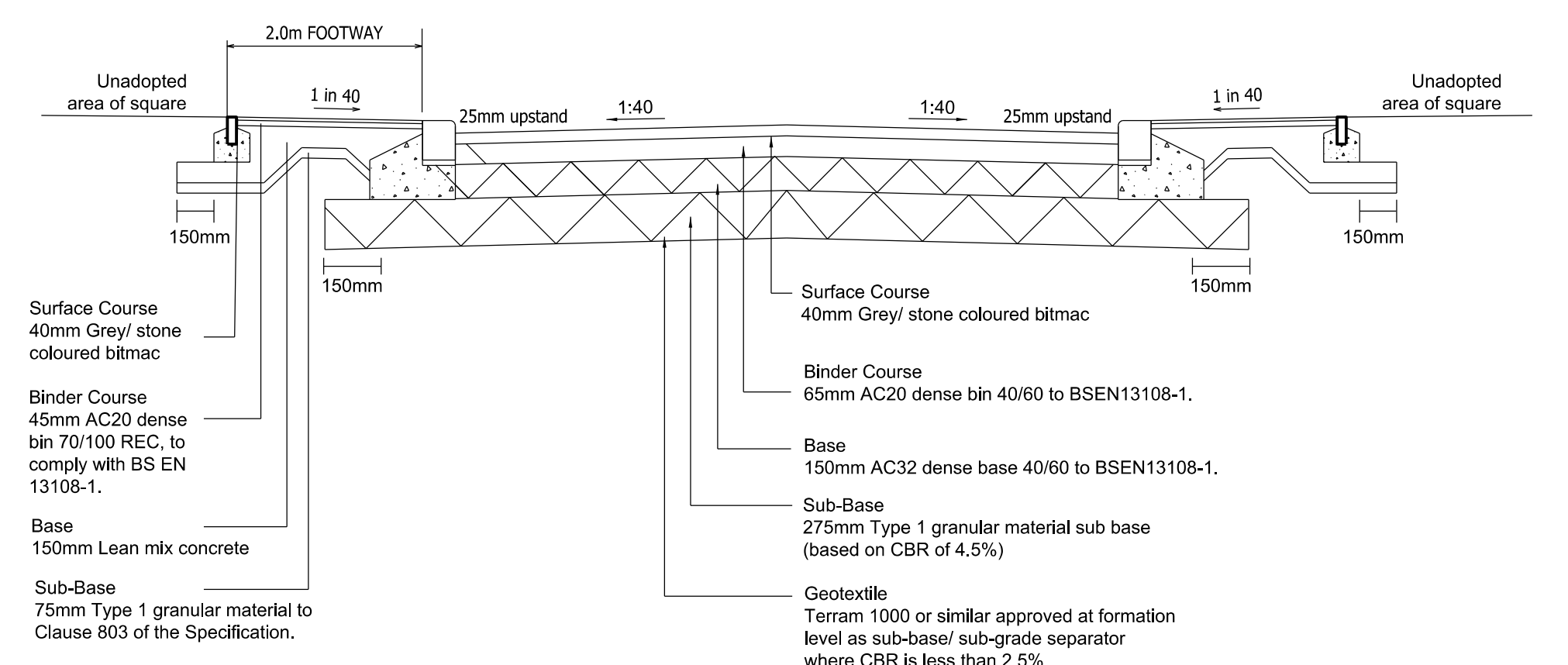
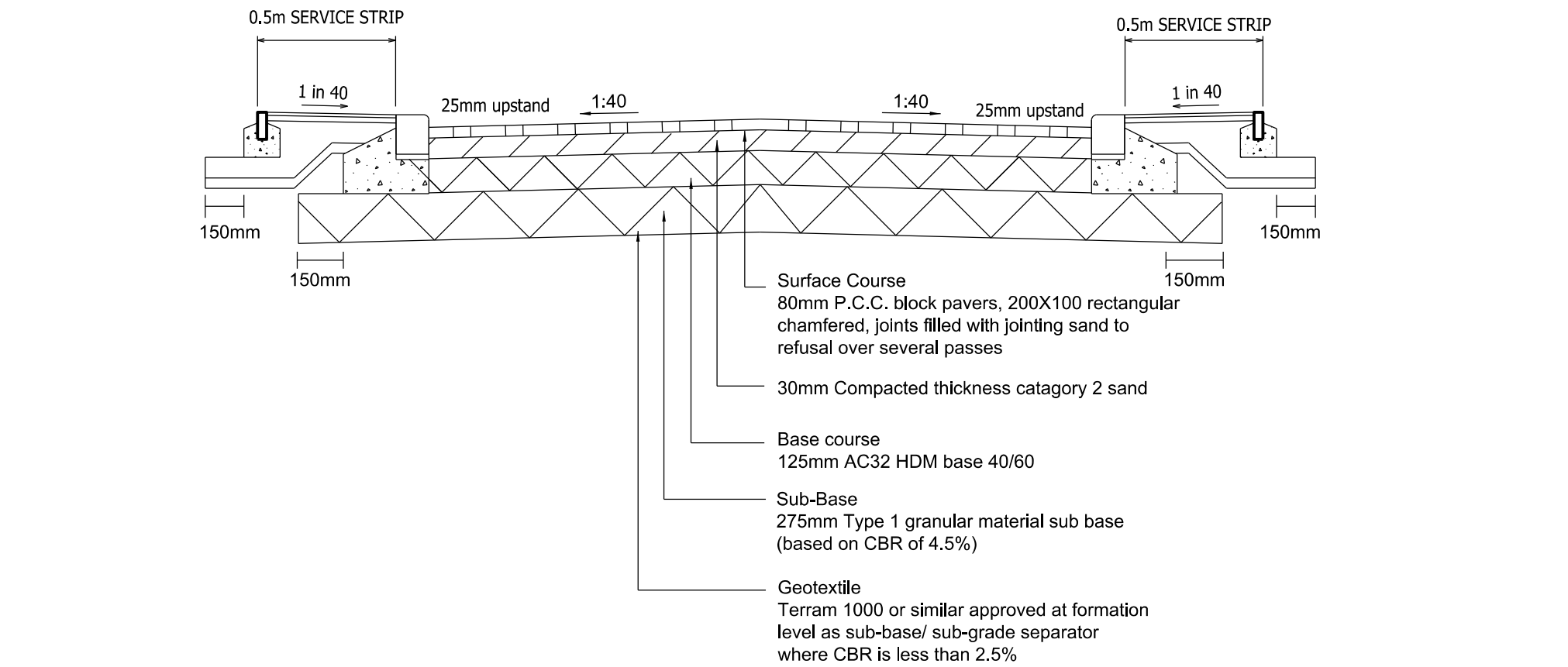


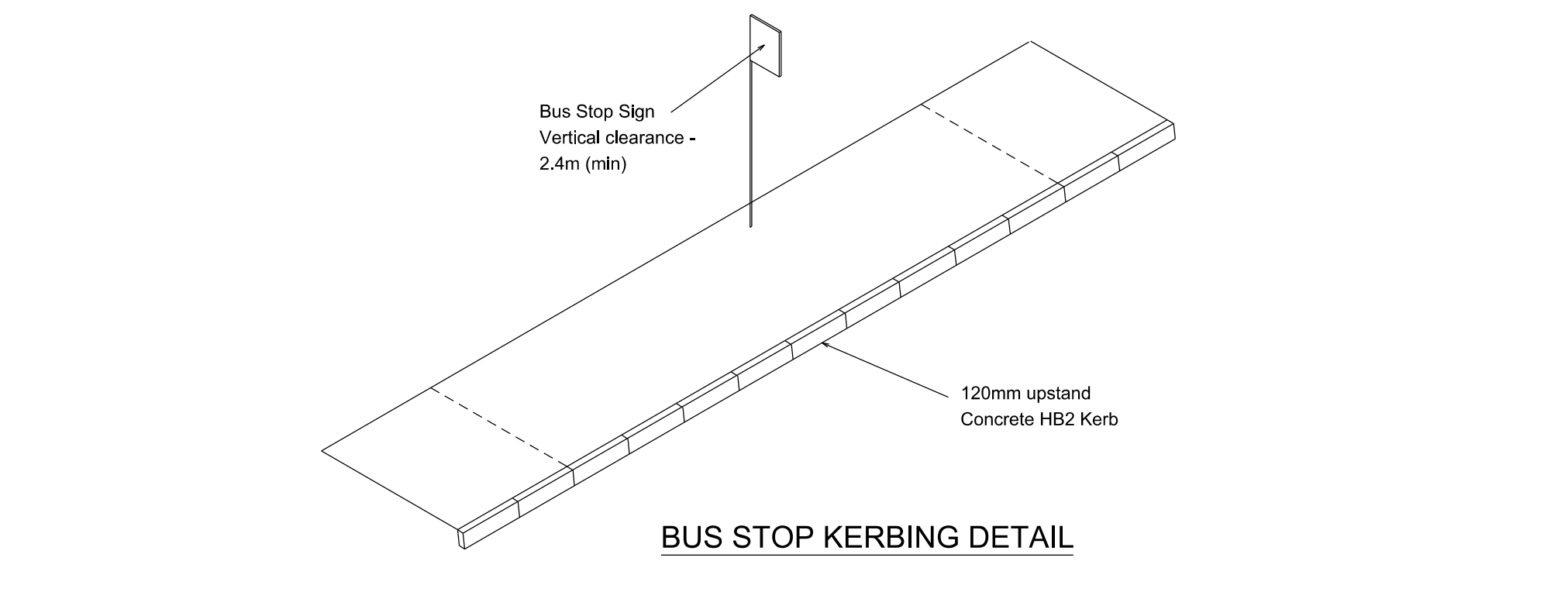
TYPICAL CAMP ROAD FOOTWAY CONSTRUCTION ADJACENT TO CARRIAGEWAY



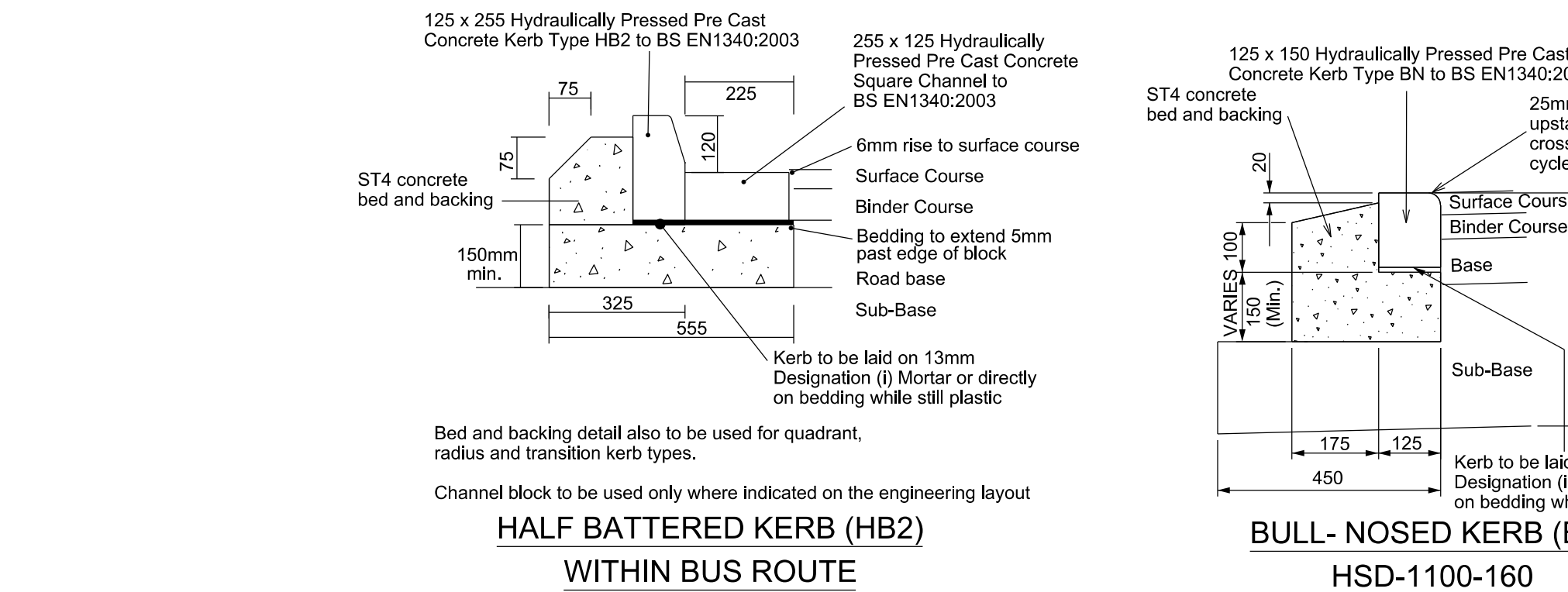
TYPICAL CAMP ROAD BITUMINOUS CONSTRUCTION DETAIL



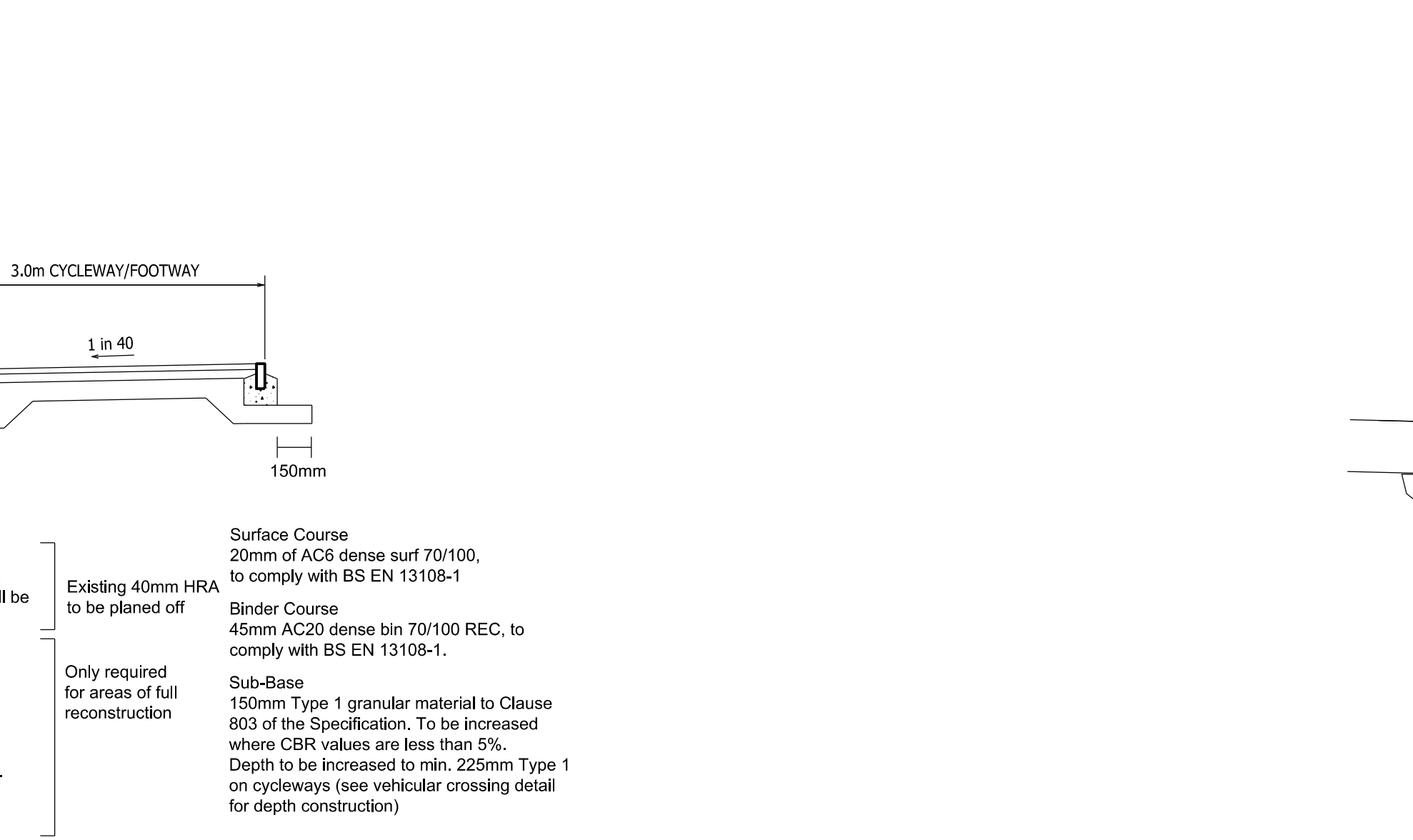
TYPICAL CAMP ROAD ADOPTABLE 'VILLAGE SQUARE' RAISED TABLE



TYPICAL CAMP ROAD ADOPTABLE BLOCK PAVING RAISED TABLE



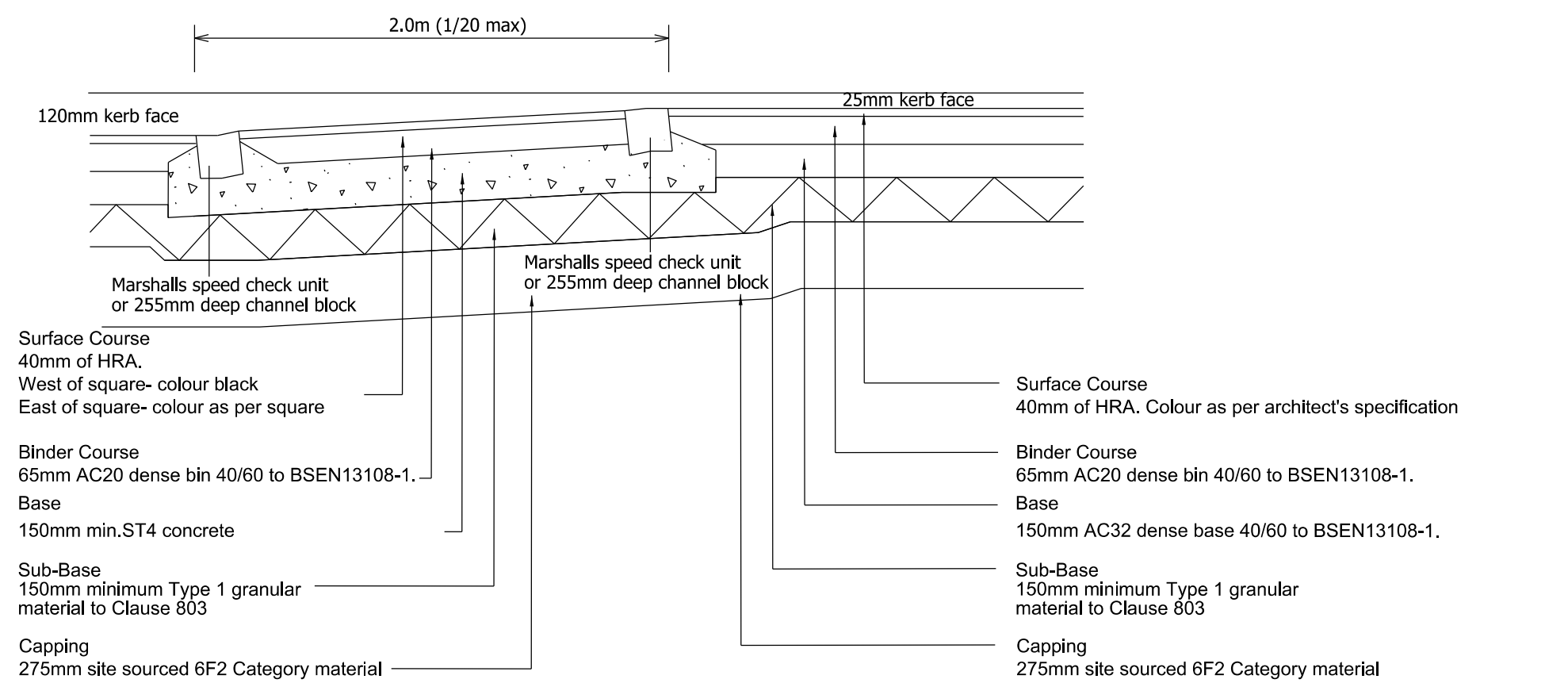
BUS STOP KERBING DETAIL



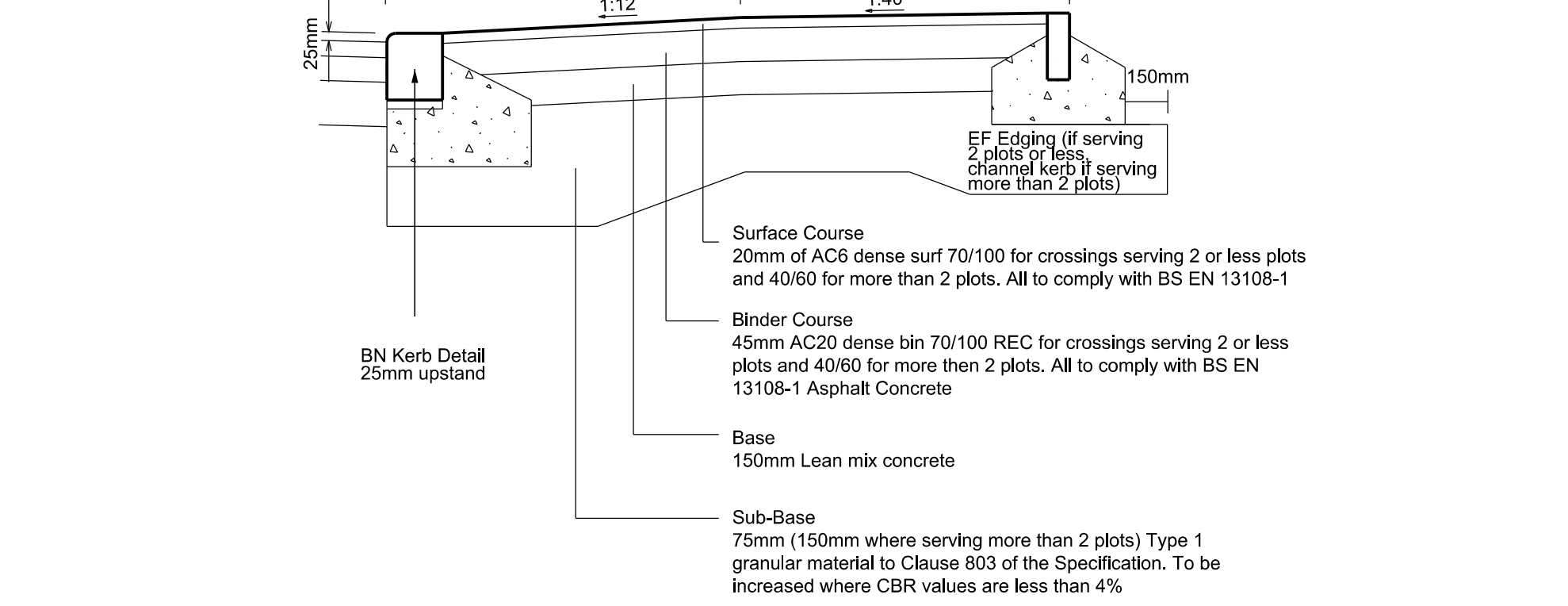
TYPICAL CAMP ROAD FOOTWAY CONSTRUCTION NOT ADJUTING TO CARRIAGEWAY



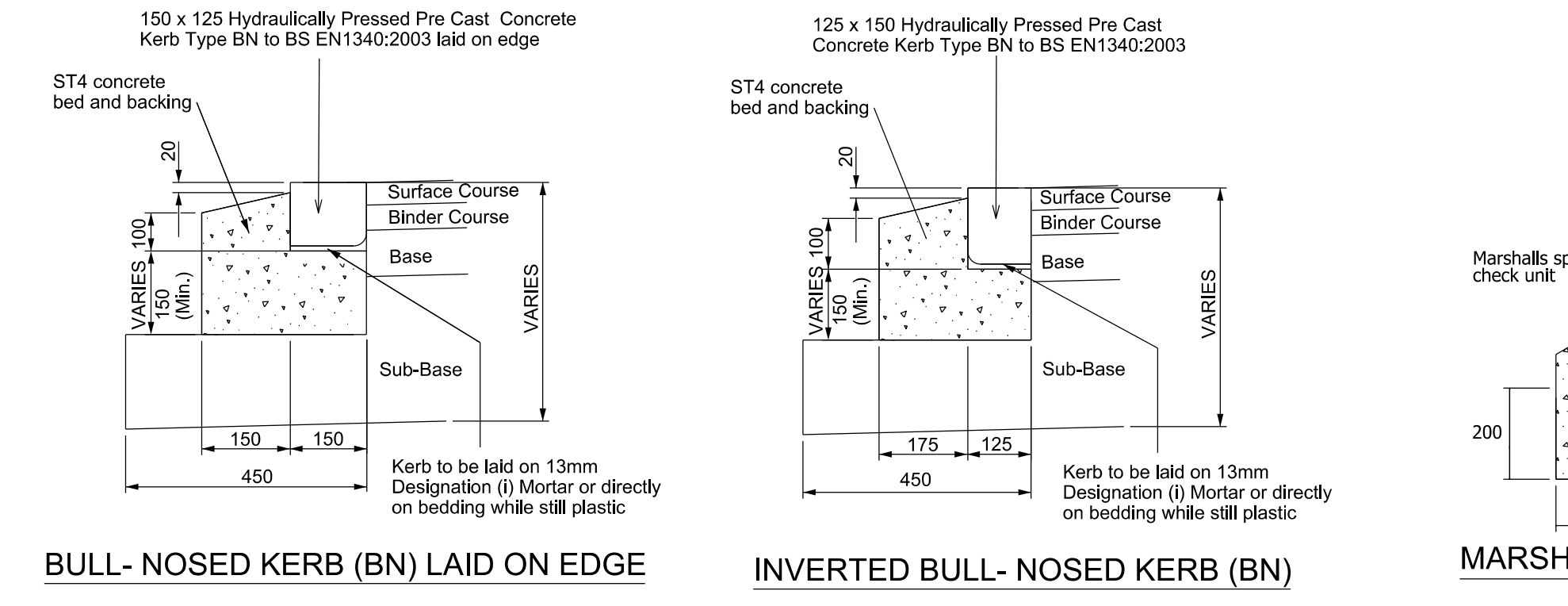
TYPICAL CAMP ROAD ADOPTABLE 'VILLAGE SQUARE' RAISED TABLE



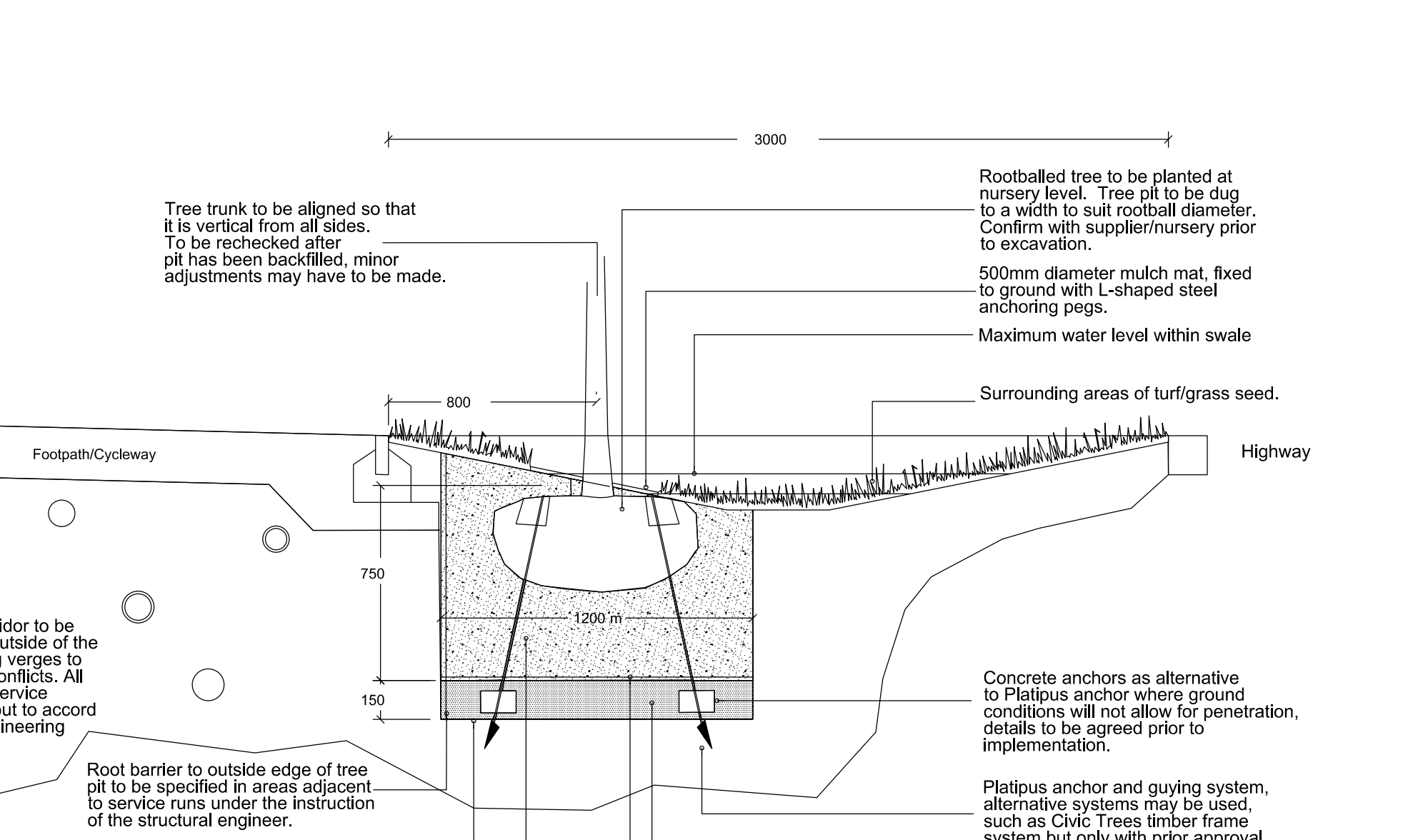
TYPICAL CAMP ROAD ADOPTABLE BLOCK PAVING RAISED TABLE



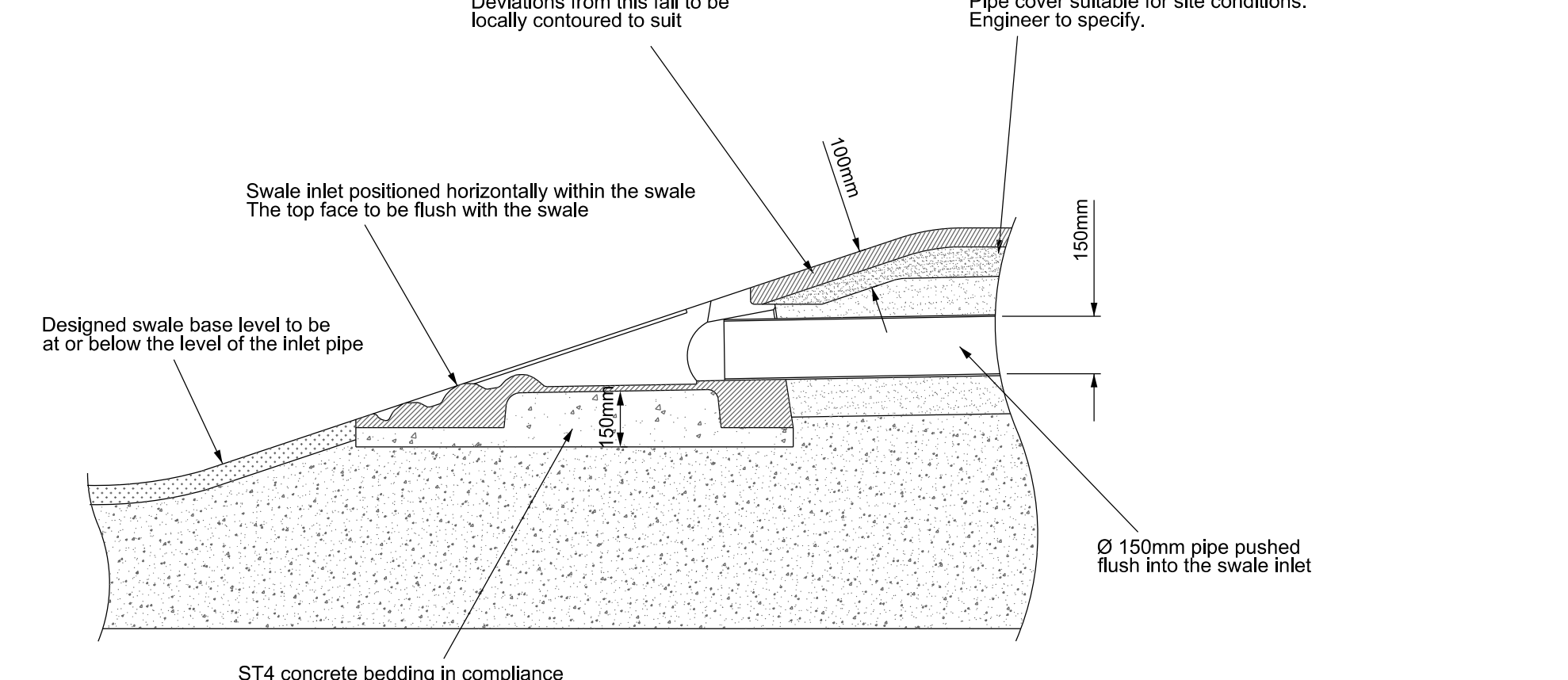
TYPICAL CAMP ROAD ADOPTABLE BLOCK PAVING RAISED TABLE



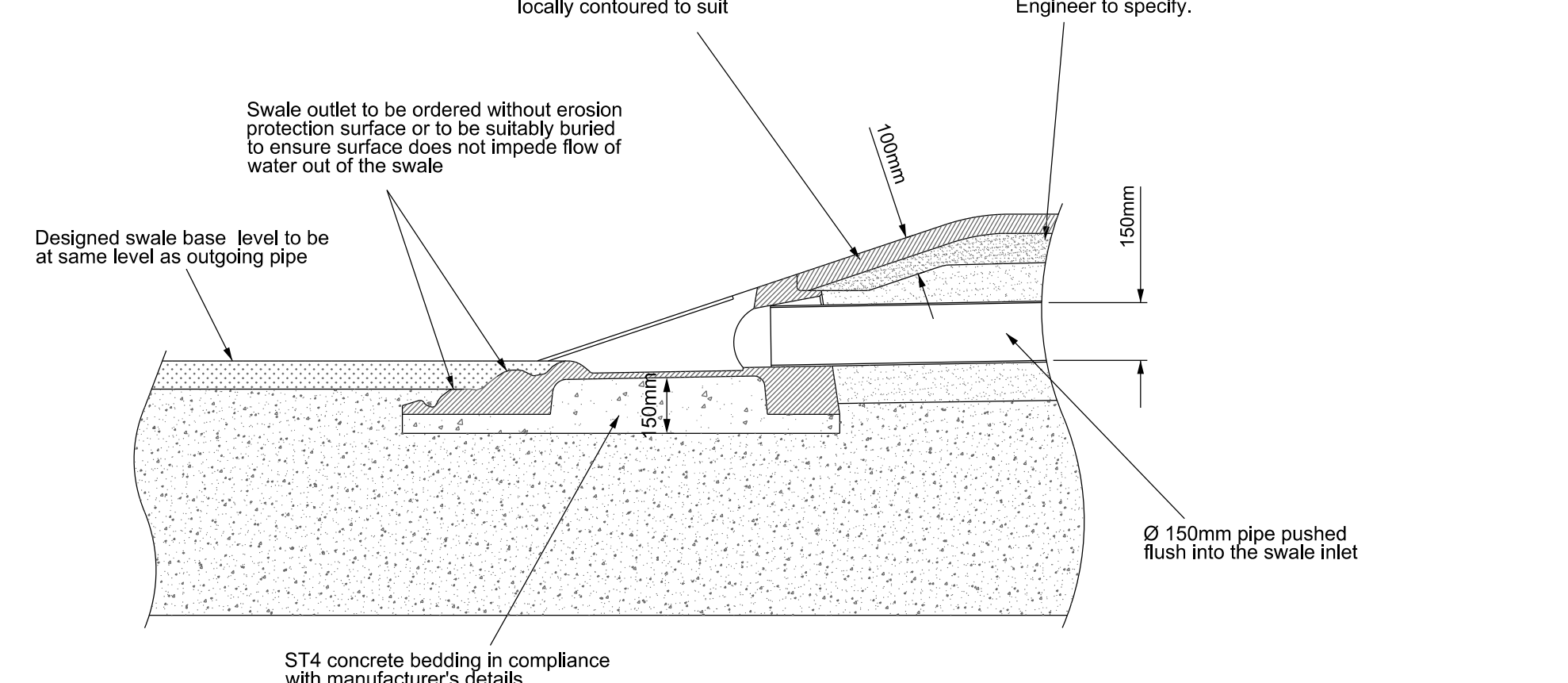
TYPICAL CAMP ROAD ADOPTABLE BLOCK PAVING RAISED TABLE



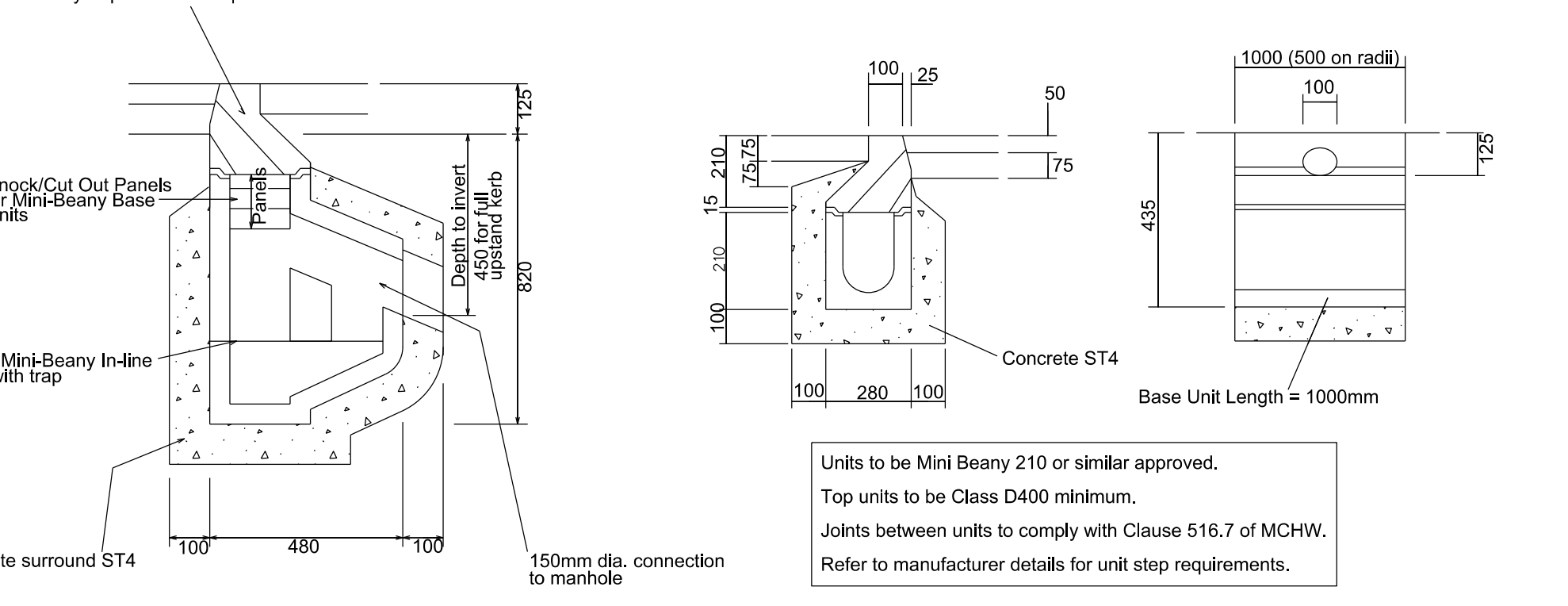
TYPICAL SECTION THROUGH SWALE FEATURE



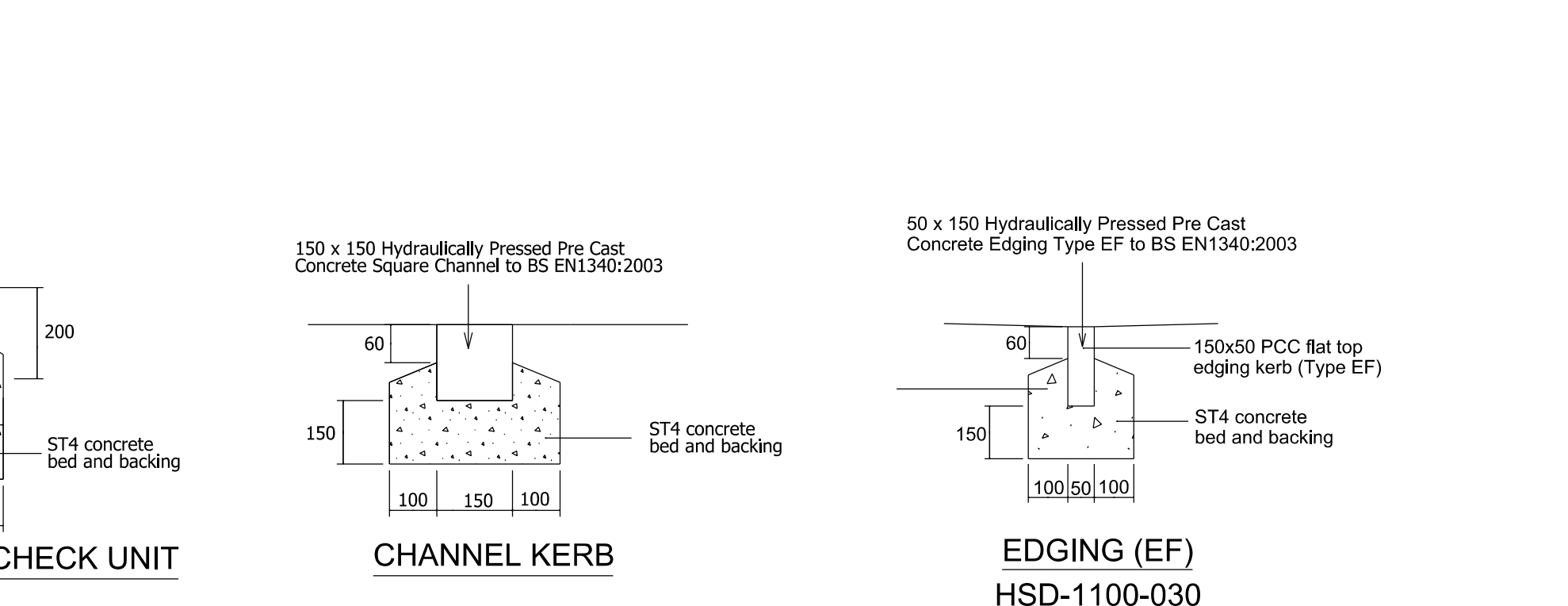
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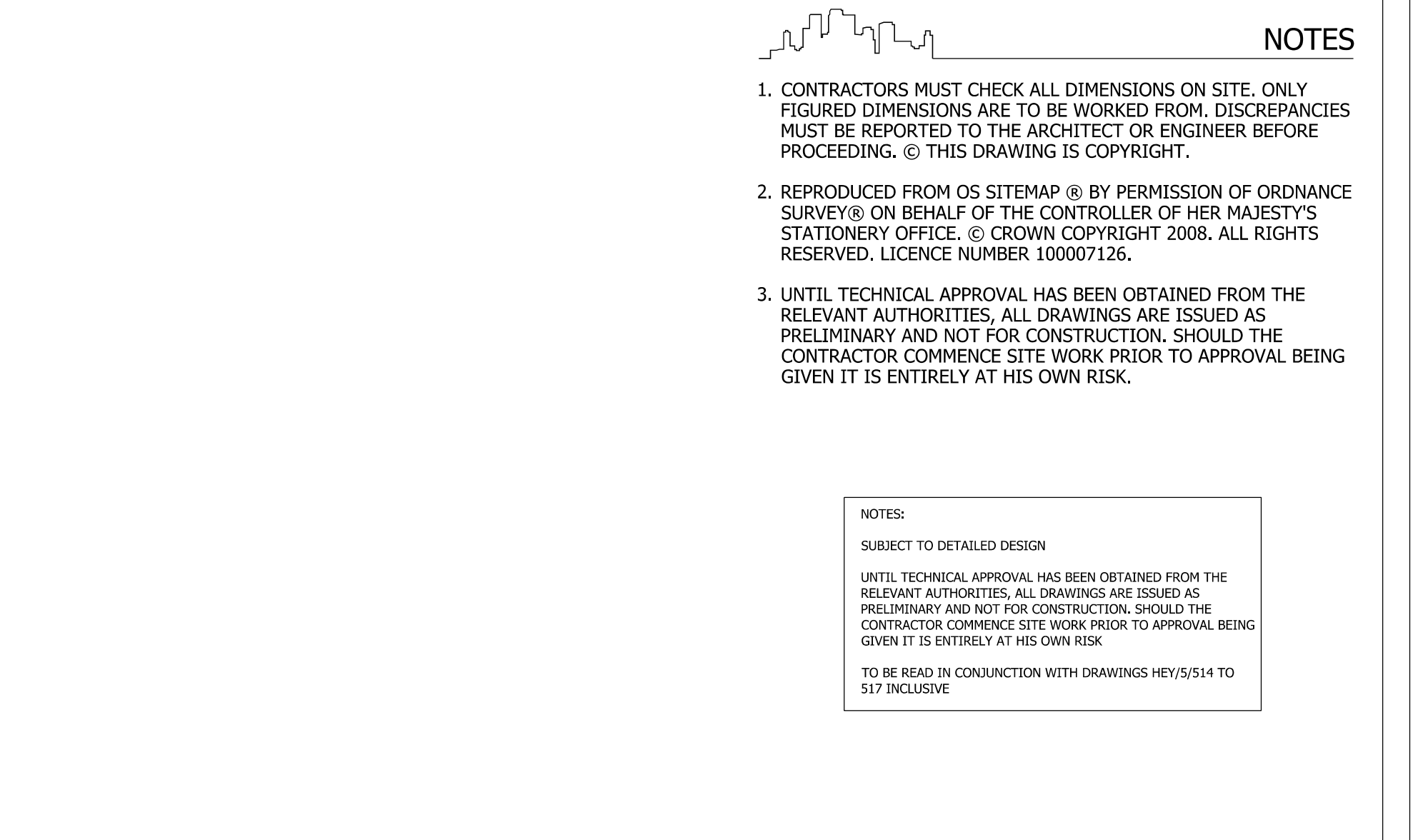
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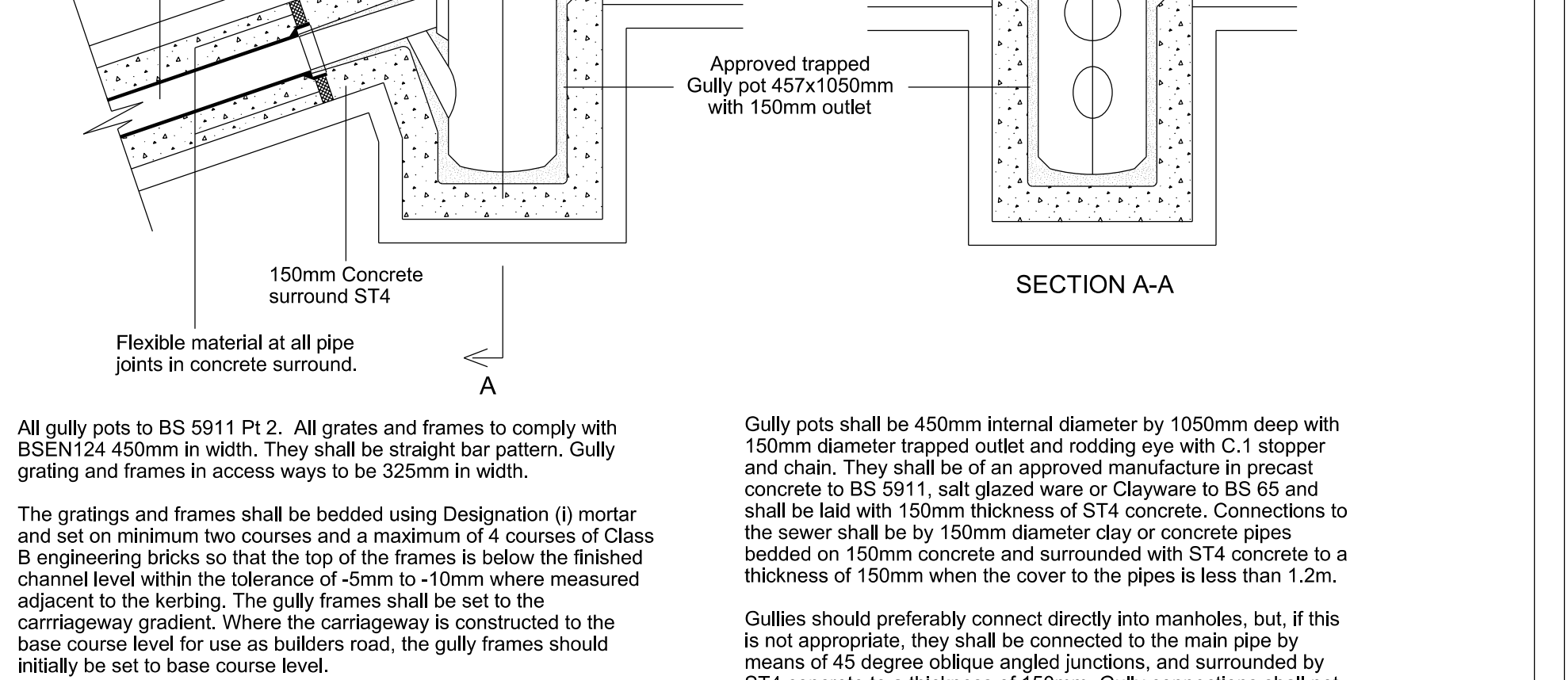
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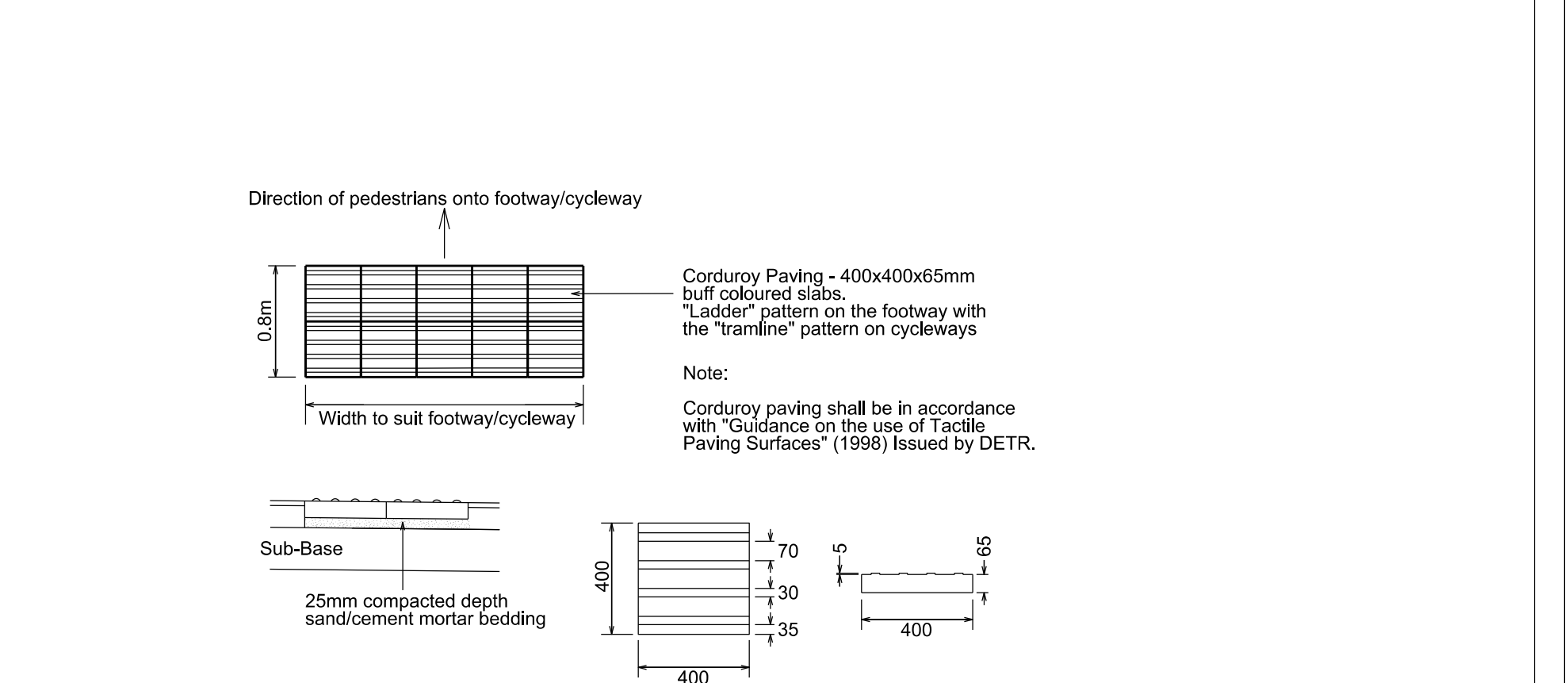
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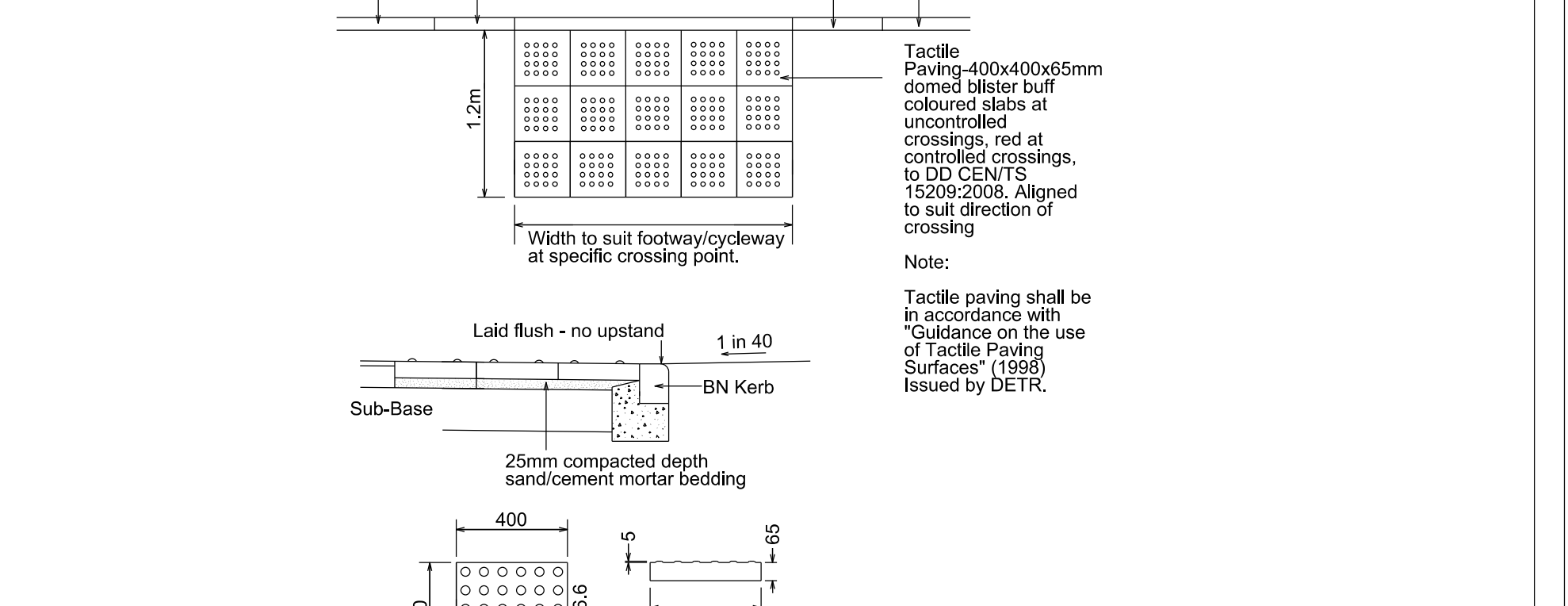
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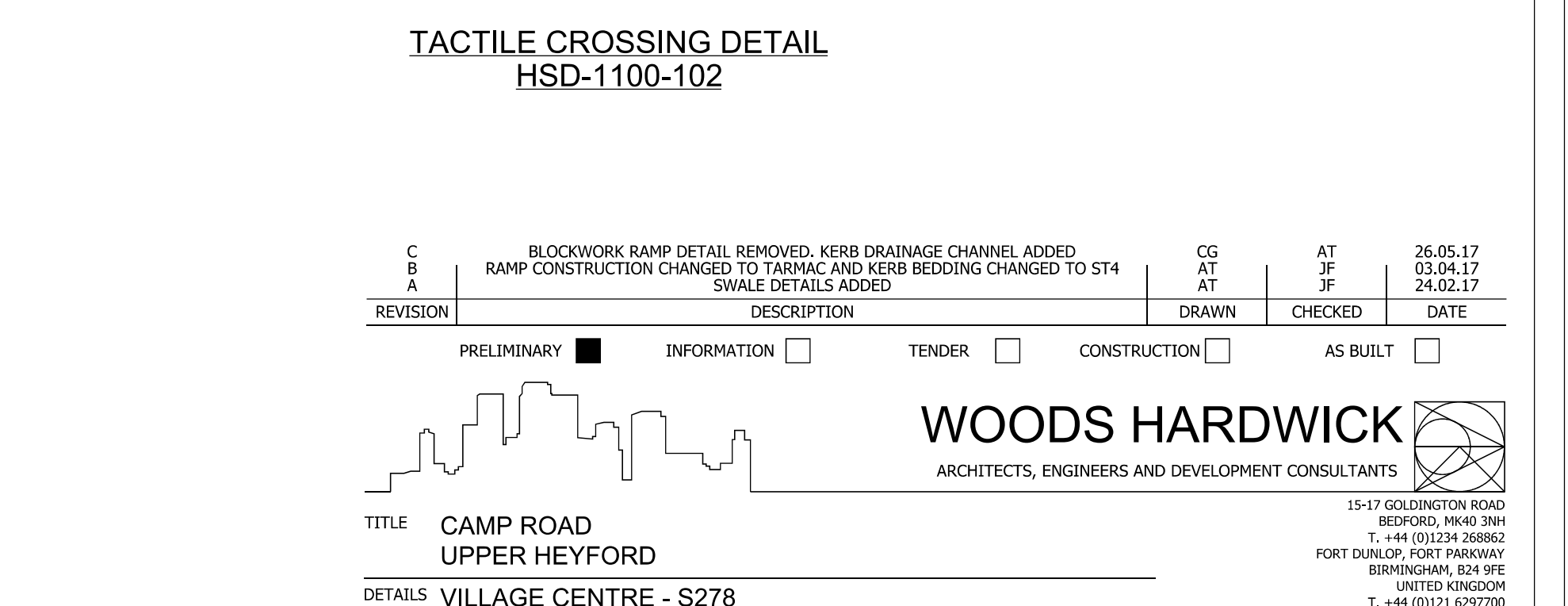
TYPICAL SECTION THROUGH SWALE FEATURE



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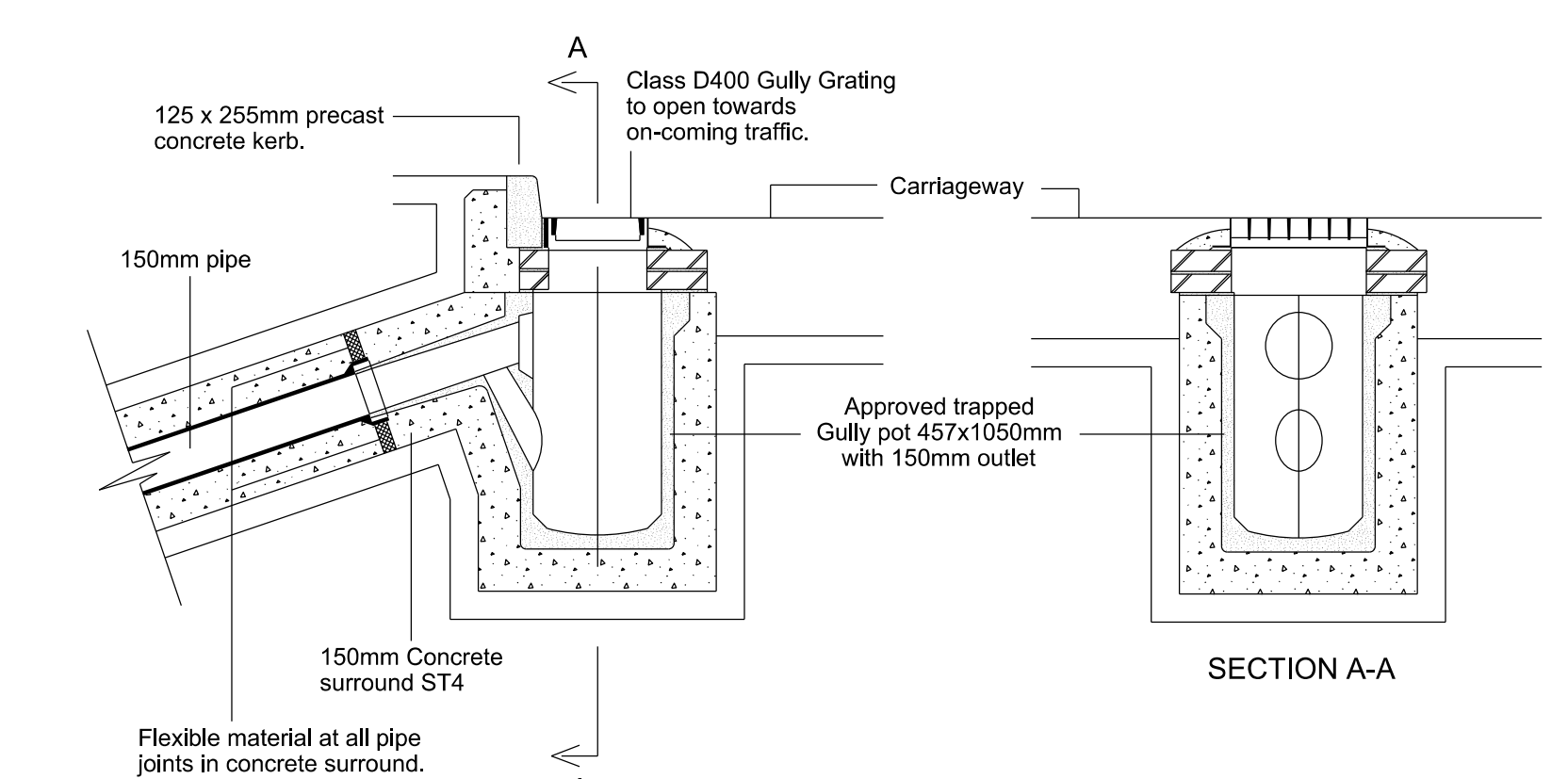
TYPICAL SECTION THROUGH SWALE FEATURE

NOTES

- CONTRACTORS MUST CHECK ALL DIMENSIONS ON SITE. ONLY FIGURED DIMENSIONS ARE TO BE WORKED FROM. DISCREPANCIES MUST BE REPORTED TO THE ARCHITECT OR ENGINEER BEFORE PROCEEDING. © THIS DRAWING IS COPYRIGHT.
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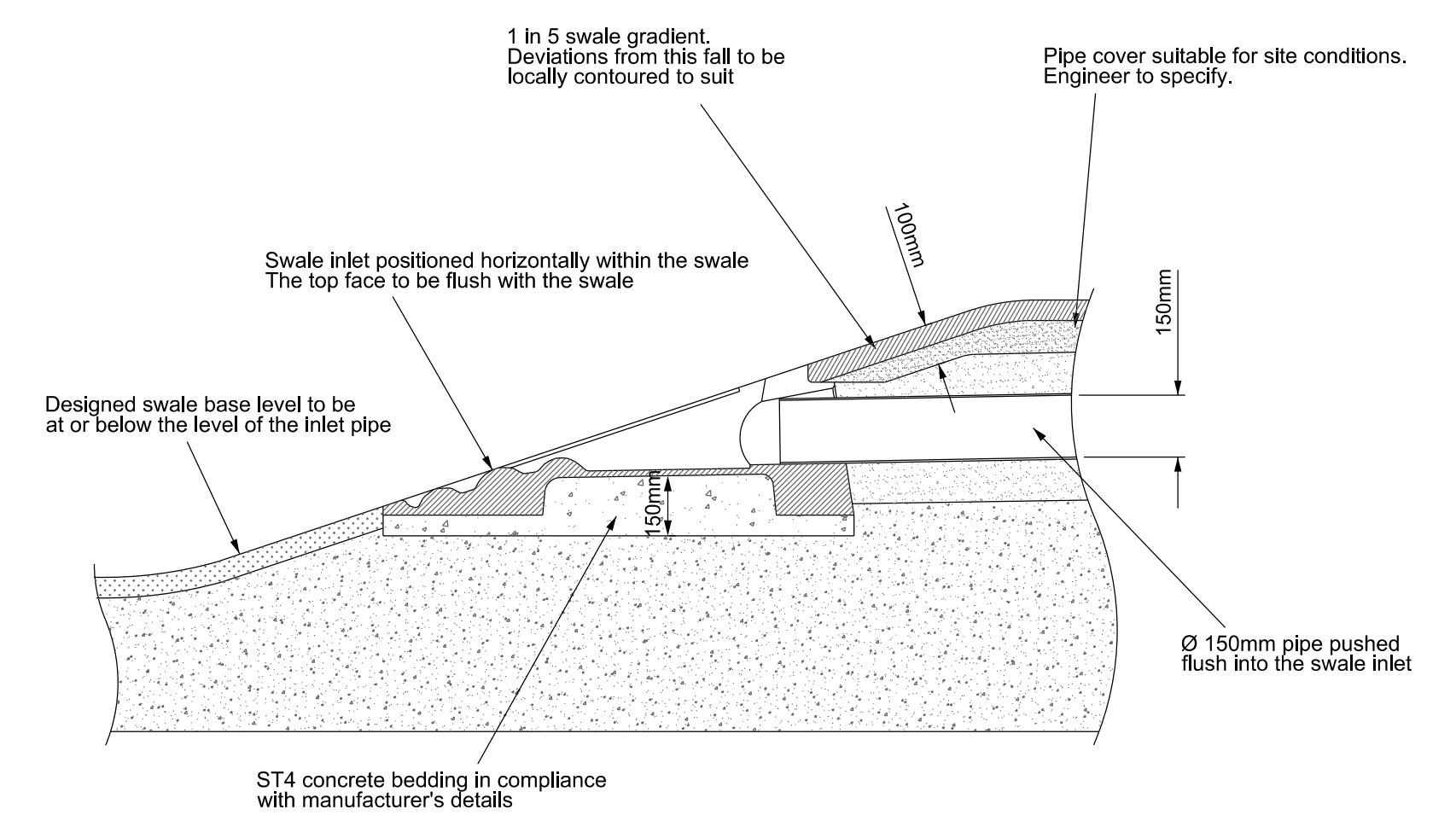
NOTES:
SUBJECT TO DETAILED DESIGN
UNITS: TECHNICAL APPROVAL HAS BEEN OBTAINED FROM THE RELEVANT AUTHORITIES. ALL DRAWINGS ARE ISSUED AS PRELIMINARY AND NOT FOR CONSTRUCTION. SHOULD THE CONTRACTOR COMMENCE SITE WORK PRIOR TO APPROVAL BEING GIVEN IT IS ENTIRELY AT HIS OWN RISK.
TO BE READ IN CONJUNCTION WITH DRAWINGS HEY/5/24 TO 517 INCLUSIVE

GULLY DETAIL



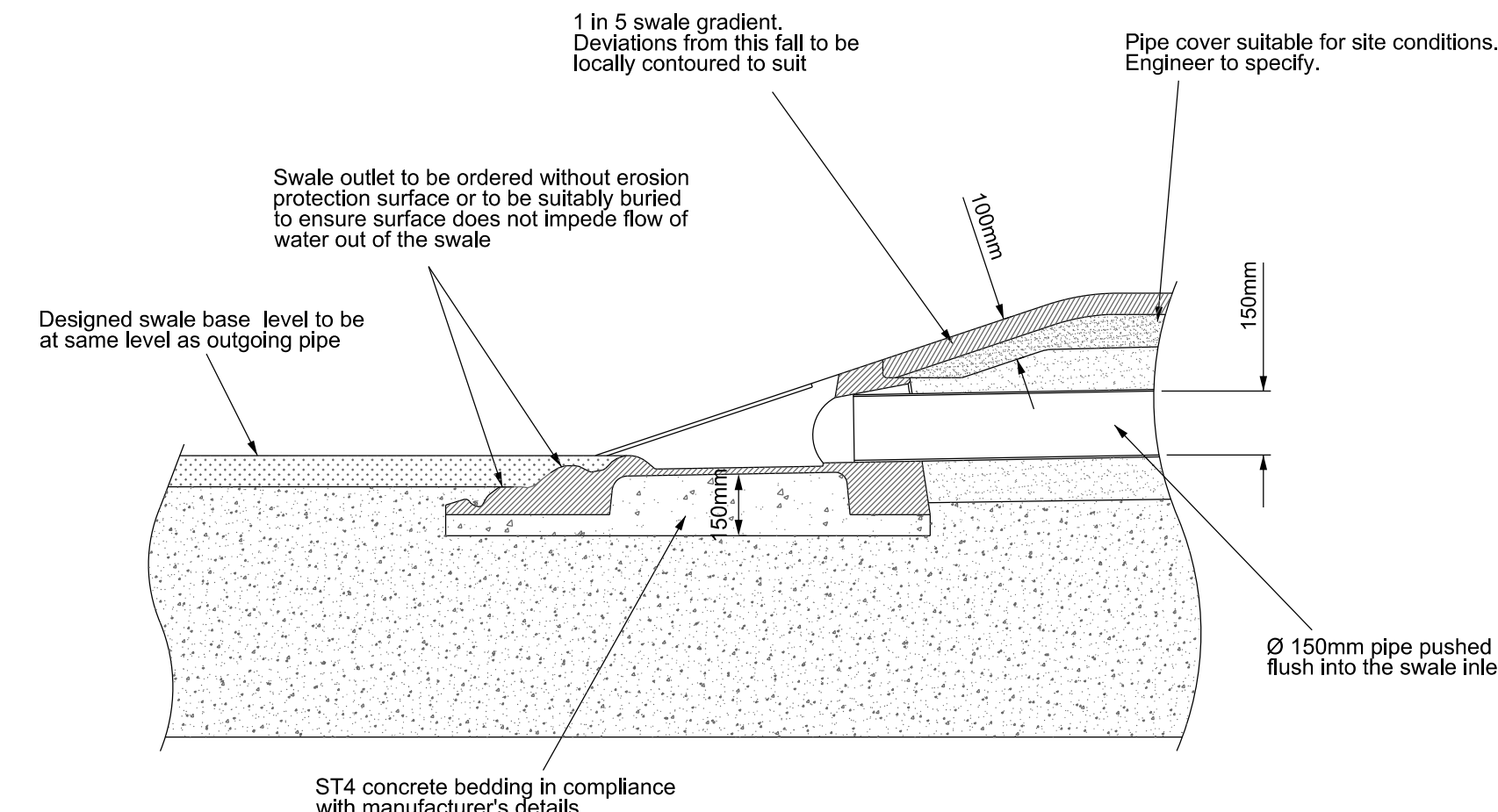
All gully pots to BS 5911 Pt 2. All grates and frames to comply with BSEN124 450mm in width. They shall be straight bar pattern. Gully grating and frames in access ways to be 325mm in width.
The gratings and frames shall be bedded using Designation (i) mortar and set on minimum two courses and a maximum of 4 courses of Class B engineering bricks so that the top of the frames is below the finished channel level within the tolerance of -5mm to +10mm where measured adjacent to the kerfing. The gully frames shall be set to the carriageway gradient. Where the carriageway is constructed to the base course level for use as builders road, the gully frames should initially be set to base course level.
Class D400 gully grating and frame to open towards on-coming vehicles and be single piece, hinged, non-rock type to BS EN 124 (size 370 x 450mm) minimum waterway area 1000sqcm with straight bar pattern.
Gully pots shall be 450mm internal diameter by 1050mm deep with 150mm diameter trapped outlet and rodding eye with C.1 stopper and chain. They shall be of an approved manufacture in precast concrete to BS 5911, self-glazed ware or Clayware to BS 65 and shall be laid with 150mm thickness of ST4 concrete. Connections to the sewer shall be by 150mm diameter clay or concrete pipes bedded on 150mm concrete and surrounded with ST4 concrete to a thickness of 150mm when the cover to the pipes is less than 1.2m.
Gullies should preferably connect directly into manholes, but, if this is not appropriate, they shall be connected to the main pipe by means of 45 degree oblique angled junctions, and surrounded by ST4 concrete to a thickness of 150mm. Gully connections shall not be longer than 20m.

ACO HEADWALL INLET TO SWALE



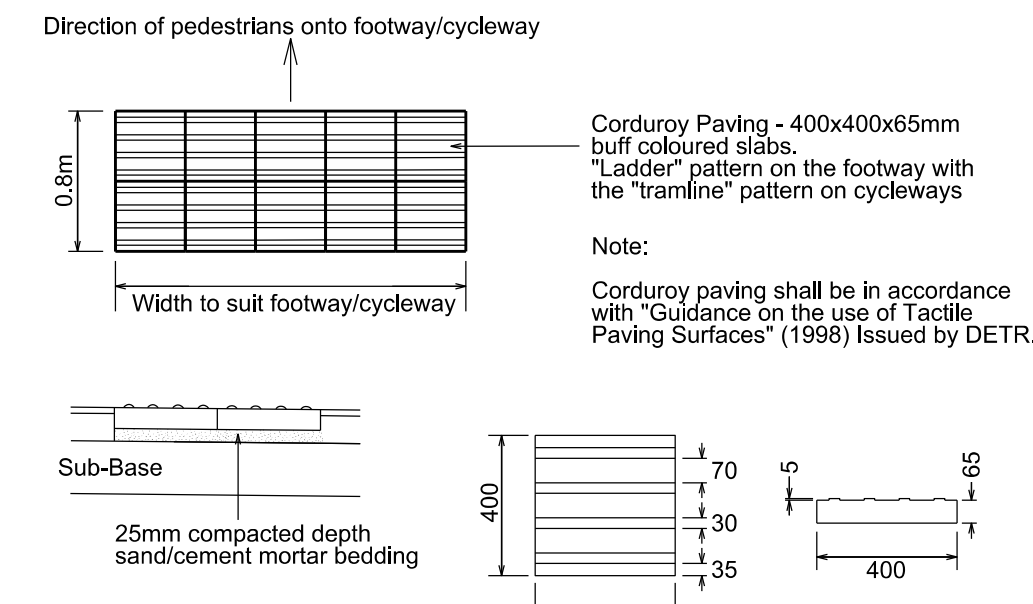
ACO HEADWALL INLET TO SWALE

ACO HEADWALL OUTLET FROM SWALE



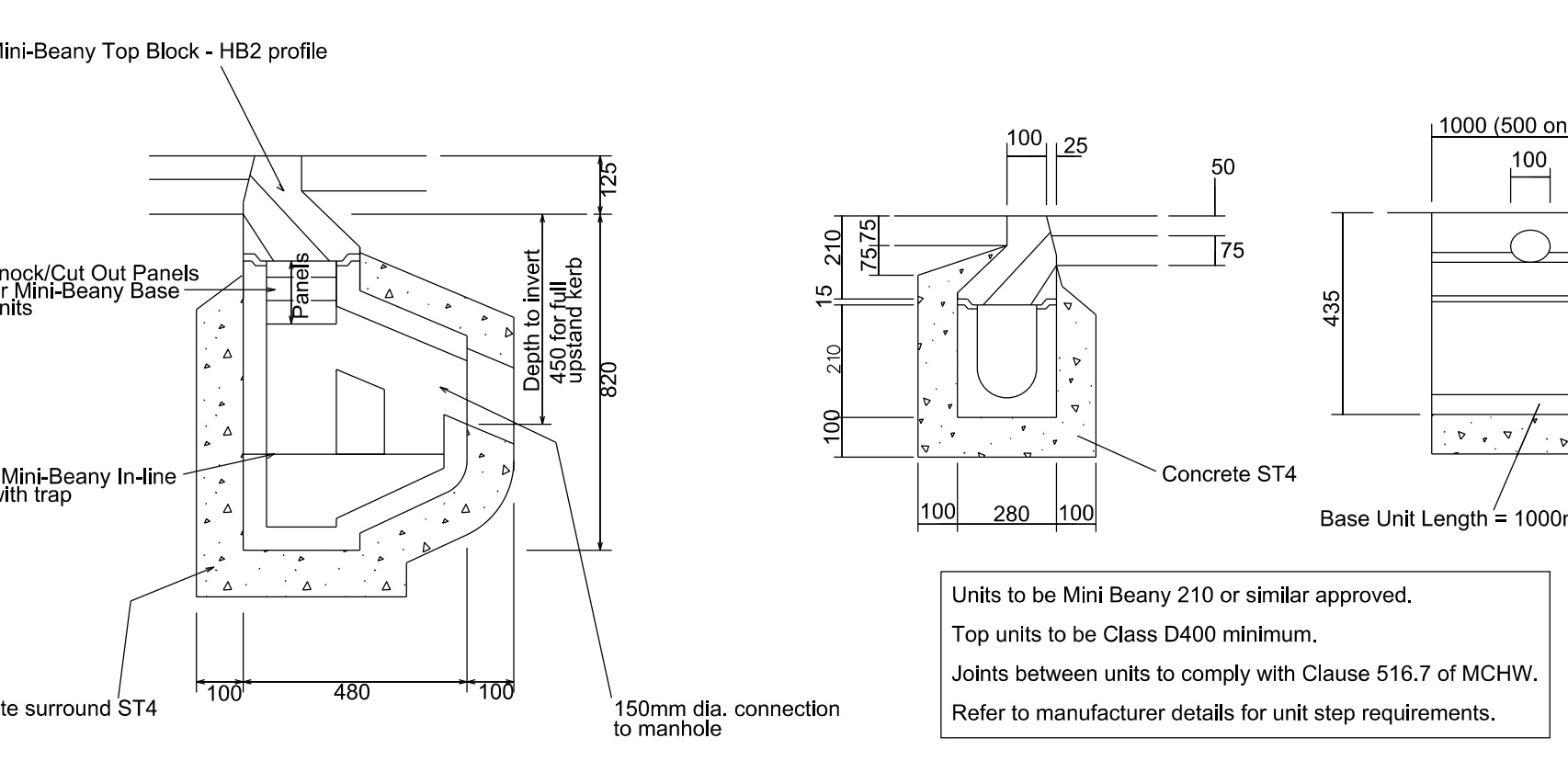
ACO HEADWALL OUTLET FROM SWALE

CORDOURY DETAIL



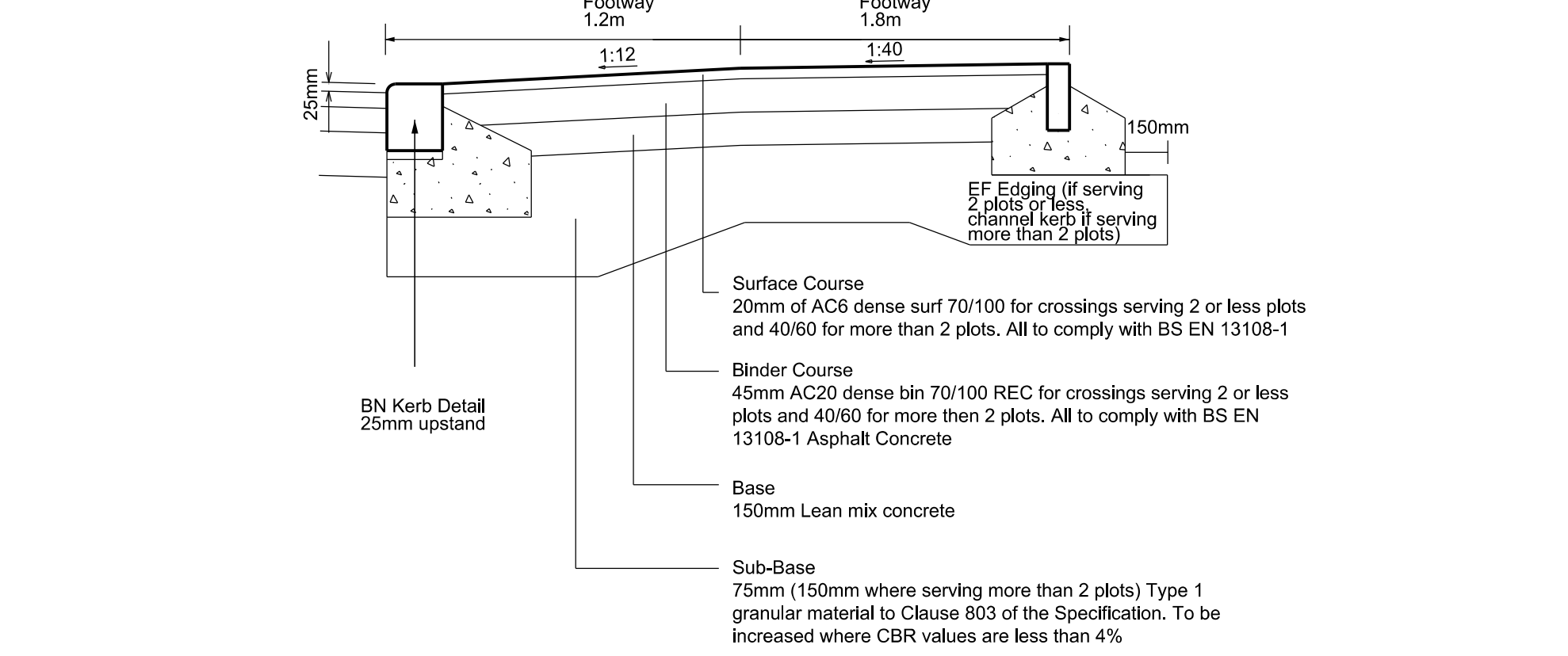
CORDOURY DETAIL

LINEAR KERB DRAINAGE CHANNEL



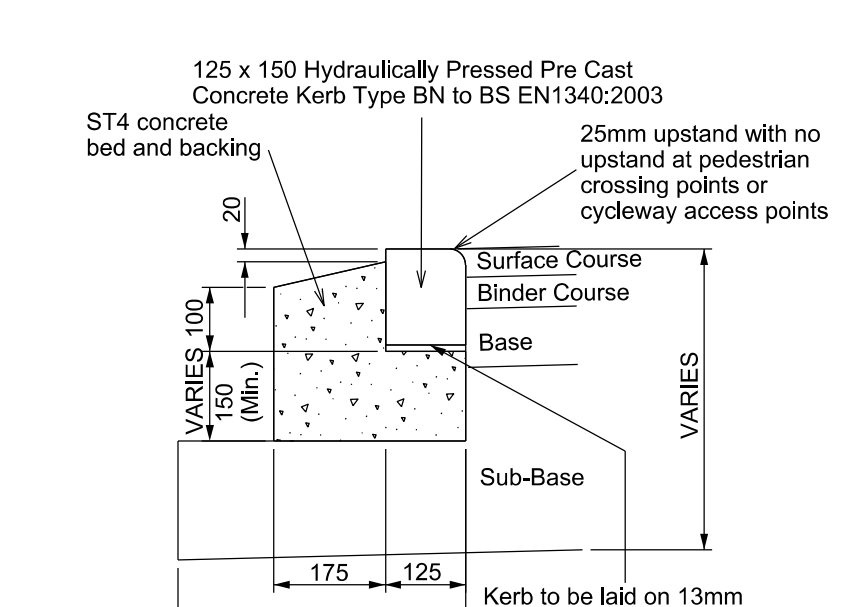
LINEAR KERB DRAINAGE CHANNEL

CAMP ROAD VEHICULAR CROSSING DETAIL WITHOUT VERGE



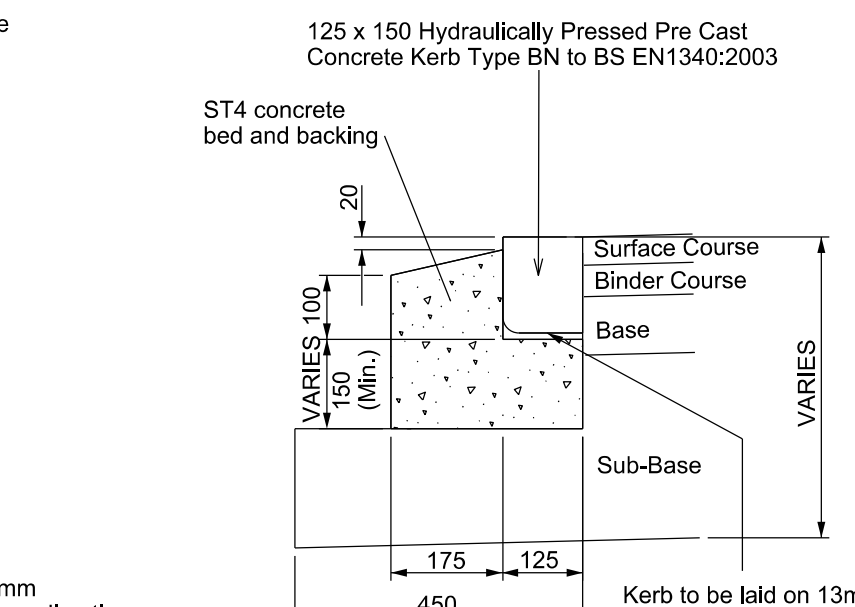
CAMP ROAD VEHICULAR CROSSING DETAIL WITHOUT VERGE

BULL- NOSED KERB (BN) HSD-1100-160



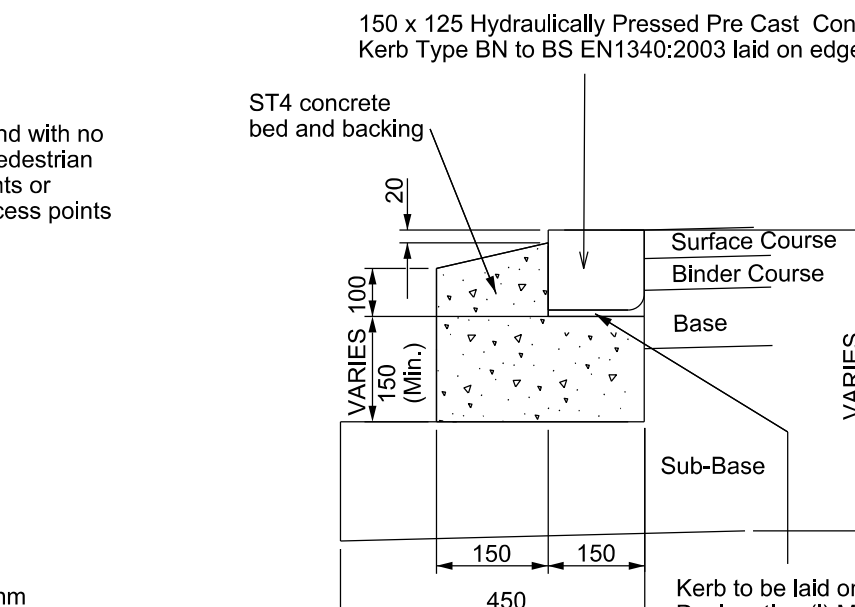
BULL- NOSED KERB (BN) HSD-1100-160

INVERTED BULL- NOSED KERB (BN)



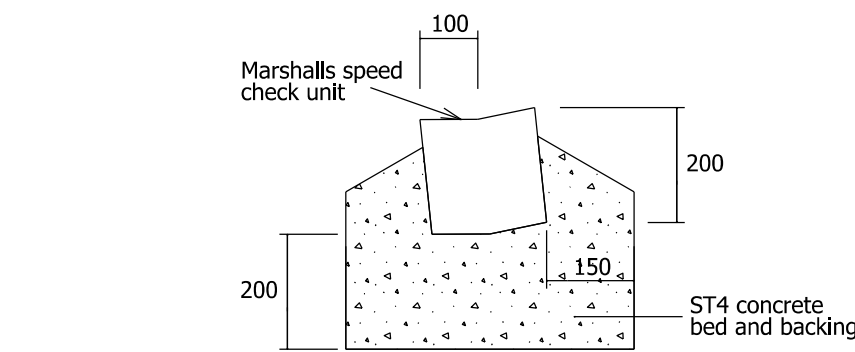
INVERTED BULL- NOSED KERB (BN)

BULL- NOSED KERB (BN) LAID ON EDGE



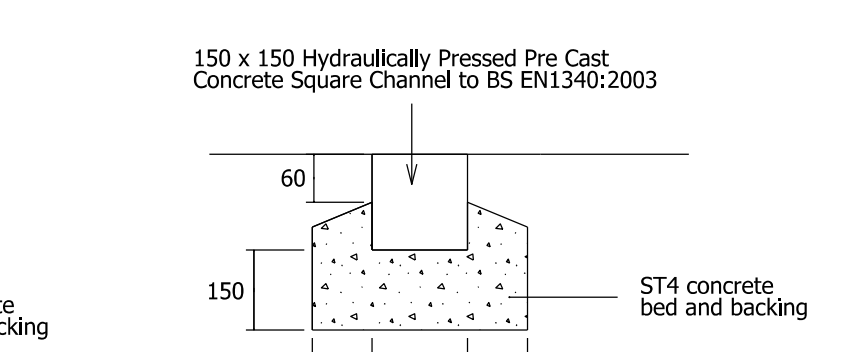
BULL- NOSED KERB (BN) LAID ON EDGE

MARSHALLS SPEED CHECK UNIT



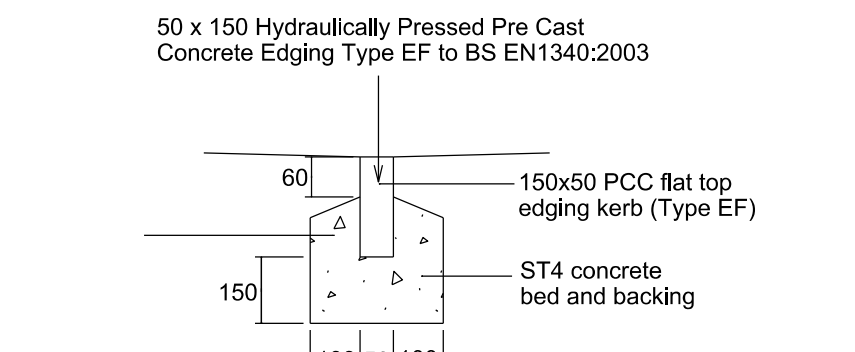
MARSHALLS SPEED CHECK UNIT

CHANNEL KERB



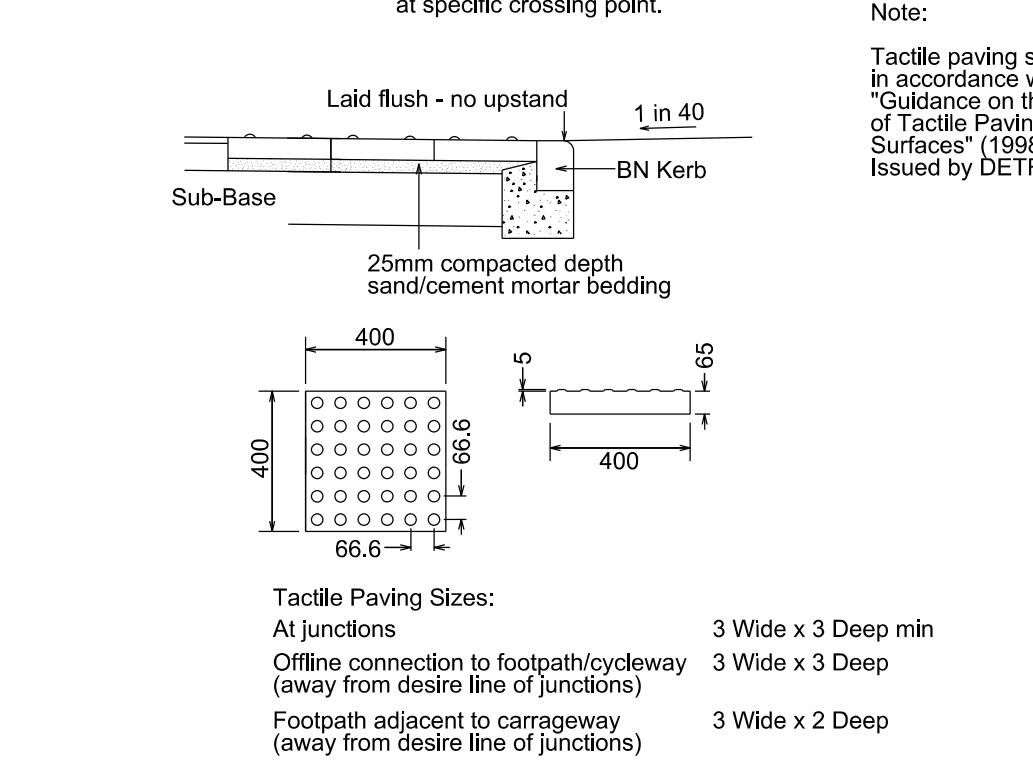
CHANNEL KERB

EDGING (EF) HSD-1100-030



EDGING (EF) HSD-1100-030

TACTILE CROSSING DETAIL HSD-1100-102



TACTILE CROSSING DETAIL HSD-1100-102

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