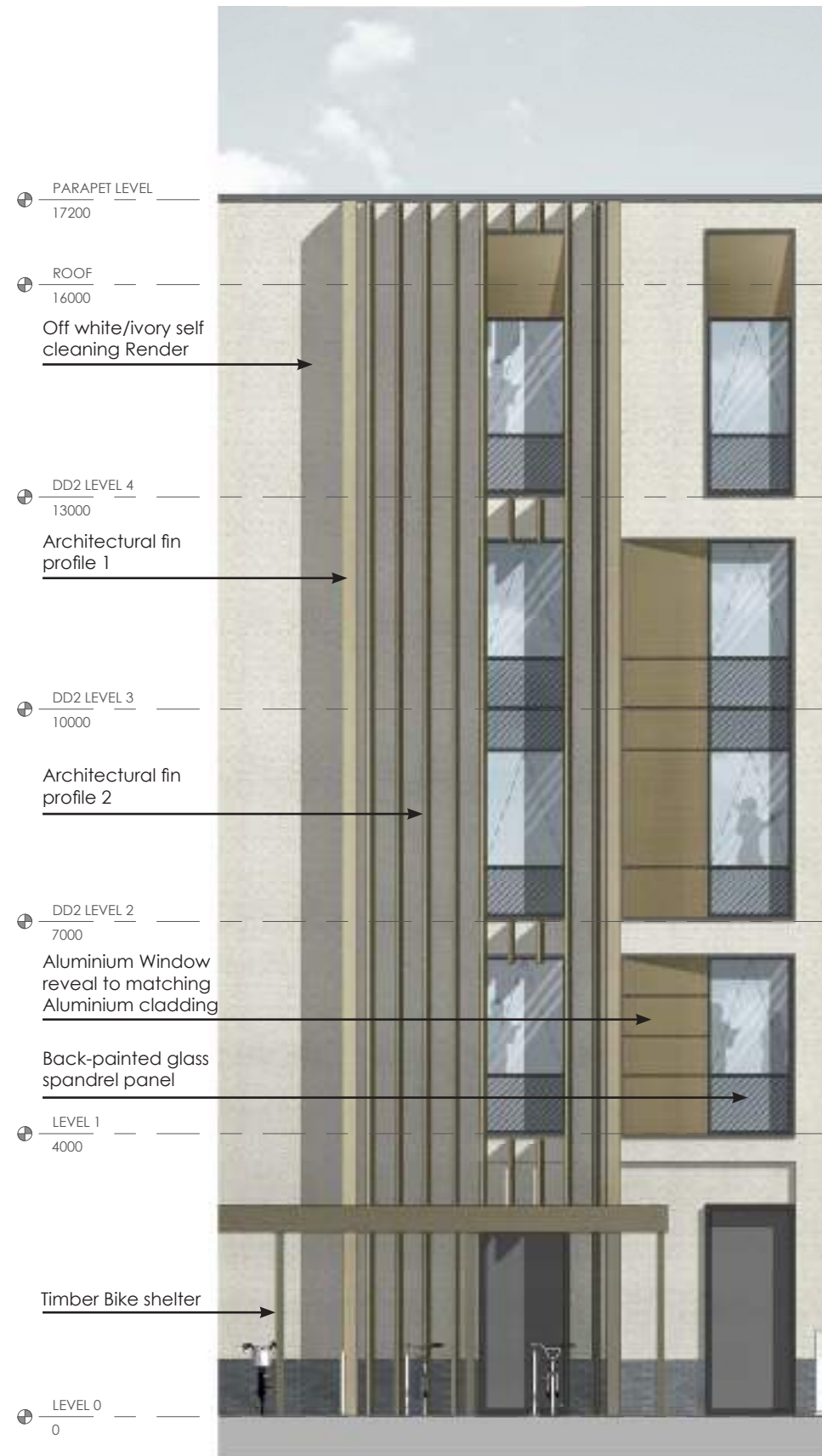
**HOLIDAY INN EXPRESS - BICESTER GATEWAY**  
BICESTER

DESIGN STATEMENT. RESERVED MATTERS SUBMISSION  
DECEMBER 2017

LR

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TYPICAL BAY ELEVATION A



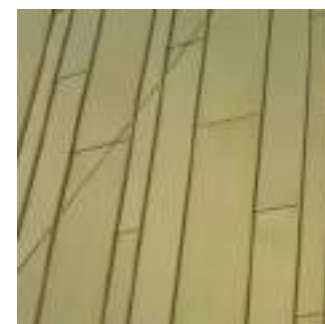
IVORY RENDER



BRICK PLINTH



ARCHITECTURAL FINNS



ALUMINIUM CLADDING

### 3.3 DESIGN QUALITY + APPEARANCE

The approach to the design and the selection of materials for both the overall and individual elements of the building has been taken in order to deliver a visually clean, high quality contemporary scheme with a strong architectural character and identity that reflects the building's use and function and prime setting within the new Bicester Gateway site.

Using a palette of glass, metal, render and brick, the spread of materials is carefully configured to emphasise the distinct masses. The refined colours and textures have been selected to provide a contemporary aesthetic that in parallel complements its surrounding environment to deliver elevations that gives the proposal a memorable identity, drawing visitors into the site.

#### ALUMINIUM CLADDING

The choice of aluminium cladding provides depth and interest to the proposal due to its ever changing appearance depending on the perspective and time of day, providing renewed experiences for visitors.

Whilst a refined and controlled palette of materials have been selected, various façade treatments have been developed through alternative formations that come in the form of angled window reveals, architectural fins and feature book ends, all finished in an aluminium bronze finish.

Each guest room benefits from the deep window reveals finished in aluminium cladding to match the elevational treatments, allowing light to permeate the rooms but affording each room a degree of privacy naturally formed by the deep inset.

#### CURTAIN WALL

The verticality and linearity of the windows and glazing provide varying degrees of light into the proposal. The large amount of glazing at ground floor level provides a smooth transition from the outside-in, offering visitor's views into activities held in the 'Great Room'.

Glazing is also provided to both the stair cores, providing sufficient natural light into the shaft as well as offering optimal views across the site as visitors make their way through the building.

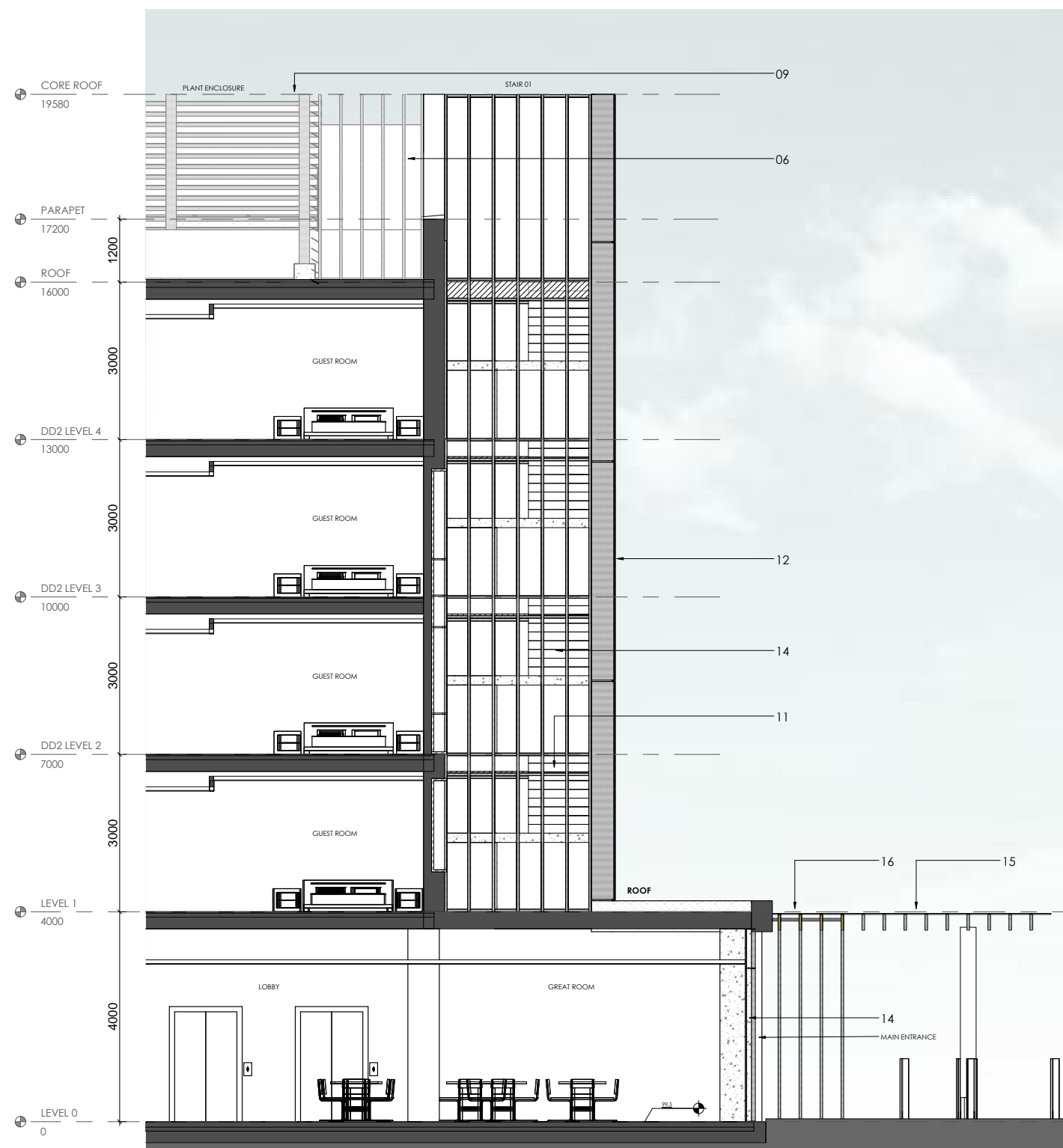
#### RENDER + BRICK

The choice of cladding contrasts against the subdued colour of the base render, evoking a welcoming and warm environment necessary for a hospitality ambience. The provision of a defined brick plinth alongside architectural fins that extend to the ground level, helps to scale and ground the building to the pedestrian environment.

#### ENTRANCE CANOPY

The Canopy plays an important role in welcoming visitors to the hotel. The prominent projection of this lightweight and contemporary canopy provides visitors with a definite sense of arrival and destination to reflect the Gateway nature of this site.



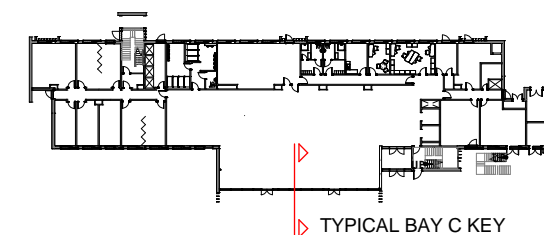


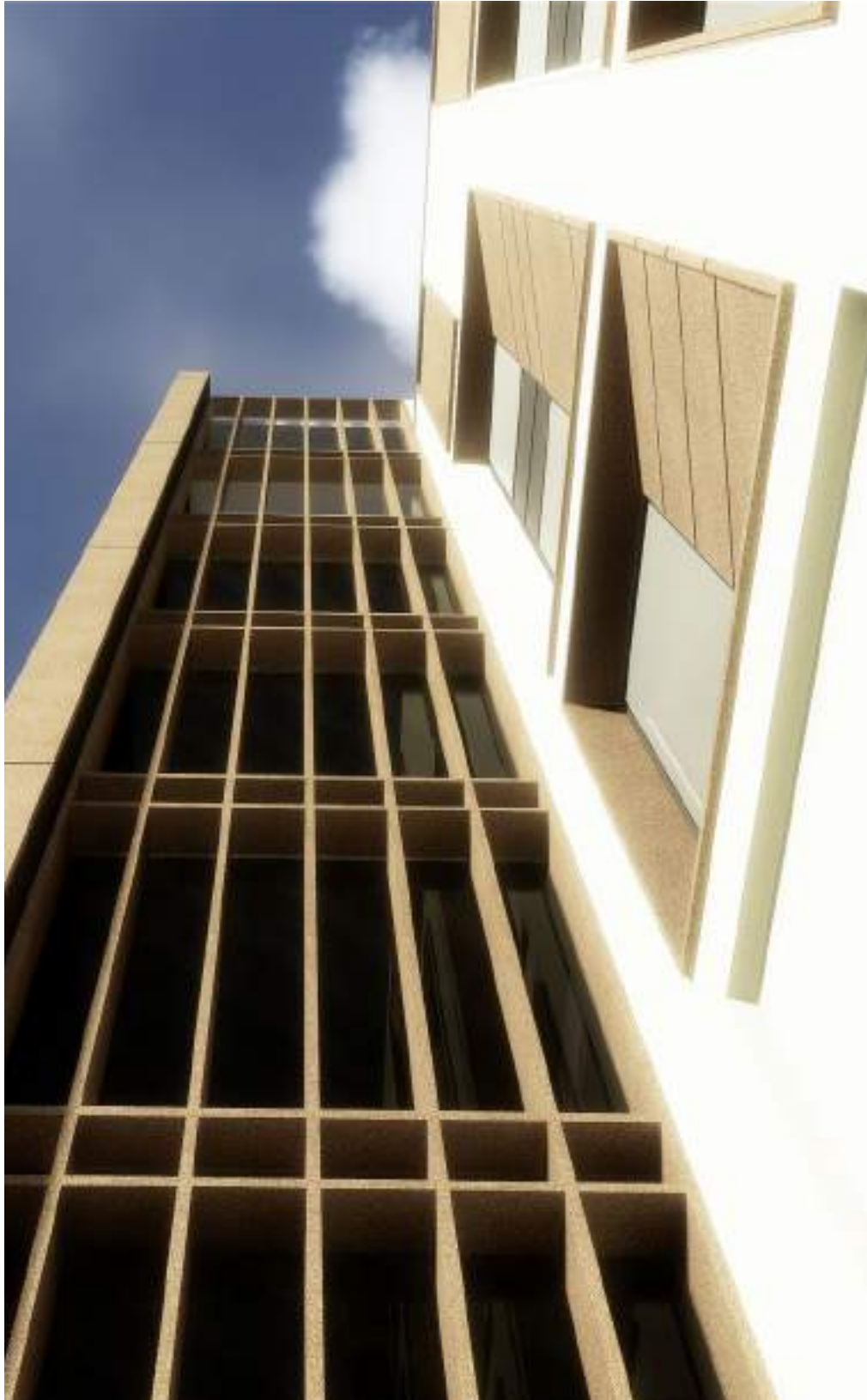
**01** TYPICAL BAY C - SECTION  
1:50 @ A1



**02** TYPICAL BAY C - ELEVATION  
1:50 @ A1

- LEGEND
- 01 EXTERNAL BRICK PLINTH WITH DARK MORTAR
  - 02 ALUMINIUM FRAME DOUBLE GLAZED WINDOW
  - 03 SELF CLEANING THROUGH COLOUR RENDER IN IVORY/OFF WHITE
  - 04 ALUMINIUM ANGLED HALF DEPTH WINDOW TOP REVEAL TO MATCH CLADDING
  - 05 ALUMINIUM ANGLED HALF DEPTH WINDOW SIDE REVEAL TO MATCH CLADDING
  - 06 EXTRUDED ALUMINIUM FIN TO MATCH WINDOW REVEALS AND CLADDING PROFILE TYPE 1
  - 07 EXTRUDED ALUMINIUM FIN TO MATCH WINDOW REVEALS AND CLADDING PROFILE TYPE 2
  - 08 SQUARE GALVANISED STEEL BOLLARD
  - 09 LIGHT WEIGHT ALUMINIUM LOUVRE SCREENING SYSTEM
  - 10 RECESSED SELF CLEANING RENDER PANEL IN OFF WHITE/IVORY
  - 11 BACK PAINTED GLASS PANEL
  - 12 ALUMINIUM RAINSCREEN CLADDING TO MATCH WINDOW REVEALS AND ARCHITECTURAL FINIS
  - 13 PROPRIETARY ALUMINIUM PARAPET CAP TO MATCH WINDOW FRAMES
  - 14 ALUMINIUM FRAME CURTAIN WALL SYSTEM
  - 15 ENTRANCE CANOPY WITH GLAZED LID. CONSTRUCTED FROM HORIZONTAL FINIS AND STRUCTURAL COLUMNS TO MATCH CLADDING AND WINDOW REVEALS
  - 16 GOAL POST CANOPY FINIS TO FRONT OF THE GREAT ROOM TO MATCH CLADDING AND WINDOW REVEALS
  - 17 ALUMINIUM LOUVRES TO M&E SPECIFICATION





**VERTICAL FINS / CURTAIN WALLING**



**DEEP & ANGLED WINDOW REVEALS**



**RECESSED RENDER PANELS**



**STAGGERED ALUMINIUM CLADDING  
+ SLOT FENESTRATION**



**FEATURE ENTRANCE CANOPY**

### 3.3 DESIGN QUALITY + APPEARANCE

#### DESIGN FEATURES

The brief is to deliver a suitable approach to the existing site that will provide a high quality, contemporary and flexible hotel that not only will meet the clients expectations, but also a landmark building which is more in line with the expanding commercial hub that is Bicester.

Upon review of the local hotels within the surrounding area, it was decided to aim for a 'best in class' hotel - a contemporary design that is sensitive, considered, inclusive and is a worthy addition to Bicester's growing landscape. Furthermore, discussions throughout the Pre-application process further emphasised the need for designing a building that both acknowledges the importance of this Gateway site (Vendee Drive/A41 Junction) but one that also lessens the impact it has on the environment. Creating a building that will become a prestigious facility that will attract staff, guests and business users alike.

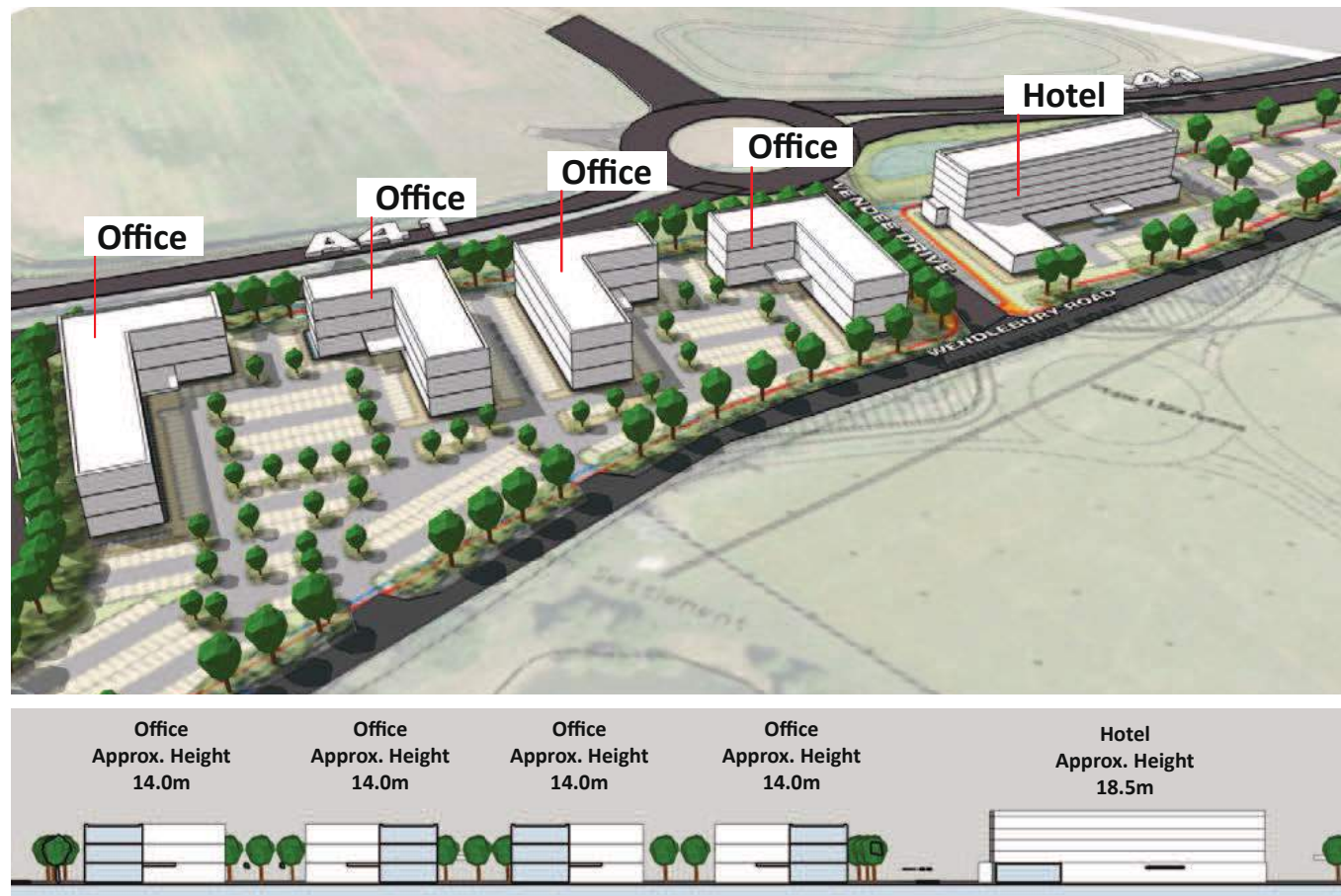
In order to guarantee design quality, steps have been taken to focus design details and materials from the outset.

#### Key design moves :

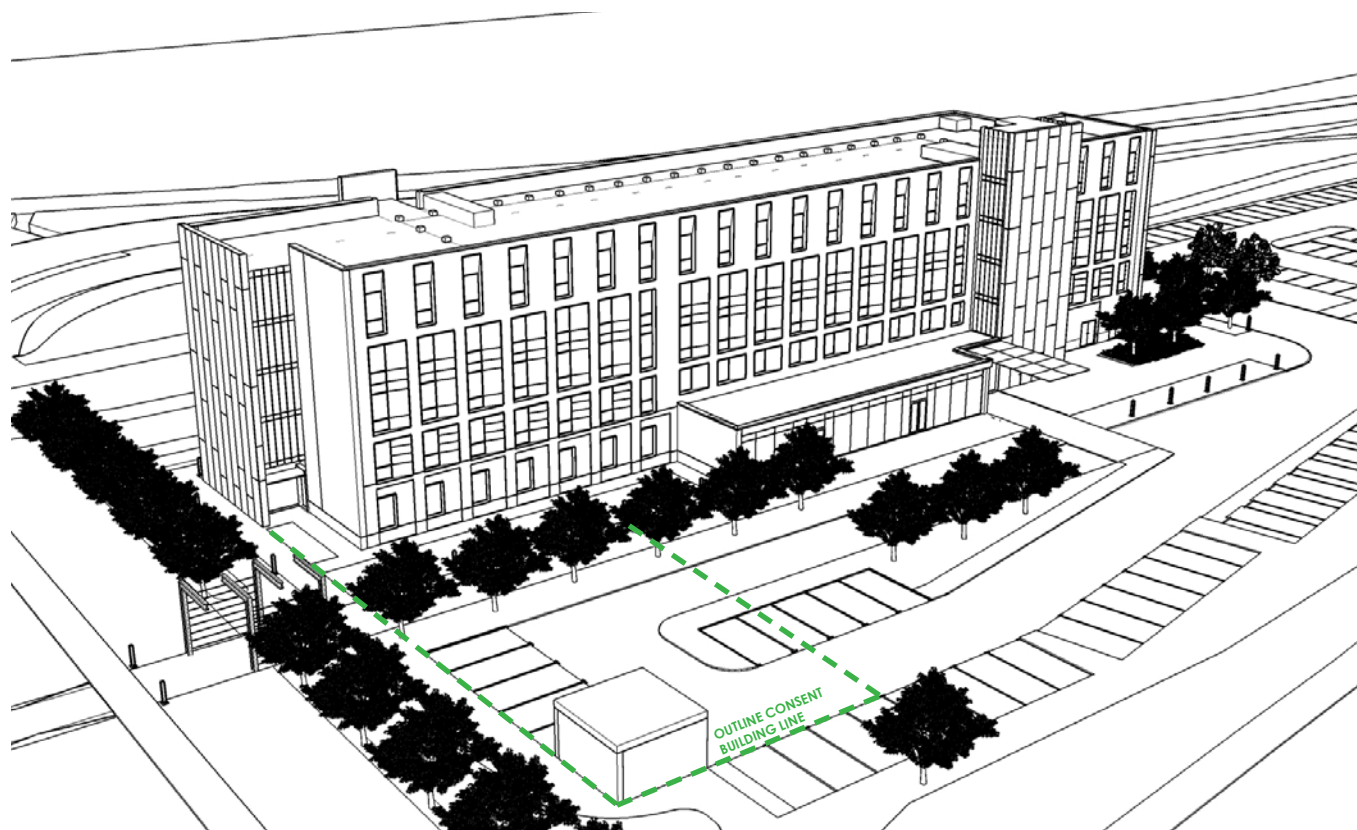
- Vertical fins + Curtain walling
- Fenestration to Feature Bookend - Vendee Drive
- Deep & Angled window reveals
- Staggered Aluminium Cladding
- Recessed Render Panels
- Feature entrance canopy

All the listed design features provide the clean-cut scheme with a suitable but unique aesthetic and architectural rhythm, that combined with the warm material palette & bronze tones is aimed to integrate the building with the sites rich natural landscape.





INITIAL SITE MASSING



PROPOSED SITE MASSING

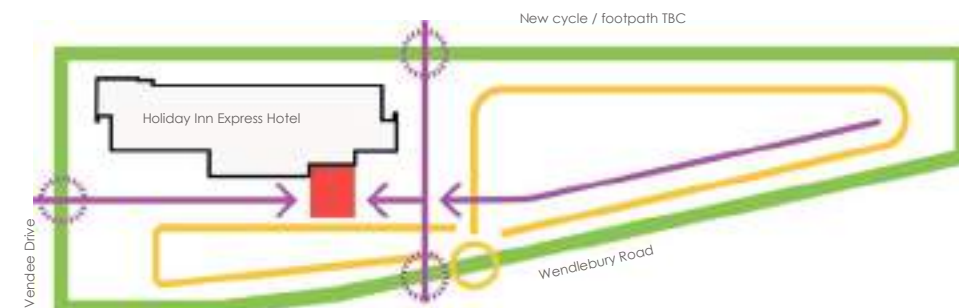
### 3.4 SCALE: HEIGHT + MASS

The height and mass of the proposal responds to the surrounding building types in keeping with the local vernacular. The height of the proposal reflects the 'Gateway' nature of this site to create prominence to the A41 frontage without dominating existing landscape.

Pedestrian visitors will pass through several structures before arriving at the main entrance not only to enrich the arrival experience but to also create a coherent and robust edge to the site with clearly defined entrance routes to gently guide visitors in/through the Gateway site.

#### SITE ENTRANCE

Upon arrival, the routes to the main entrance are emphasized by feature entrance structures on the South and West sides of the proposal. These have been developed to ensure legible and linkable routes from the public footways and future developments from neighbouring sites.



#### ENTRANCE CANOPY

The 3m high entrance canopy aids in scaling the building to the pedestrian level to ensure a warm and personable welcome to the hotel. The scale of the canopy also has a practical purpose in alerting pedestrians to the imminent opening of the great room, heightening the drama and functionality of the great room.

#### BUILDING MASS

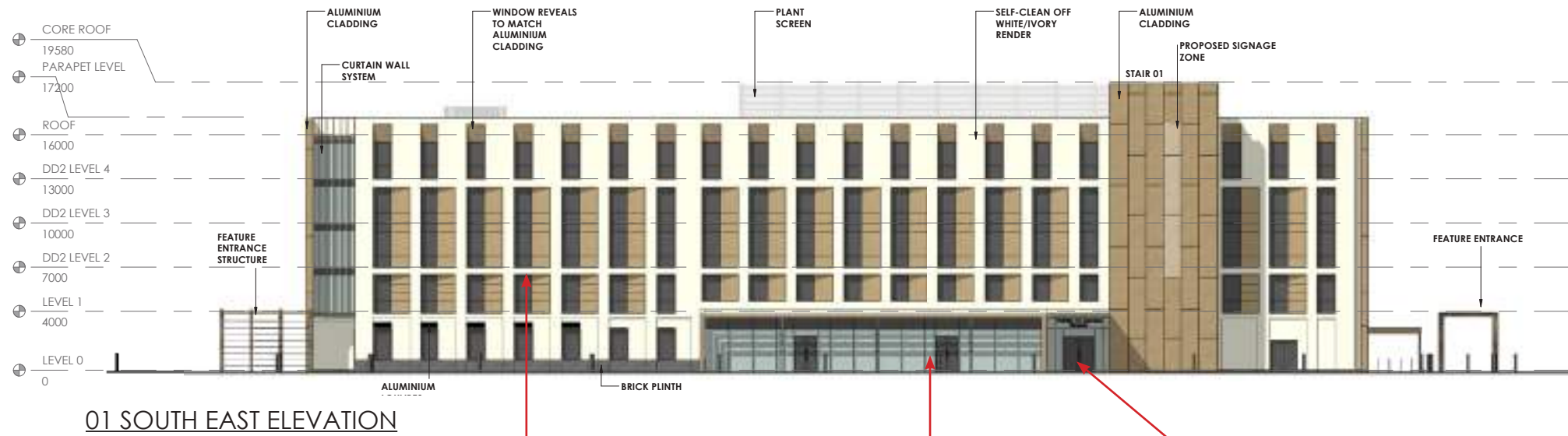
The building mass is composed of a core block consisting of the great room, conference rooms and 149 guest rooms. The density and robustness of this block is punctured with consistent rows of linear windows of varying heights to deliver a careful balance between heavy and fragile components.

Consideration has been given to break up the massing by the introduction of projecting stair cores in which the verticality of these elements with their fenestration further express a dynamic, vertical rhythm that contrasts against the horizontal nature of the core rectangular block.

The differing scales, densities and vertical architectural details expressed in the elevations aim to work alongside the A41 border so to be noticed, yet sensitively respond to the neighbouring leafy tree line and long grasses.



### 3.4 SCALE: HEIGHT + MASS

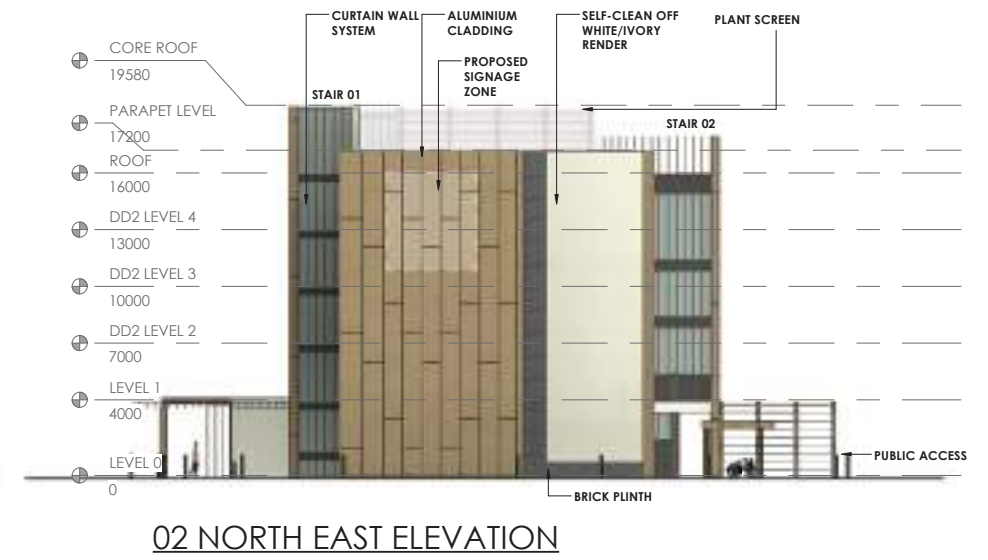


Repetition of fenestration patterns to give the elevation a sense of order and rhythm.

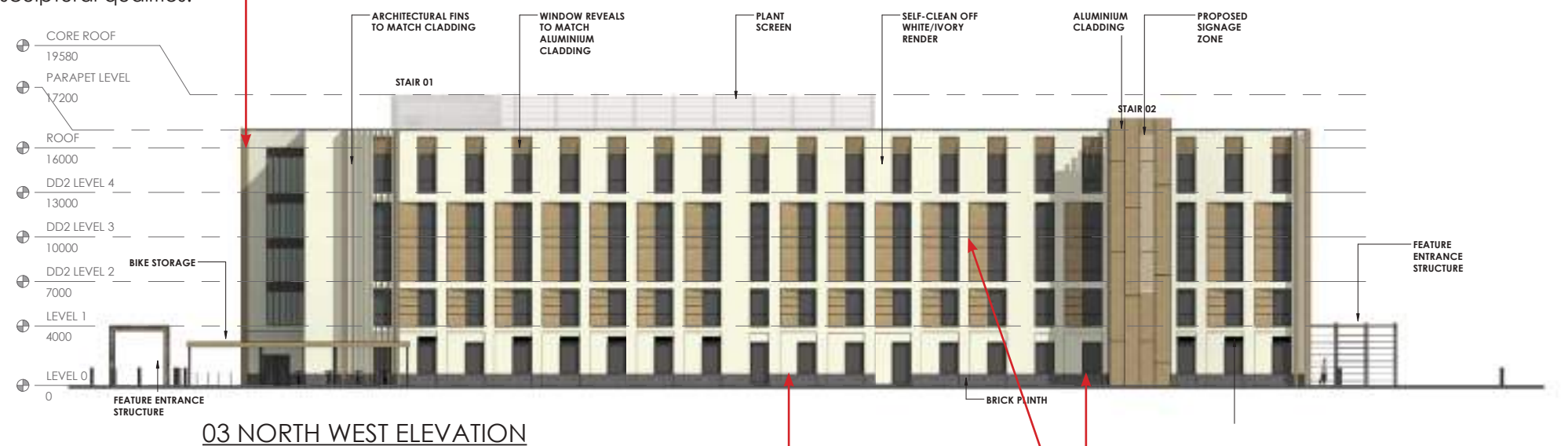
Openings at ground level reflect comfortable pedestrian scale.

Visual interest and vitality is expressed in use of low level main entrance canopy. The canopy projects and recesses at pedestrian level to welcome and 'sweep' visitors into the great room

Deep recesses either end lightens and relieves the density of the horizontal mass and to help strengthen the sculptural qualities.



Vertical element either side of the core mass consist of large expanses of curtain walling which are sub divided horizontally to balance the verticality of these structures.



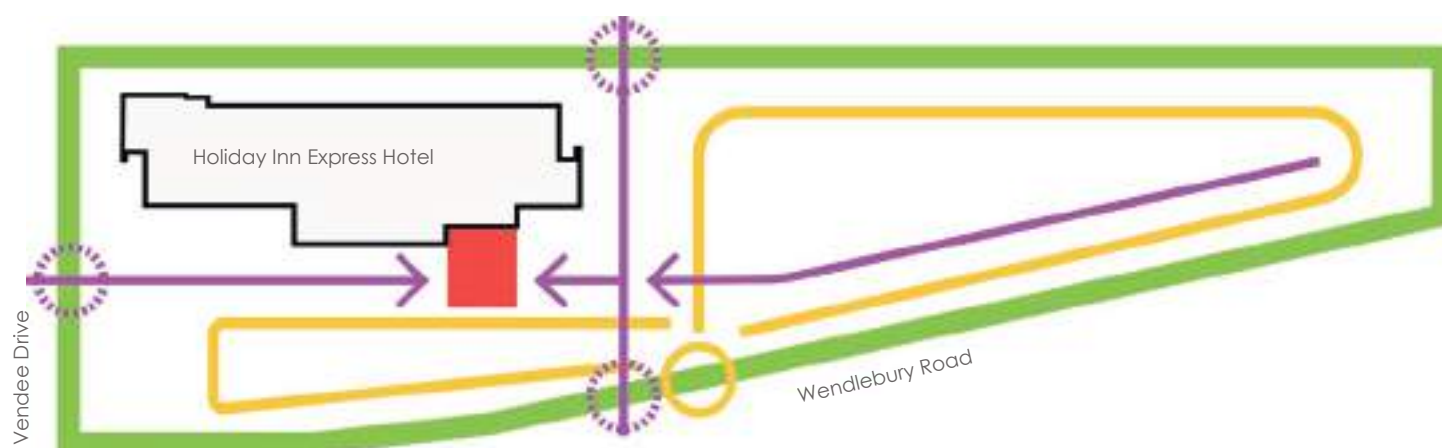
Core mass

Vertical elements including the stair core and vertically proportioned openings both project and puncture the core mass to give a sense of visual balance.





PEDESTRIAN ACCESS PLAN



ACCESS DIAGRAM

### 3.5 ACCESS TO THE SITE

Primary access to the site for vehicles will be from the dedicated existing Gateway access directly off Vendee Drive. For cyclists and pedestrians alike there are numerous means access onto the site.

Within the site demise the roadways and pathways have been set out to accommodate the necessary vehicular and pedestrian movements within the site. Largely the site remains as existing but the new layout around the new entrance area has been tracked by our highways engineer.

#### PEDESTRIAN ACCESS

The site is fully accessible to pedestrians with numerous pathways surrounding the site and within the site itself. Additionally, within this package of works the neighbouring pedestrian route and cycle path are going to be extended and updated to further enhance pedestrian access. Proposed footpath links from the new bus stop and signalised pedestrian crossing will connect public transport to the site. Pedestrian routes into the site are located at the west near the bus stops and to the south with Vendee Link Road shared cycle route and link towards the future office development to the south. Details of the proposed pedestrian crossing are to be agreed through the discharge of conditions.

#### CYCLING

The site is fully accessible to cyclists by road and neighbouring cycle routes. There is an existing shared use footway/cycleway to the south and east of the site from the A41 roundabout along Vendee Drive Link Road, and along Wendlebury Road from the junction with the Vendee Drive Link road to a point opposite the sewage works entrance next to Bicester Avenue Garden Centre.

A new illuminated 3m wide shared use footway/cycleway parallel with the A41 to the west of the site will be formed by widening the existing footway across the junction of A41/Wendlebury Road and extending to the junction with Vendee Drive Link Road.

36 secure & covered cycle spaces are to be provided within the scheme and the building has dedicated showers, lockers and changing areas for members of staff.

#### BUS

Currently there is a good bus service on the A41 past the site and the existing HGV layby on the southbound A41 is to be converted to a new bus stop with shelter. There is an existing bus stop with shelter on the northbound side. A new signal controlled pedestrian crossing will be provided across the A41 near the bus stop locations to improve crossing of the A41. Details of the new bus stop are to be agreed through the discharge of conditions.



### 3.5 ACCESS TO THE SITE

#### CAR

The site can be easily accessed by private car and 149 spaces are provided within the scheme. The site is neighbours the A41 dual carriageway and is a few minutes drive to junction 9 of the M40 motorway.

#### RAIL

Bicester village rail station is located approximately 15 minutes walk away and a 5 minute ride by bus, where a regular & fast service into London Marylebone is provided.

#### < TRANSPORT DIAGRAM

The diagram to the left clearly illustrates travel distances/times from the site to the local transport links and amenities. The site's close proximity to Bicester town centre, Bicester Village & the A41 means the future scheme will be easily accessible by all means accessible travel & transport.

#### KEY

- 1. Bicester Park & Ride
- 2. Existing Residential
- 3. Wyevale Garden Centre
- 4. Tesco Superstore
- 5. Bicester Village
- 6. Towards Bicester Town Centre
- 7. Bicester Village Train Station



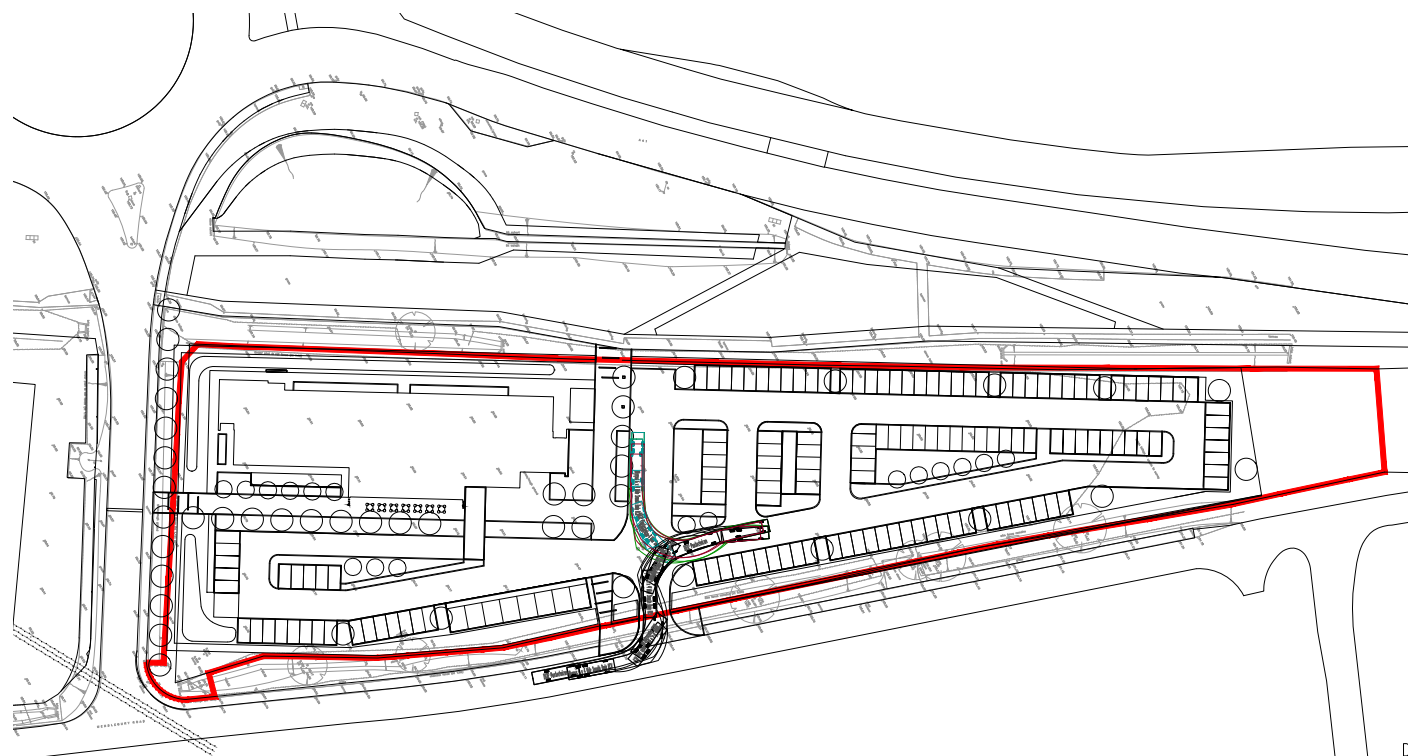


DIAGRAM 1 - VEHICULAR TRACKING

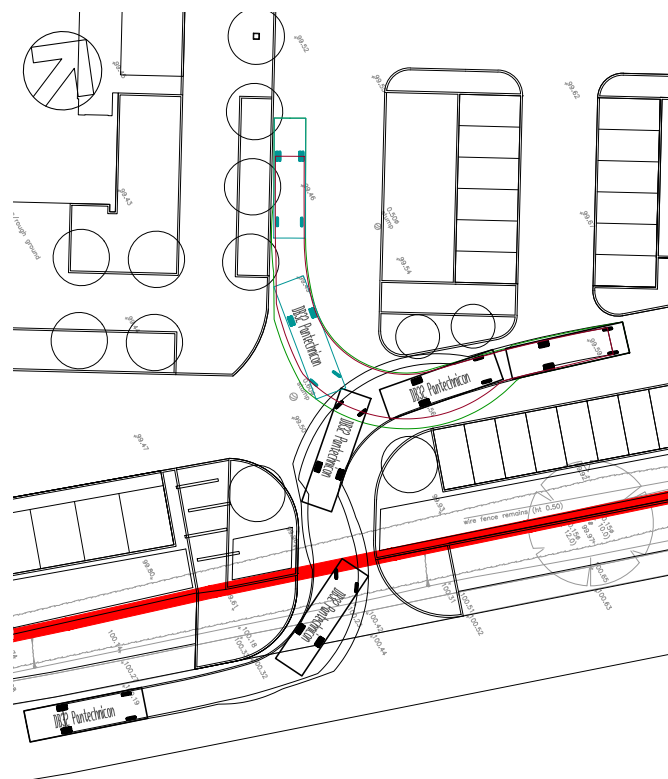


DIAGRAM 2 - VEHICULAR TRACKING

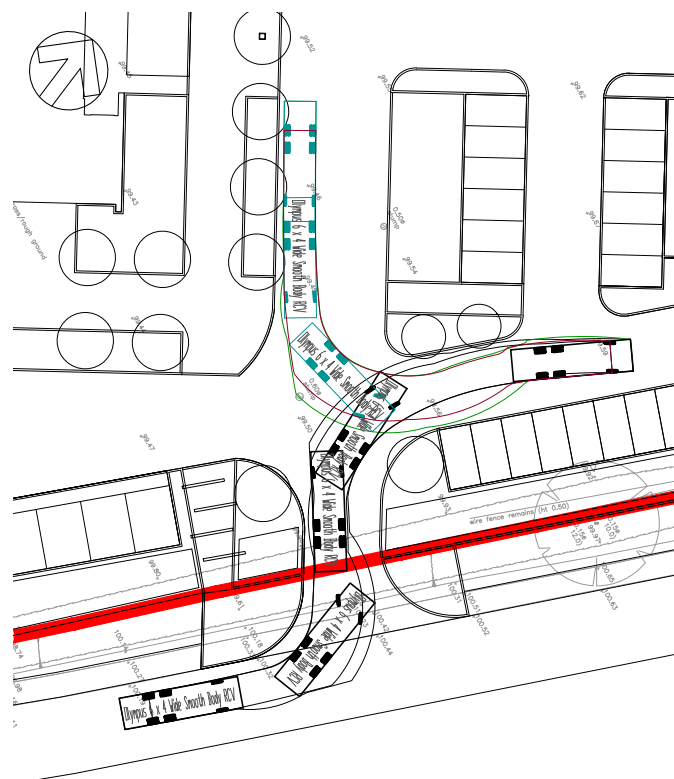


DIAGRAM 3 - VEHICULAR TRACKING

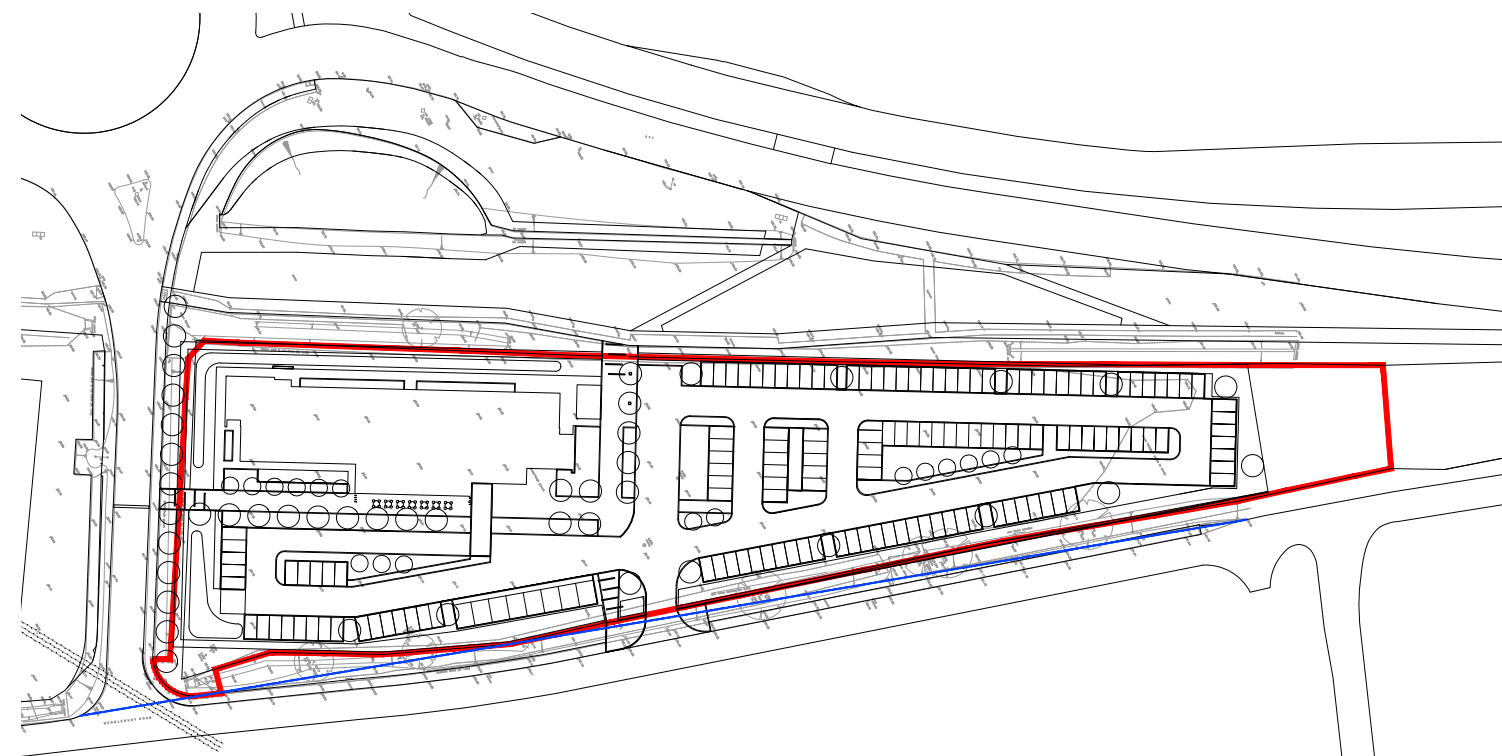


DIAGRAM 4 - VISIBILITY SPLAYS

### 3.5 ACCESS TO THE SITE

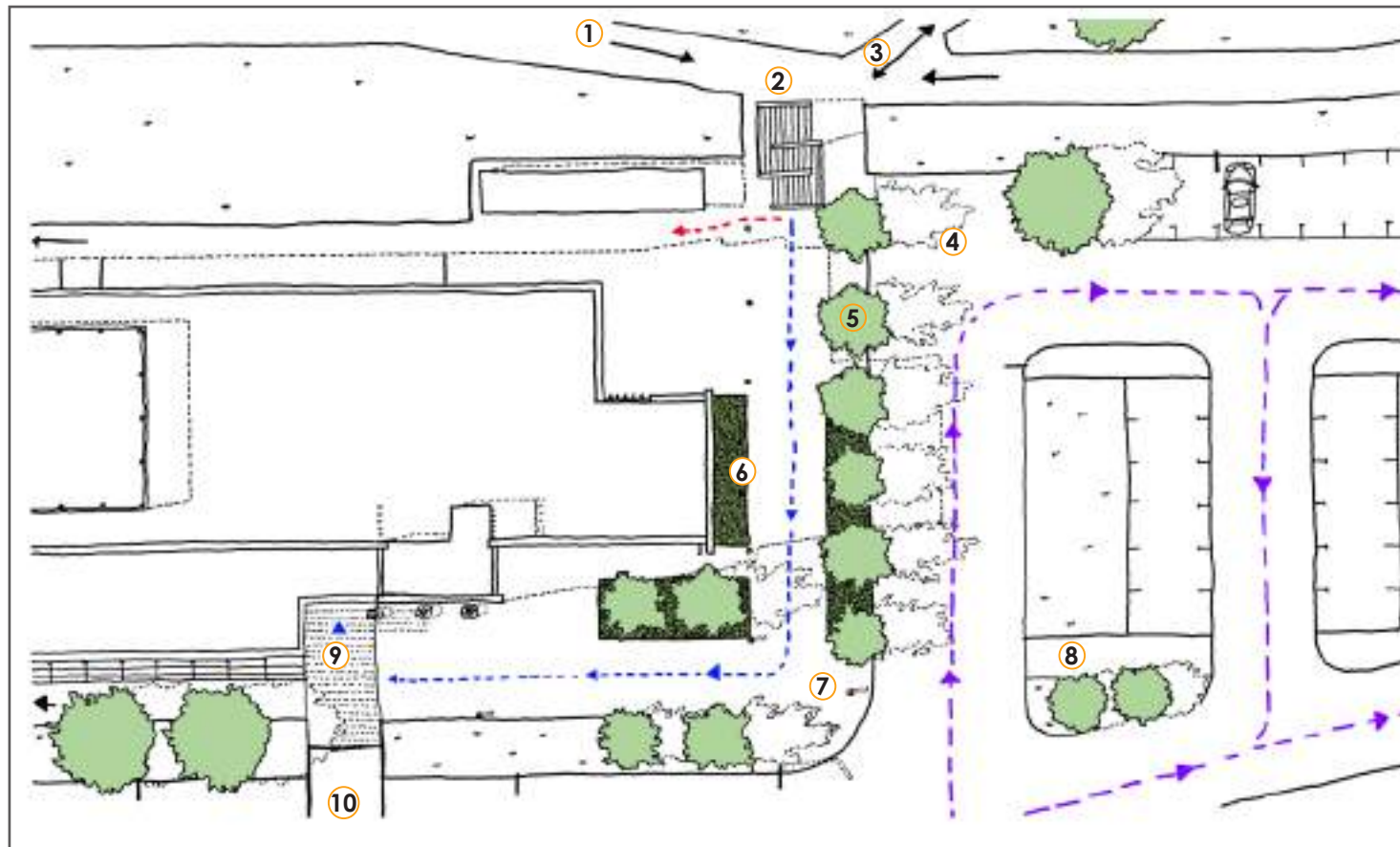
#### VEHICULAR ACCESS

The site is located to the northeast of the A41/Vendee Drive roundabout. Proposed vehicular access is proposed from Wendlebury Road to the north of Vendee Drive Link Road via a simple T-junction layout. Vehicle swept path analysis has been provided to ensure large delivery and service vehicles can turn in and out of the site from Wendlebury Road. Junction visibility splays of 2.4m x 120m suitable for 40 mph are shown that maximise the retention of the existing hedgerow whilst meeting the appropriate visibility requirements. The existing National speed limit will be reduced to 40mph along Vendee Drive Link Road and Wendlebury Road adjacent the site from the junction with the A41, past the junction with Vendee Drive Link Road extending to the existing T-junction to the south.

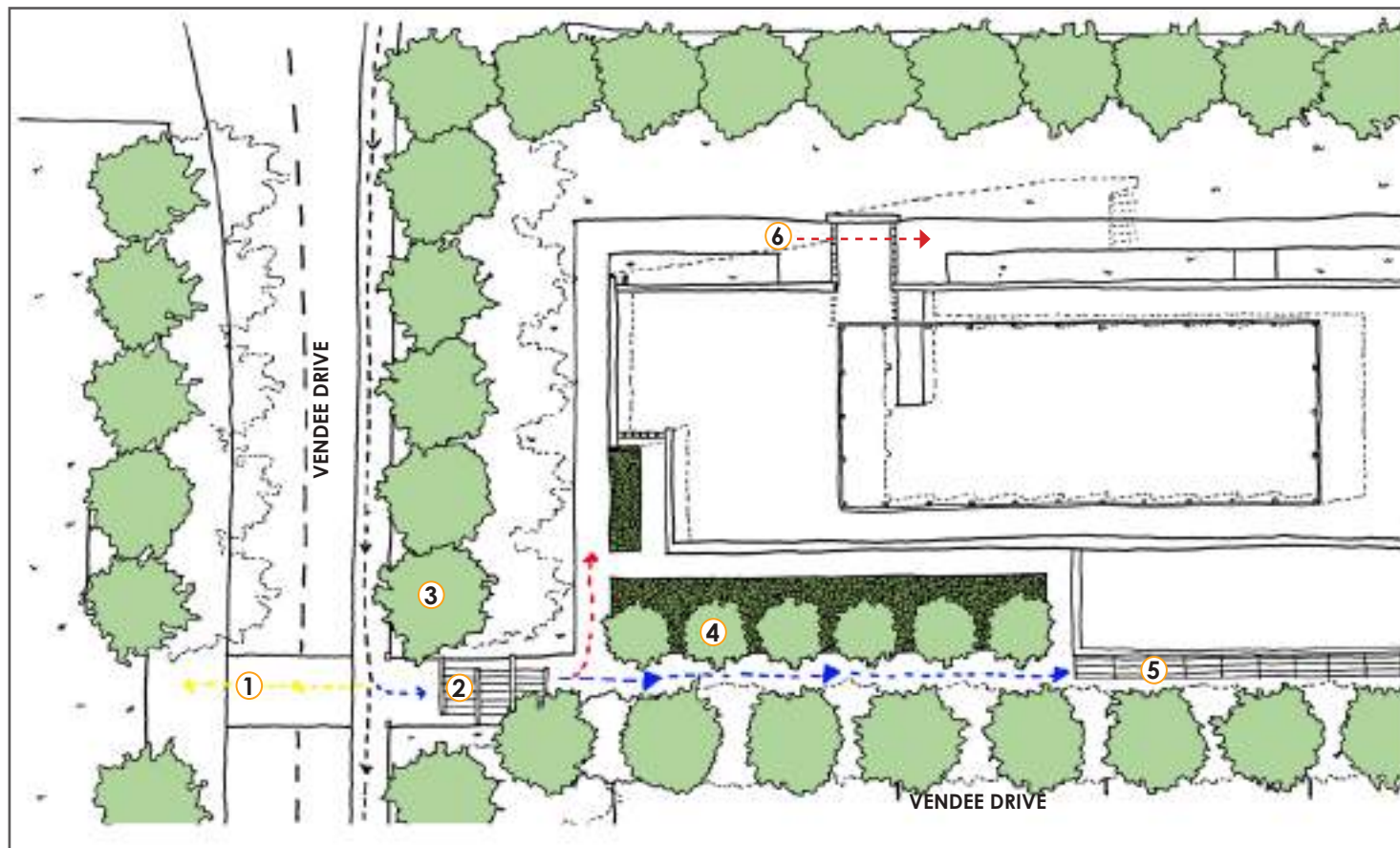
A separate minor access from Wendlebury Road is provided to a proposed foul water pumping station compound to the north of the site.

SEE WSP TRACKING + VISIBILITY SPLAYS BELOW.





PUBLIC ACCESS DIAGRAM FROM A41



PUBLIC ACCESS DIAGRAM FROM VENDEE DRIVE

#### PUBLIC ACCESS DIAGRAM FROM A41 KEY

1. New cycle path from Vendee drive
2. Feature entrance canopy
3. Direct connection to new bus stop
4. Deliveries Area
5. Liquidamber Styracifua boulevard Trees
6. Feature planting directed around main entrance route
7. Access to main entrance from car park
8. Pedestrian routes through car parking
9. Main Entrance
10. Drop-off Zone

Car Park Route

Route to main entrance

Towards Back of house facilities

#### PUBLIC ACCESS DIAGRAM FROM VENDEE DRIVE KEY

1. Pedestrian crossing towards phase 1 office scheme
2. Feature entrance canopy
3. Dense Hedging/Pleached trees to Vendee Drive
4. Liquidamber Styracifua boulevard Trees
5. Solar shaded external seating
6. Pedestrian walkway under stair core.

Route towards phase 1 office scheme Site

Route to main entrance

Towards Back of house facilities

### 3.5 ACCESS TO THE SITE

#### ACCESS FOR ALL

Access for all within the scheme has been a key consideration for the design team and the development will be fully compliant with Approved Document M of the Building Regulations (AD M) and the Equality Act. The following areas have been incorporated at pre-application stage and the design will continue to be refined during the planning, detail and construction phases of the project.

The site is relatively flat and level access is provided into the building from external areas. The drop off area is level and the entrance areas have drop kerbs to allow easy access for wheelchair users. Entrances have been designed to allow for ambulant disabled and wheel chair access. The main entrance opens onto a common reception and circulation areas situated at the heart of the building.

Access to the buildings is via a glazed automatic doors with a diameter width compliant with AD.M.

All pedestrian pathways within the site will be suitably sized to accommodate wheelchair users.

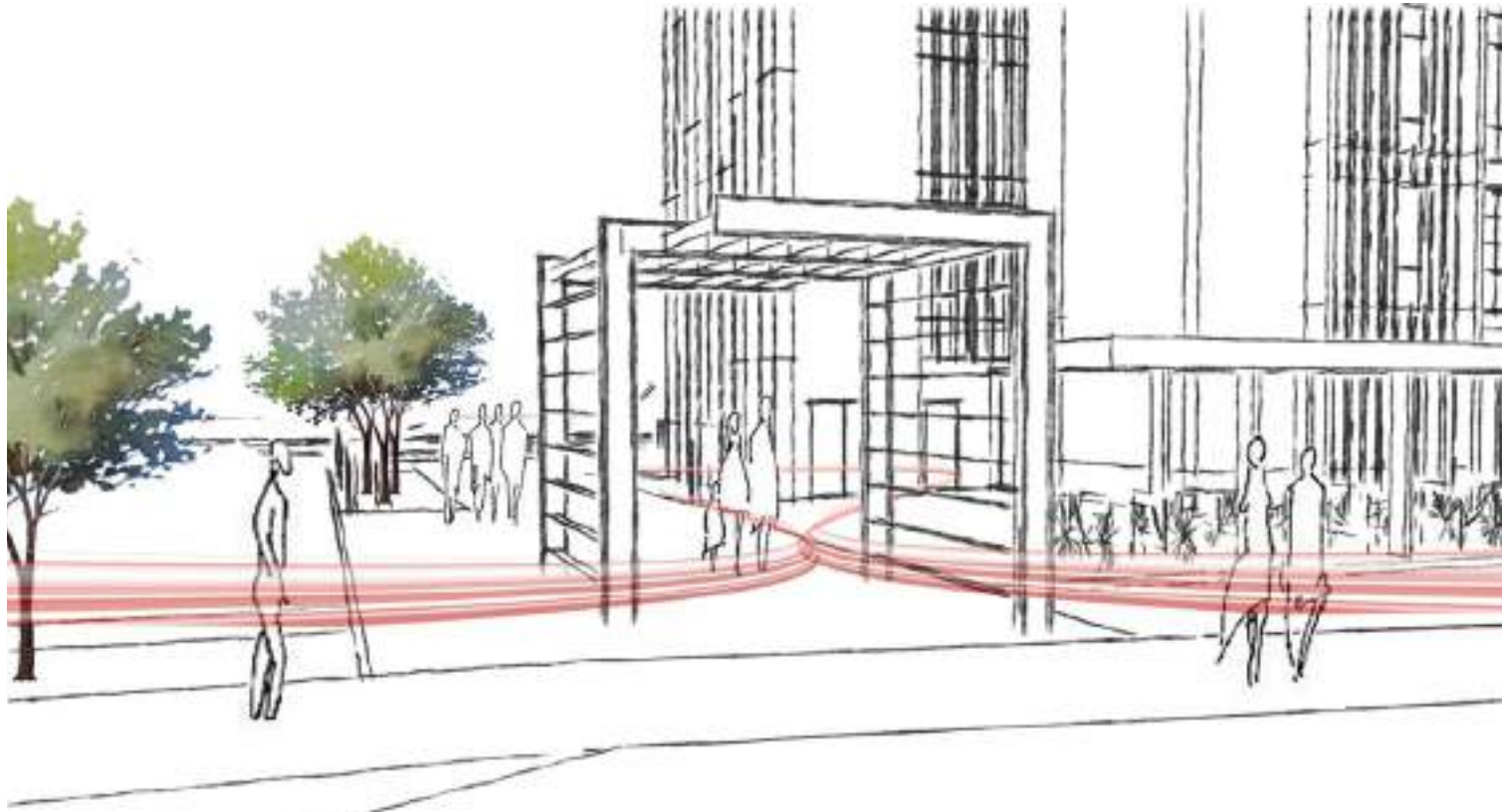
Circulation of the reception, toilet accommodation and entrances into the work spaces are fully compliant with AD M.

Wheelchair users (and non-disabled persons) can enter the building using the proposed 'street' approaches, from the carpark/ drop off area into the primary core of the building. The vehicle access route is directly off Wendlebury Road and dedicated disabled spaces are being provided directly adjacent to the main drop off point.

Ambulant disabled and non-disabled persons can enter the building from the car park via the level access 'street' powered doors to the main receptions.

The reception area will contain easily identifiable waiting areas and good signage, to comply with the Equality Act to promote access for disabled people.





ENTRANCE GATEWAY CONCEPT SKETCH

### 3.5 ACCESS TO THE SITE

#### PEDESTRIAN GATEWAYS

To enhance the flow of pedestrians to, from and through the site we have designed a number of purpose built entrance structures that are to create a coherent and robust edge & entrance into the site. In addition, the landscaping has been designed to mirror this sense of arrival with clearly defined entrance routes to gently guide visitors in/through the site.

The structures themselves are designed in line with the buildings contemporary aesthetic and are to be made from a combination of metallic finishes that compliment the buildings material palette. These sculptural entrance features further emphasise our commitment to design quality and will provide a unique and welcoming gateway into the site.



GATEWAY PRECEDENT 1



GATEWAY PRECEDENT 2



A41 BUILDING APPROACH



VENDEE DRIVE BUILDING APPROACH

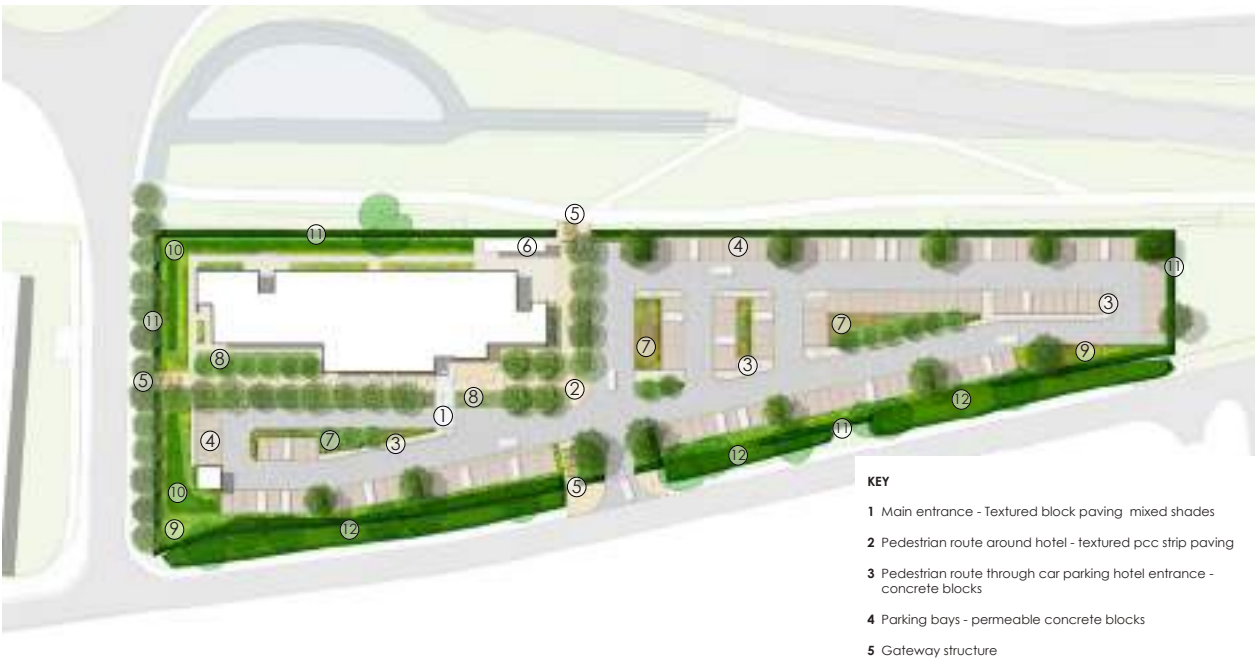


3.6 LANDSCAPING

In order to develop the landscaping proposals, both hard and soft, post our pre-application discussions, we have worked closely with our landscape designers to incorporate officers comments and suggestions to develop a thorough and considered landscape proposal for the scheme, with the landscape scheme fully informing the development of the design and, in particular, developing the following main objectives :

- To provide a clear, safe entrance with a true sense of arrival;
- To provide a strong, landscape setting that is appropriate both in scale with the building and its relationship with the parkland as a whole and sympathetic to the architectural style of the new building;
- To ‘work with’ the large amount of existing landscape that will remain and seek, where appropriate, to improve and enhance these areas;
- To provide a visually integrated edge between the new and existing landscape, which ties in with the wider landscape context beyond;
- To retain and protect the existing tree and shrub planting wherever practicable, together with appropriate mitigation measures for new landscape works and planting to enhance the objectives above;
- In collaboration with the Consultant Ecologist, to identify and protect the existing wildlife habitat and biodiversity of the site, provide mitigation measures where this may come under threat and improve and extend these to establish new habitats as and where appropriate.

A full landscape statement and landscape drawings have been produced by the Landscape Architect - Turkington Martin, and form part of the overall submission and should be referred to.



LANDSCAPE MASTERPLAN



VENDEE STREET SITE SECTION



LIQUIDAMBAR STYRACIFUA



QUERCUS RUBRA



ACHILLEA MOONSHINE

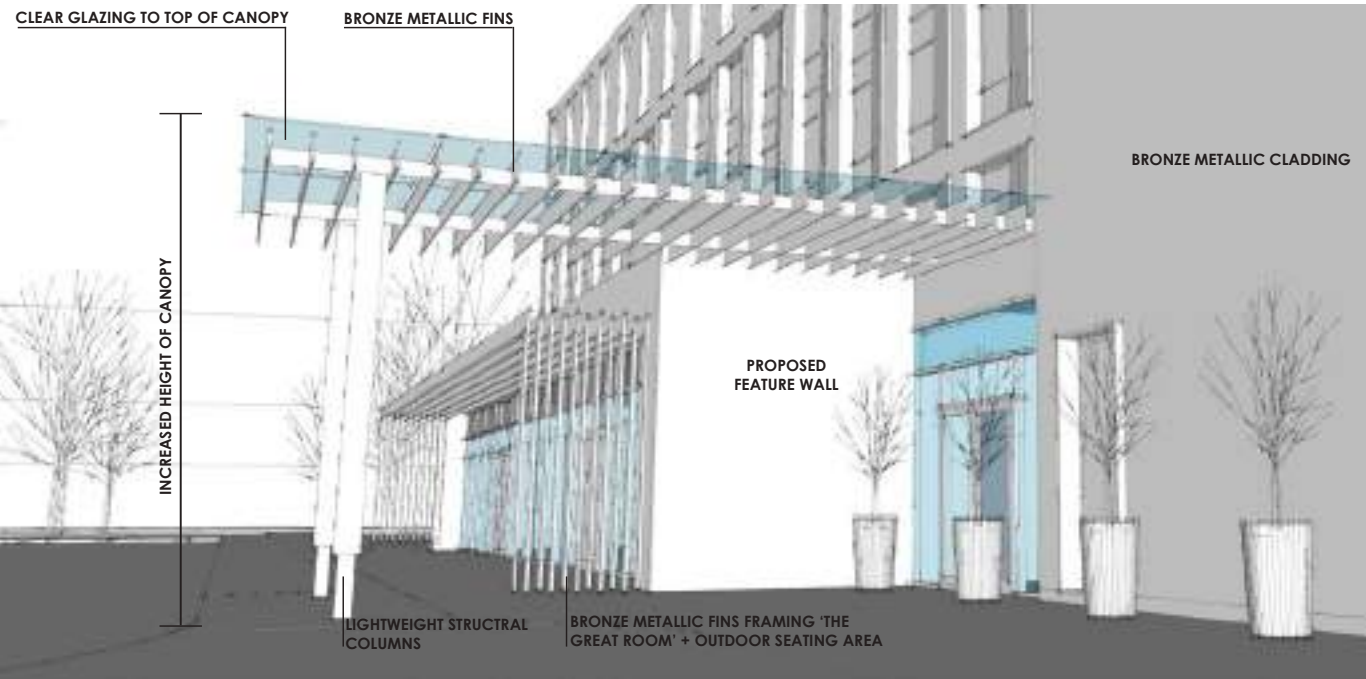


ALIUM GLOBEMASTER



CALAMAGROSTIS ACUTIFLORA

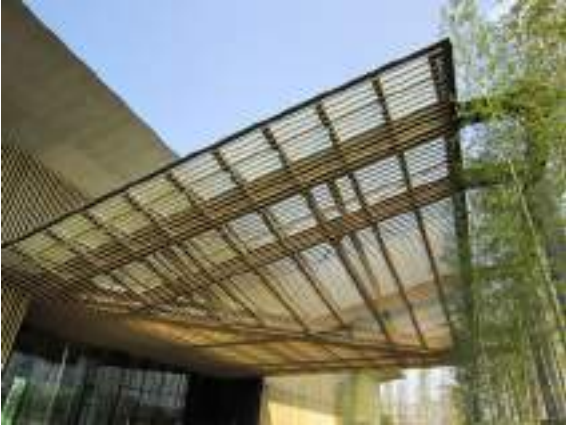




CANOPY CONCEPT SKETCH



CANOPY PRECEDENT 1



CANOPY PRECEDENT 2



CANOPY PRECEDENT 1

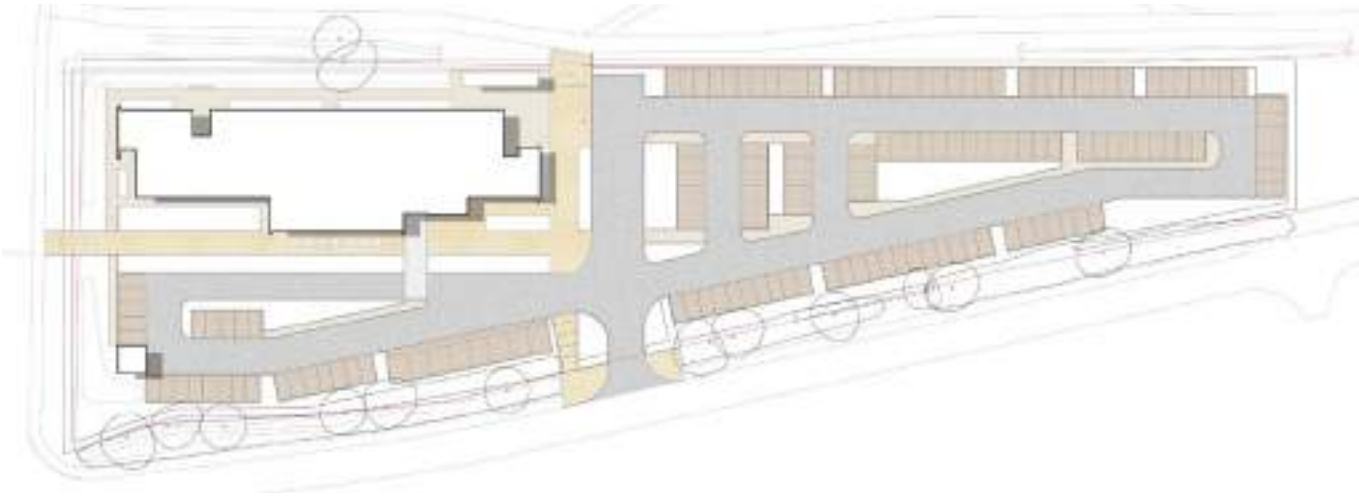
### 3.6 LANDSCAPING

#### LANDSCAPING - ENTRANCE STRATEGY

The landscape proposals seek to integrate the proposed building into the existing site and the wider landscape. The concept establishes a strong approach and arrival space utilising a shared surface which incorporates vehicle requirements whilst being inviting to visitors and hotel guests alike. The main walkway & the feature entrance canopy at the building entrance seeks to provide an inviting shared public space with an intimate outdoor seating area and contemporary planting to convey a human scale directly adjacent to the large aluminium clad feature wall.

Access for mobility impaired and disabled users has been given priority throughout the design process, the resultant proposals providing improved parking facilities with safe and level access within a short distance of the main building entrance.

Please refer to the full Landscape Statement by Turkington Martin, which has been included as part of this application.



LANDSCAPE PAVING STRATEGY



ENTRANCE PAVING  
CONCRETE BLOCK & STONE  
AGGREGATE



PEDESTRIAN WALKWAY  
LINEAR CONCRETE BLOCK



PARKING PAVING  
GREY PERMEABLE CONCRETE BLOCK



## 4.0 | CONCLUSIONS



### 4.0 SUMMARY + CONCLUSIONS

- The primary objectives have been to develop a design which is sensitive, considered, inclusive and is a worthy addition to the new 'Bicester Gateway'; to create a building that acknowledges and lessens the impact it has on the environment and to create a building that will become a prestigious facility that will attract staff, business personnel and hotel guests alike.

- Whilst there are a number of site constraints, some of which were obvious from the outset and some that we have established through the outline application process with the Cherwell District Council, these have fully informed the design from the initial mass and bulk through to the selection of specific materials and has indeed helped to create a contemporary, dynamic building, that we believe is entirely appropriate to this site.

- In summary, the proposed development will provide a distinctive high quality hotel scheme in line with the client's aspirations, the overall Bicester Gateway masterplan and those of the Local Authority.