



Figure. 106- E-facing section through linear F.6604 (Scale- 1 x 1m)



Figure. 107- E-facing section through linear F.6704 (Scale- 1 x 1m)



Figure. 108- W-facing view of Trench 67 (Scale- 2 x 2m)



Figure. 109- E-facing section through furrow F.6704 (Scale 1 x 0.2m)



Figure. 110- S-facing view of linear F.6804 (Scale- 1 x 1m)



Figure. 111- W-facing view of Trench 68 (Scale- 2 x 2m)



Figure. 112- N-facing section through linear F.6806 (Scale- 1 x 1m)

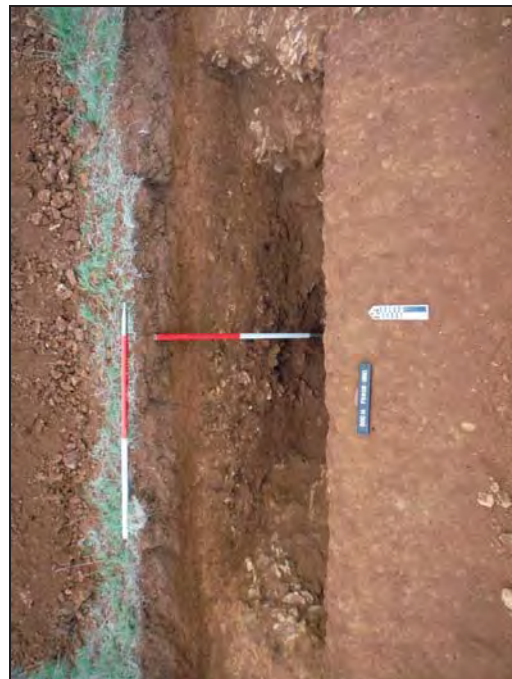


Figure. 113- S-facing section through enclosure ditches F.6808 and F.6814 (Scale- 2 x 1m)



Figure. 114- N-facing view of linear F.6812 (Scale- 1 x 1m)



Figure. 115- E-facing oblique section through pit F.7005 (Scale- 2 x 1m)



Figure. 116- N-facing view of Trench 70 (Scale- 2 x 2m)



Figure. 117- W-facing view of Trench 75 (Scale- 2 x 2m)



Figure. 118- N-facing section through linear F.7504 (Scale- 1 x 1m)

Archaeologically Sterile Trenches



Figure. 119- W-facing view of Trench 2 (Scale- 2 x 2m)

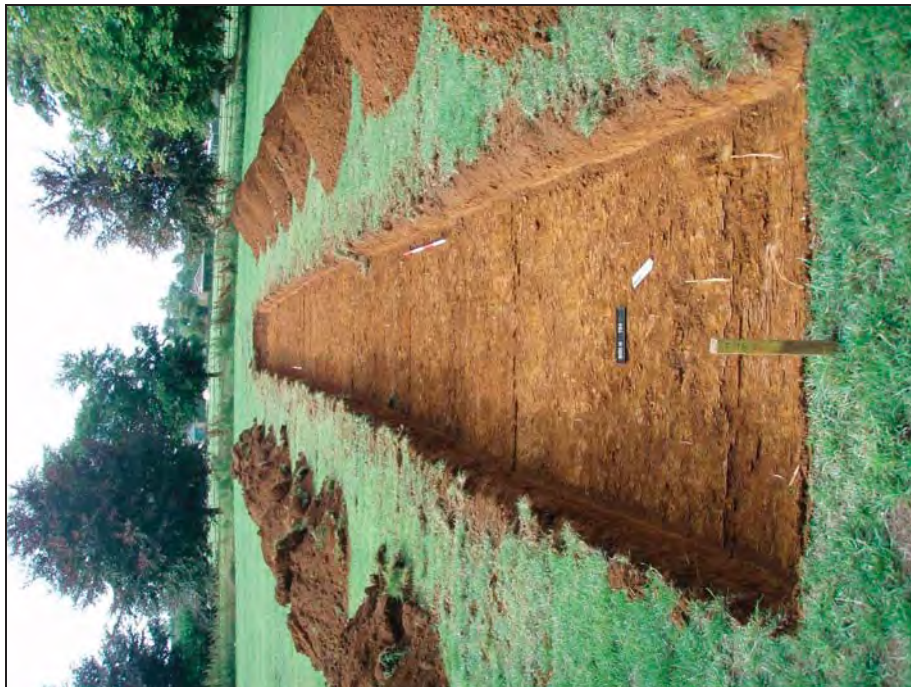


Figure .120- SW-facing view of Trench 4 (Scale- 2 x 2m)



Figure .121- WNW-facing view of Trench 5 – Trench shortened to avoid damage to nearby trees (Scale- 2 x 2m)



Figure .122- W-facing view of Trench 7. Note unexcavated foul pipe bisecting eastern extent of Trench 7 (Scale- 2 x 2m)



Figure .123- SW-facing view of Trench 9 (Scale- 2 x 2m)



Figure .124- W-facing view of Trench 10 (Scale- 2 x 2m)



Figure. 125- SSW-facing view of Trench 12 (Scale- 2 x 2m)





Figure .126- SSE-facing view of Trench 14 (Scale- 2 x 2m)



Figure .127- NE-facing view of Trench 15 (Scale- 2 x2m)



Figure 128- NW-facing view of Trench 17 (scale- 2 x 2m)



Figure 129- N-facing view of Trench 21 (Scale- 2 x 2m)



Figure. 130- NE-facing view of Trench 24 (Scale- 2 x 2m)



Figure. 131- ENE-facing view of Trench 26 (Scale- 2 x 2m)



Figure 132- NE-facing view of Trench 27 (Scale- 2 x 2m)



Figure 133- WNW-facing view of Trench 28 (Scale- 2 x 2m)



Figure. 134- SW-facing view of Trench 32 (Scale- 2 x 2m)



Figure. 135- N-facing view of Trench 33 (Scale- 2 x 2m)



Figure. 136- NE-facing view of Trench 40 (Scale- 2 x 2m)



Figure. 137- WNW-facing view of Trench 41 (Scale 2 x 2m)



Figure .138- SW-facing view of Trench 42 (Scale- 2 x 2m)



Figure .139-SW-facing view of Trench 44 (Scale- 2 x 2m)



Figure. 140- E-facing view of Trench 45 (Scale- 2 x 2m)



Figure. 141- E-facing view of Trench 46 (Scale- 2 x 2m)





Figure. 142-SW-facing view of Trench 49 (Scale- 2 x 2m)



Figure. 143 - E-facing view of Trench 55 (Scale- 2 x 2m)



Figure. 144 - E-facing view of Trench 56 (Scale- 2 x 2m)



Figure. 145- E-facing view of Trench 62 (Scale- 2 x 2m)



Figure. 146- E-facing view of Trench 69 (Scale- 2 x 2m)



Figure. 147- E-facing view of Trench 72- (Scale- 2 x 2m)



Figure. 148- S-facing view of Trench 63 (Scale- 2 x 2m)



Figure. 149- E-facing view of Trench 73 (Scale- 2 x 2m)



Figure .150- NE-facing view of Trench 74 (Scale- 2 x 2m)



Figure .151- W-facing view of Trench 75 (Scale- 2 x 2m)



Figure. 152- W-facing view of Trench 76 (Scale- 2 x 2m)



Figure. 153- SSW-facing view of Trench 77 (Scale- 2 x 2m)



Figure. 154- S-facing view of Trench 79 (Scale- 2 x 2m)



Figure. 155- S-facing view of Trench 80 (Scale- 2 x 2m)

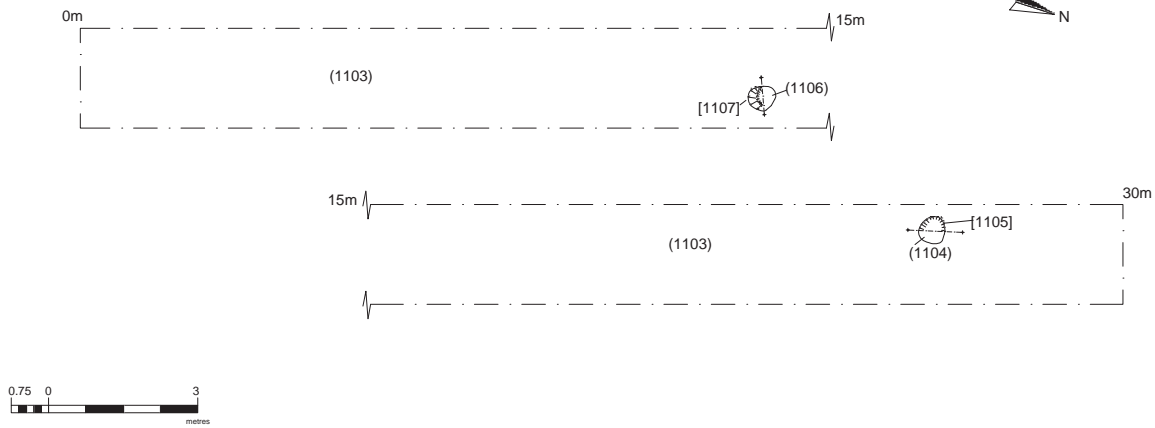
Figure 158 - Plan of Trench 8



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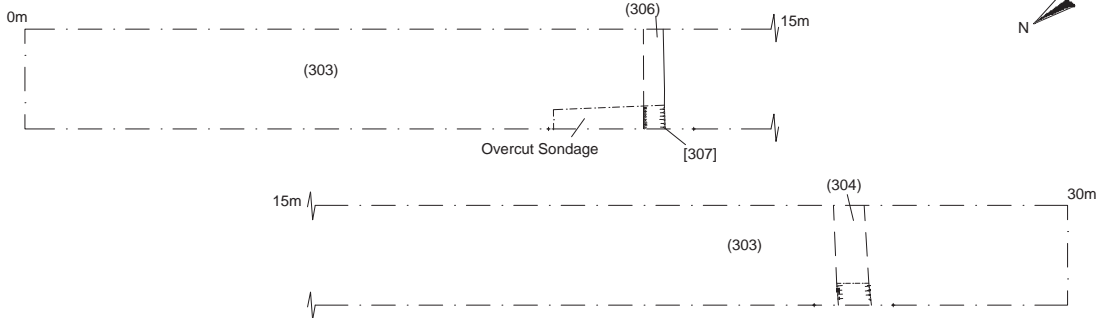
Figure 159 - Plan of Trench 11



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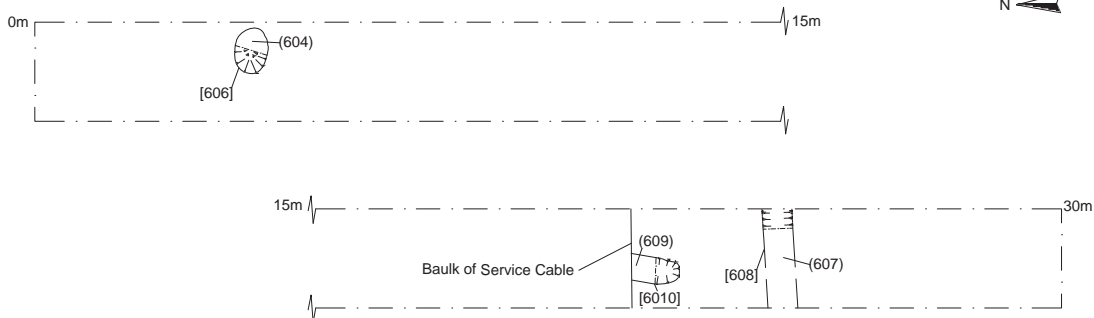
Figure 156 - Plan of Trench 3



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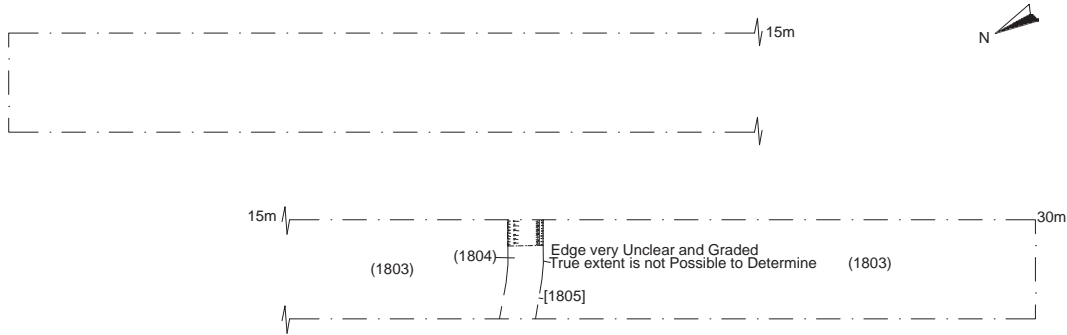
Figure 157 - Plan of Trench 6



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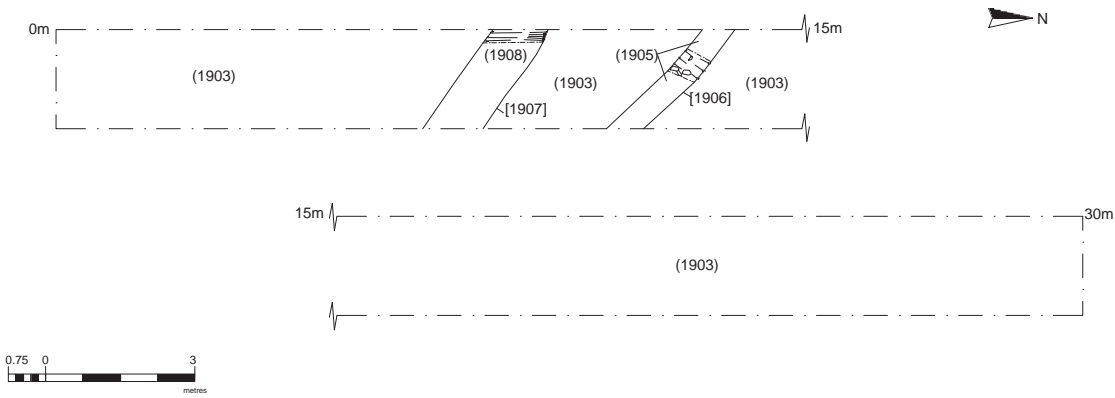
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Figure 162 - Plan of Trench 18



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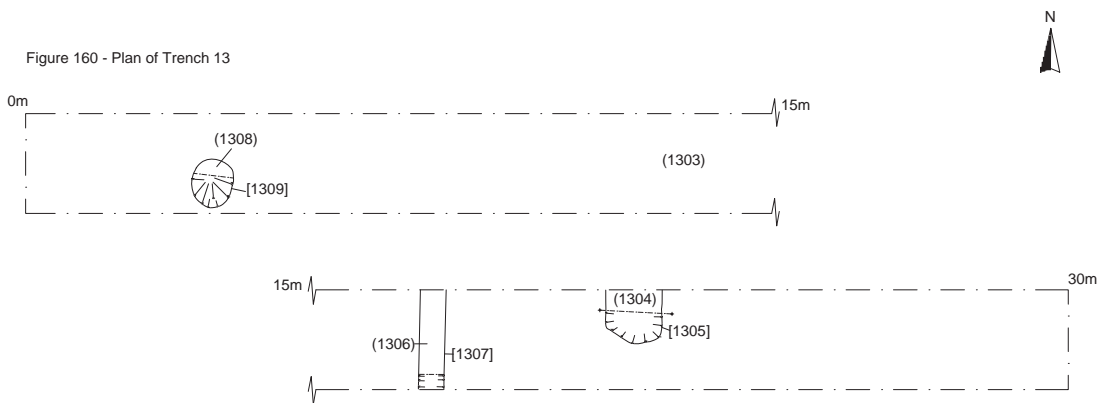
Figure 163 - Plan of Trench 19



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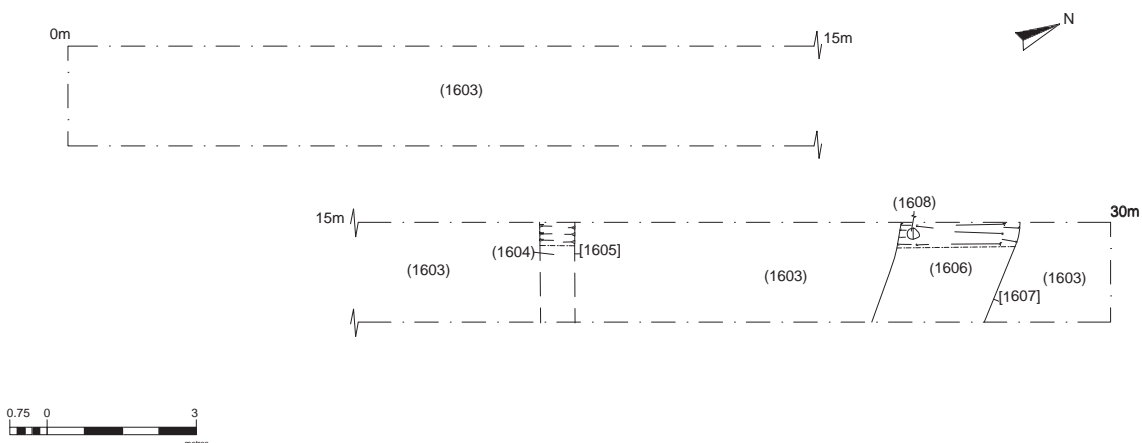
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Figure 160 - Plan of Trench 13



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Figure 161 - Plan of Trench 16

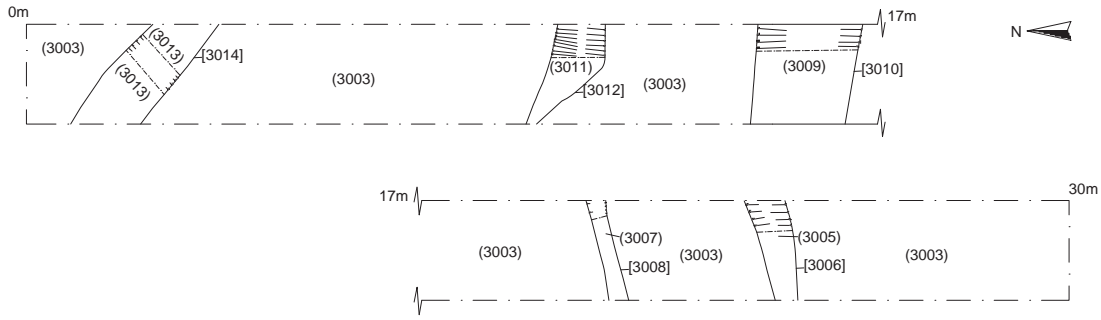


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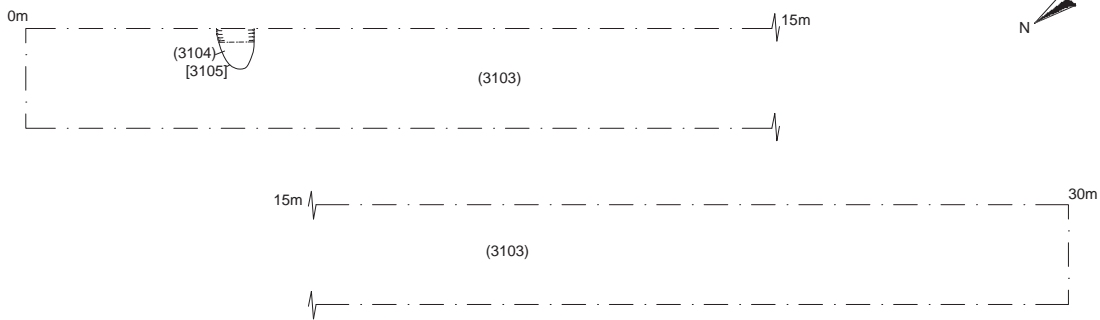


Figure 166 - Plan of Trench 30



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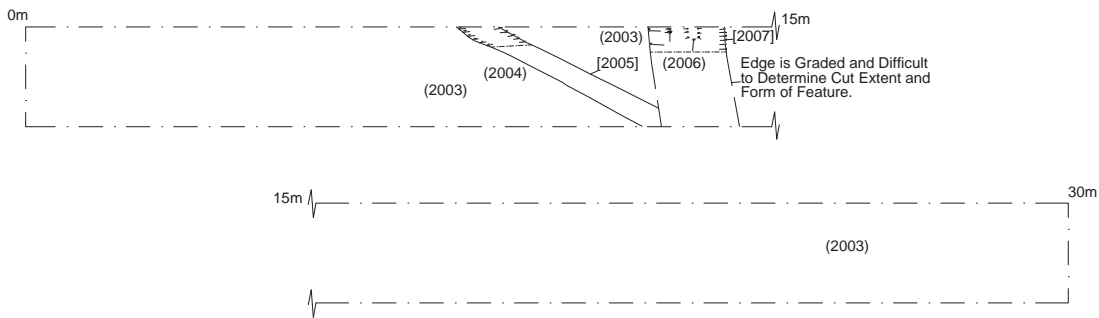
Figure 167 - Plan of Trench 31



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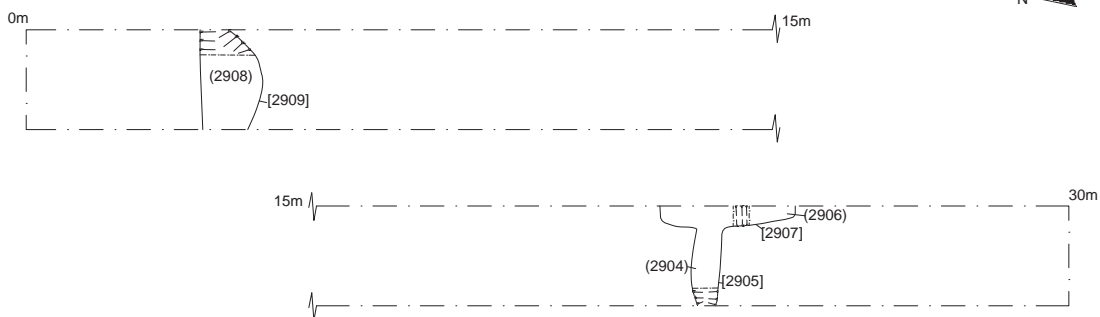
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Figure 164 - Plan of Trench 20



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Figure 165 - Plan of Trench 29



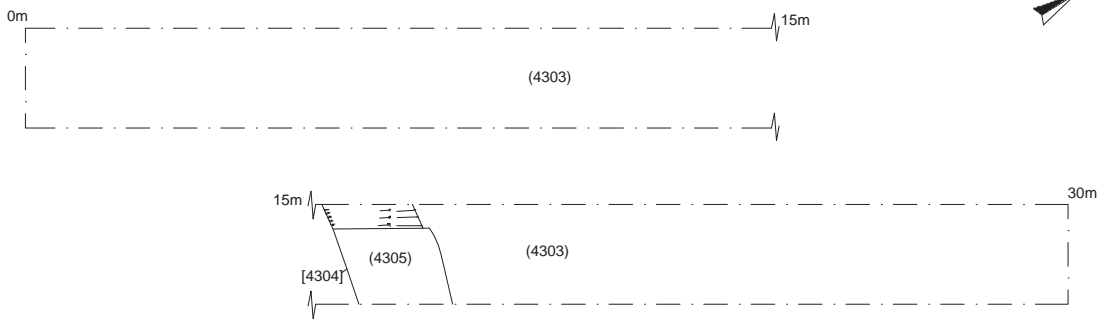
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Figure 170 - Plan of Trench 39



Figure 171- Plan of Trench 43



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Figure 168 - Plan of Trench 37

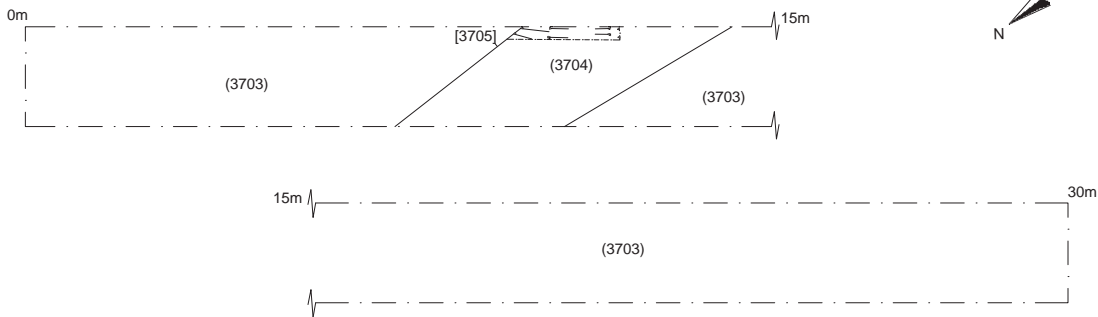


Figure 169 - Plan of Trench 38



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Figure 174 - Plan of Trench 50

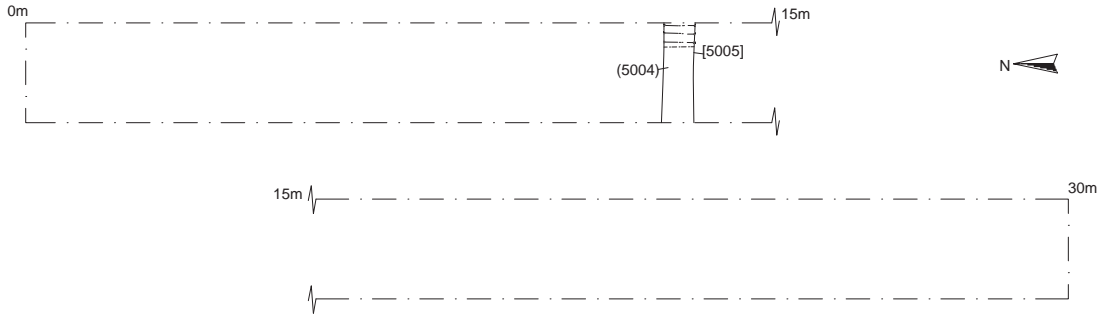
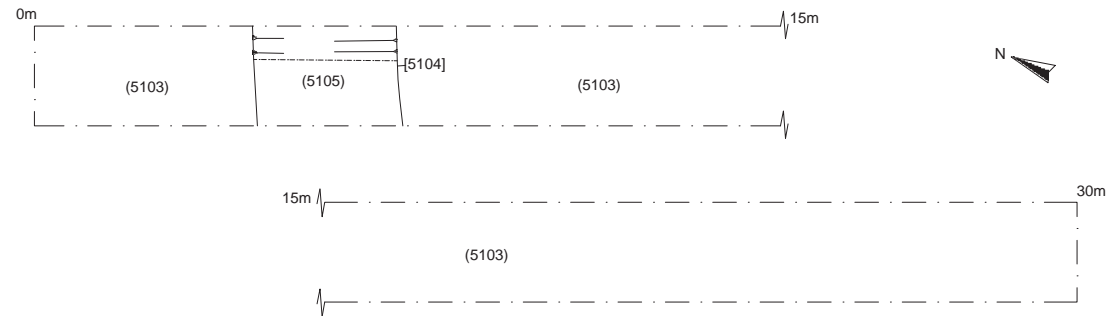


Figure 175 - Plan of Trench 51



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Figure 172 - Plan of Trench 47

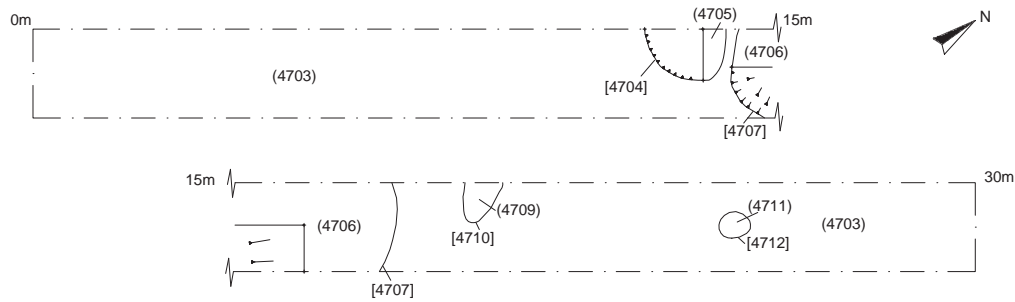


Figure 173 - Plan of Trench 48

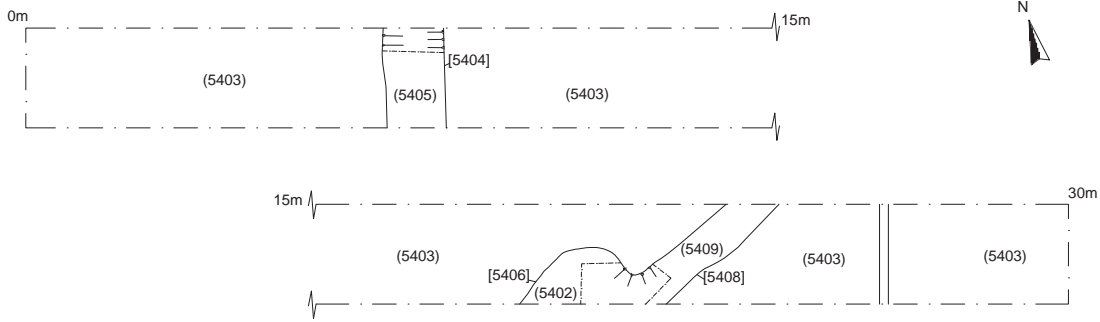


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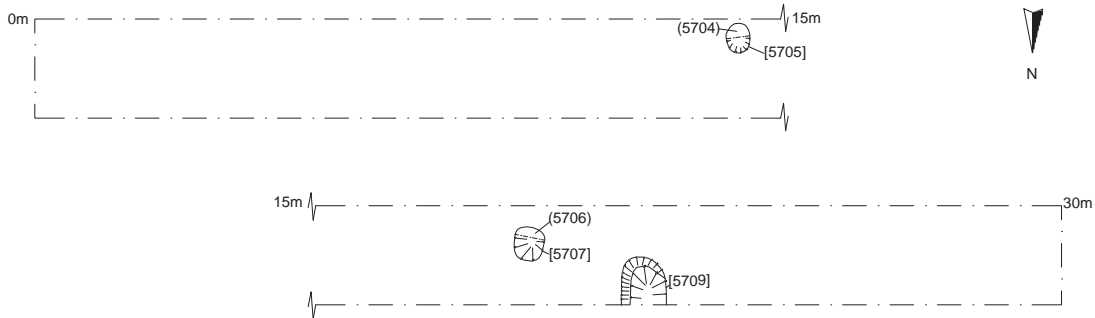
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Figure 178 - Plan of Trench 54



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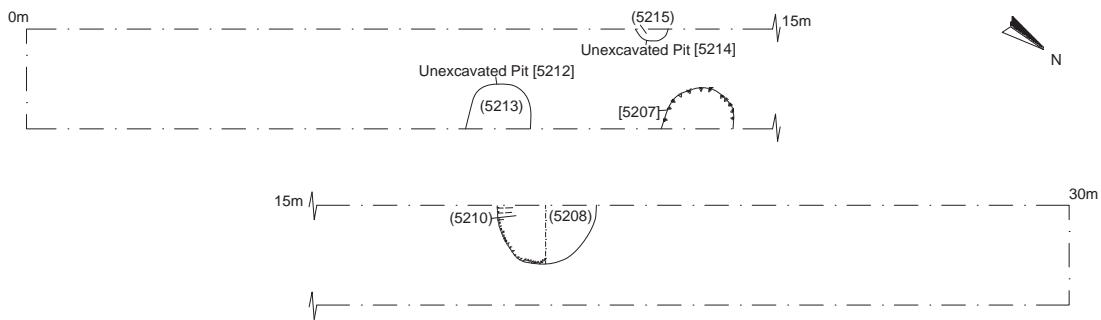
Figure 179 - Plan of Trench 57



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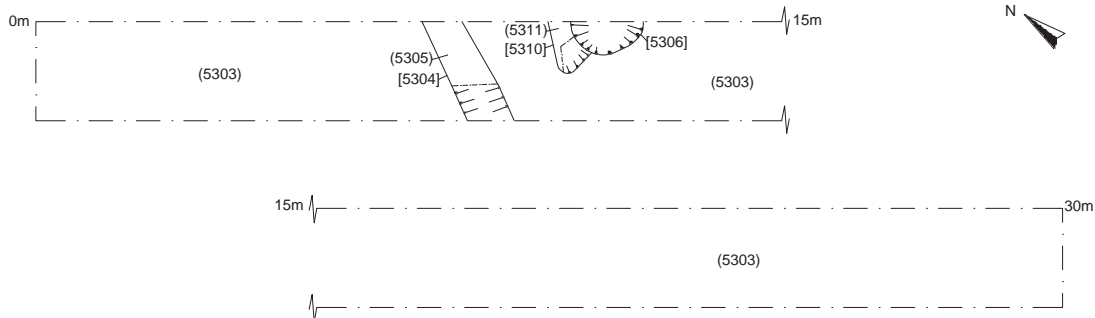
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Figure 176 - Plan of Trench 52



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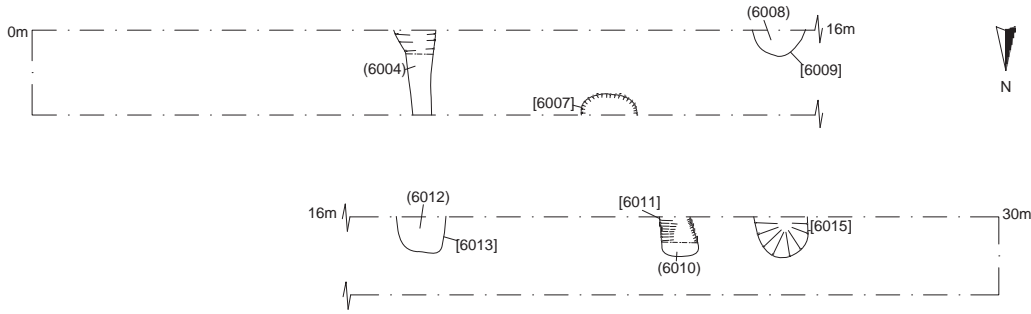
Figure 177 - Plan of Trench 53



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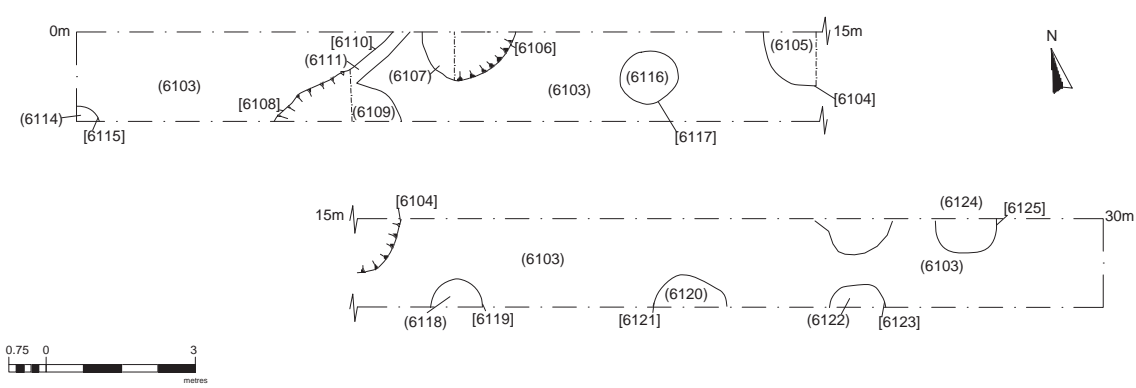
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Figure 182 - Plan of Trench 60



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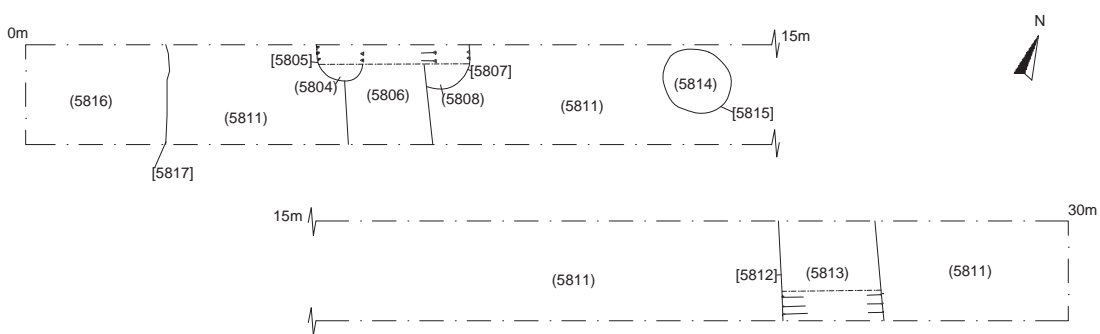
Figure 183 - Plan of Trench 61



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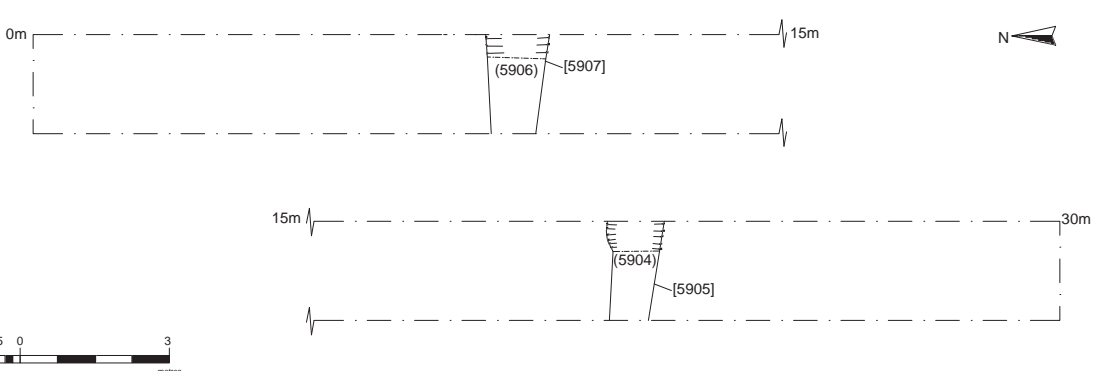
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Figure 180 - Plan of Trench 58



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Figure 181 - Plan of Trench 59



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Figure 186 - Plan of Trench 66

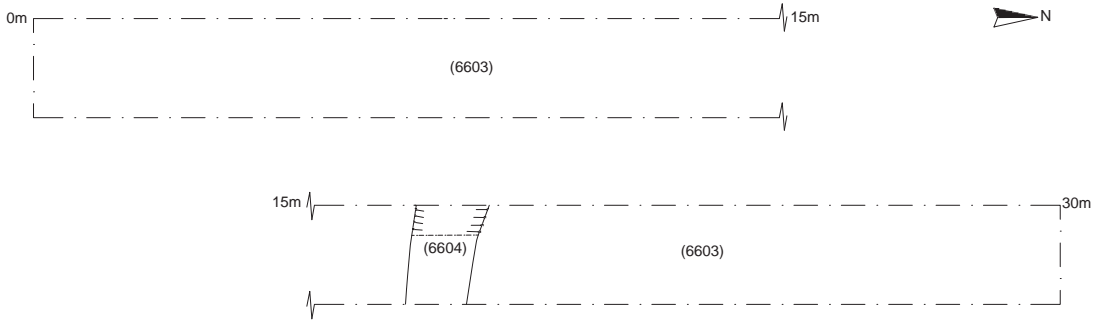


Figure 187 - Plan of Trench 67

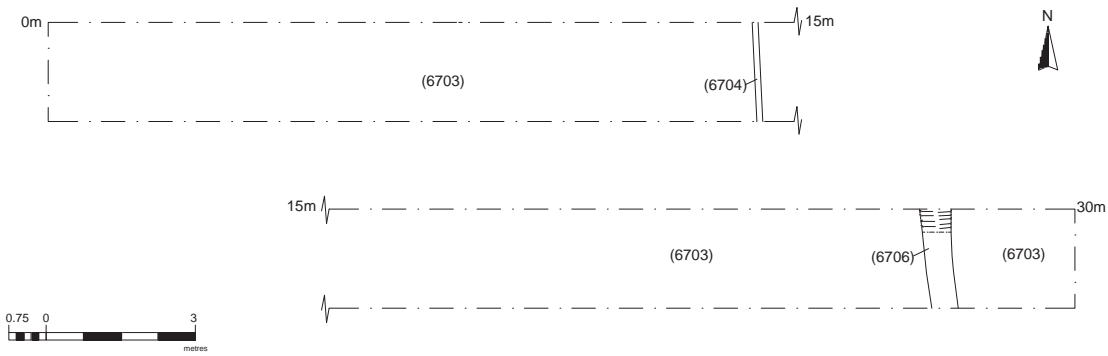


Figure 184 - Plan of Trench 64

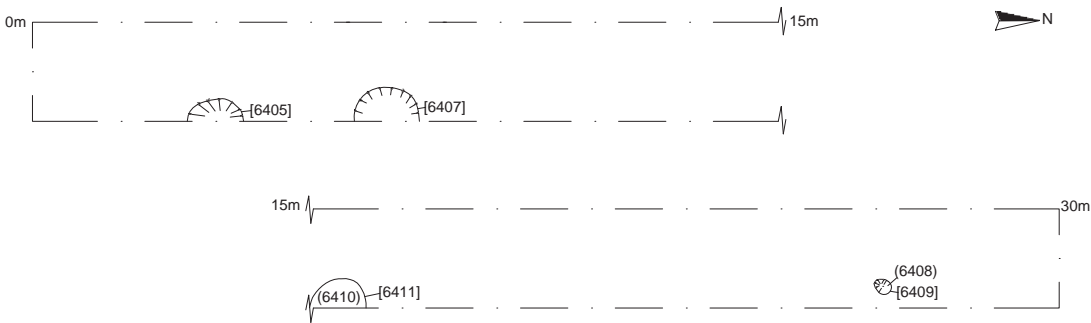
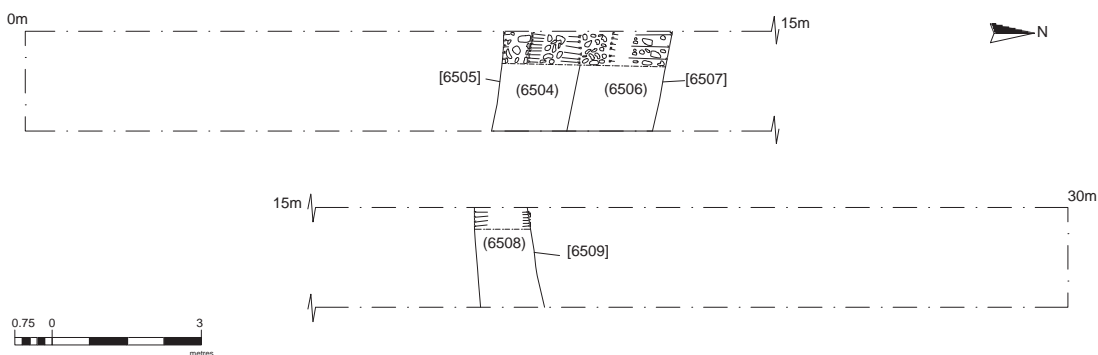


Figure 185 - Plan of Trench 65

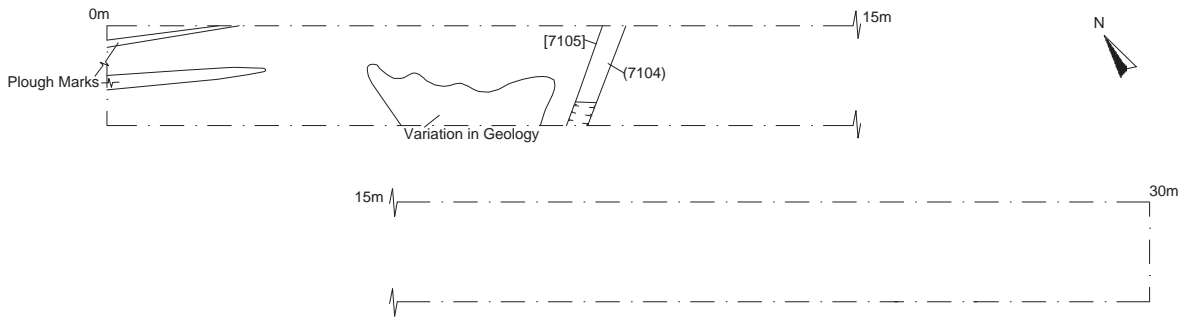


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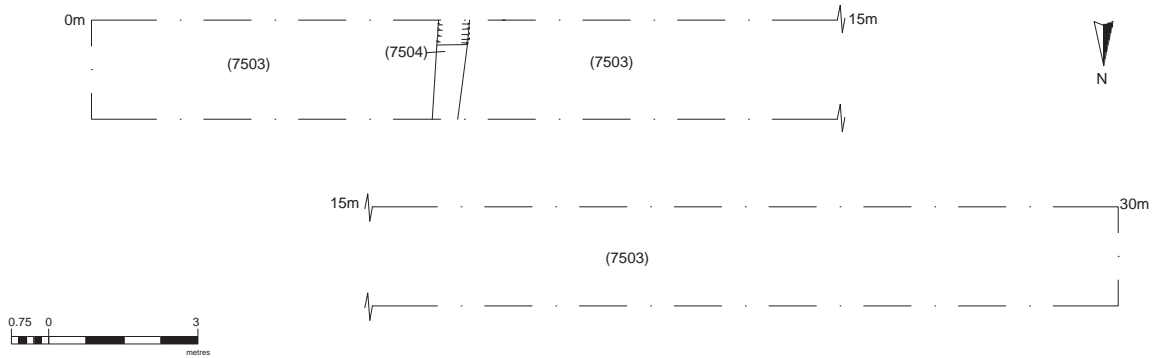
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Figure 190 - Plan of Trench 71



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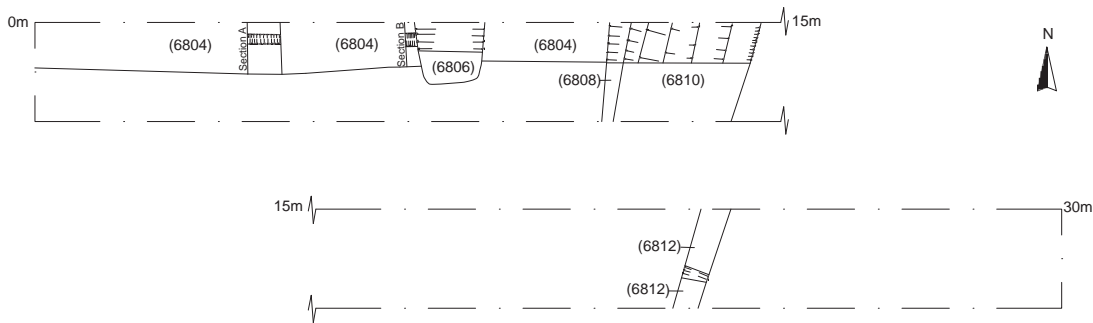
Figure 191 - Plan of Trench 75



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Figure 188 - Plan of Trench 68

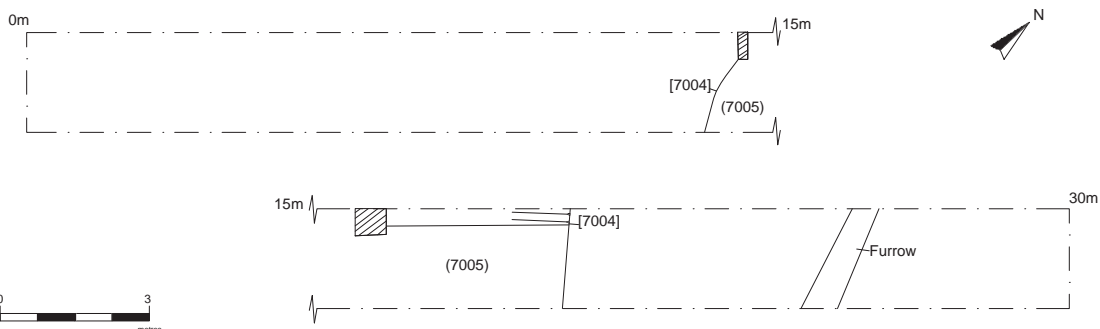


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Figure 189 - Plan of Trench 70



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Figure 197 - E facing section of F.804

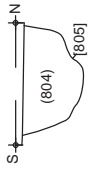


Figure 198 - NW facing section of F.1104



Figure 199 - SW facing section of F.1106



Figure 192 - SE facing section of F.304

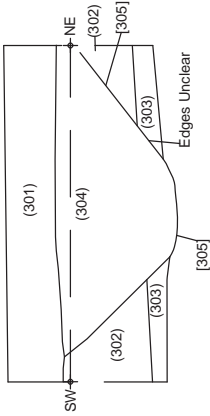


Figure 193 - SE facing section of F.306

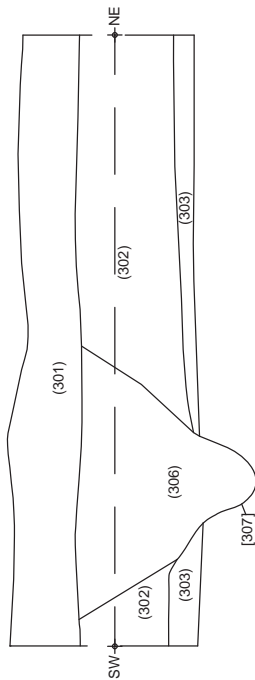


Figure 194 - W facing section of F.604



Figure 195 - E facing section of F.607

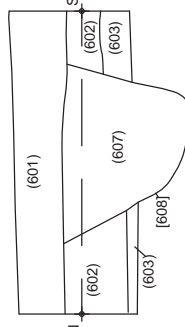


Figure 196 - S facing section of F.609



Figure 200 - S facing section of F.1304



Figure 201 - N facing section of F.1306

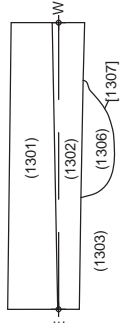


Figure 202 - S facing section of F.1308

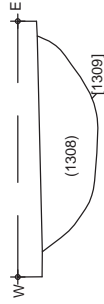


Figure 203 - NNE facing section of F.1604

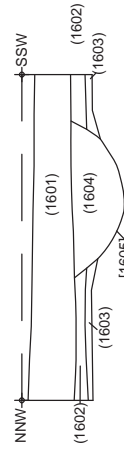


Figure 204 - NE facing section of F.1608

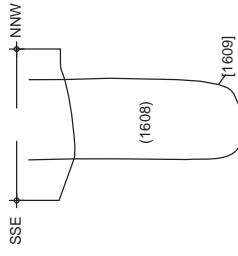
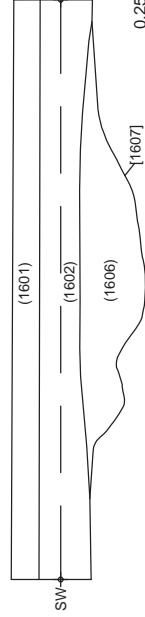


Figure 205 - SE facing section of F.1606



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Figure 206 - NW facing section of F.1804

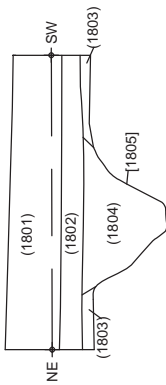


Figure 207 - SE facing section of F.1905



Figure 208 - E facing section of F.1908

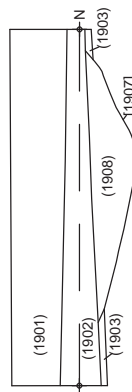


Figure 209 - SE facing section of F.2004

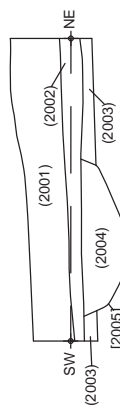
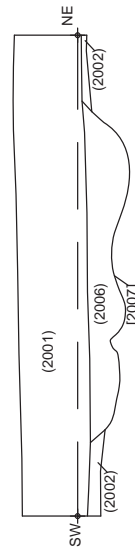


Figure 210 - SE facing section of F.2006



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Figure 211 - E facing section of F.2904

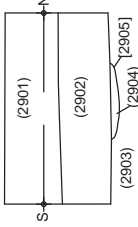
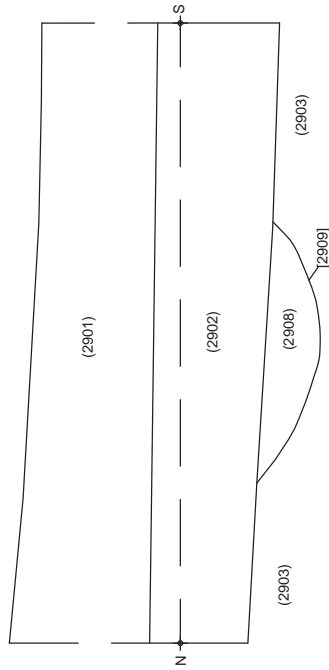


Figure 212 - S facing section of F.2906



Figure 213 - W facing section of F.2908

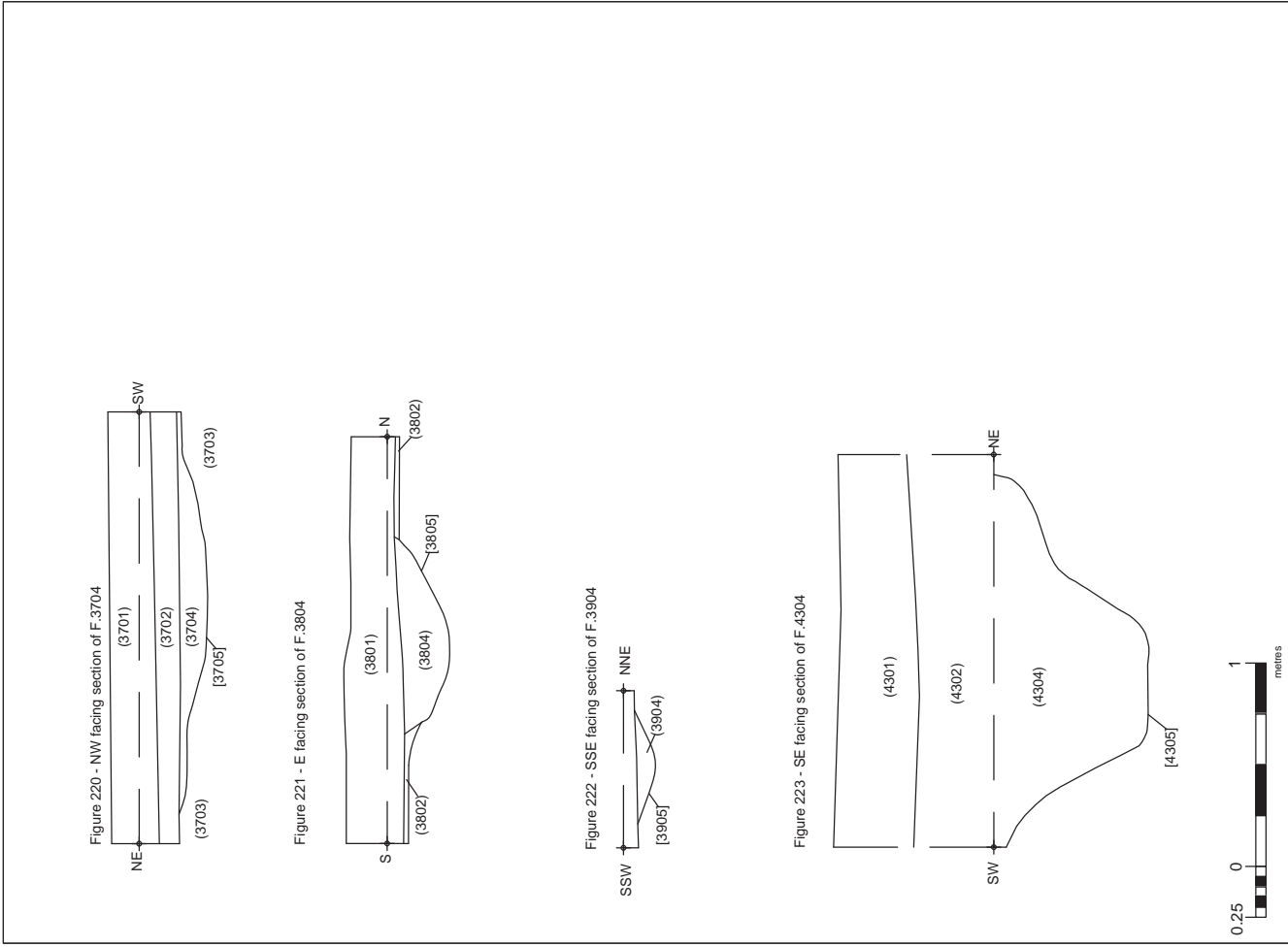


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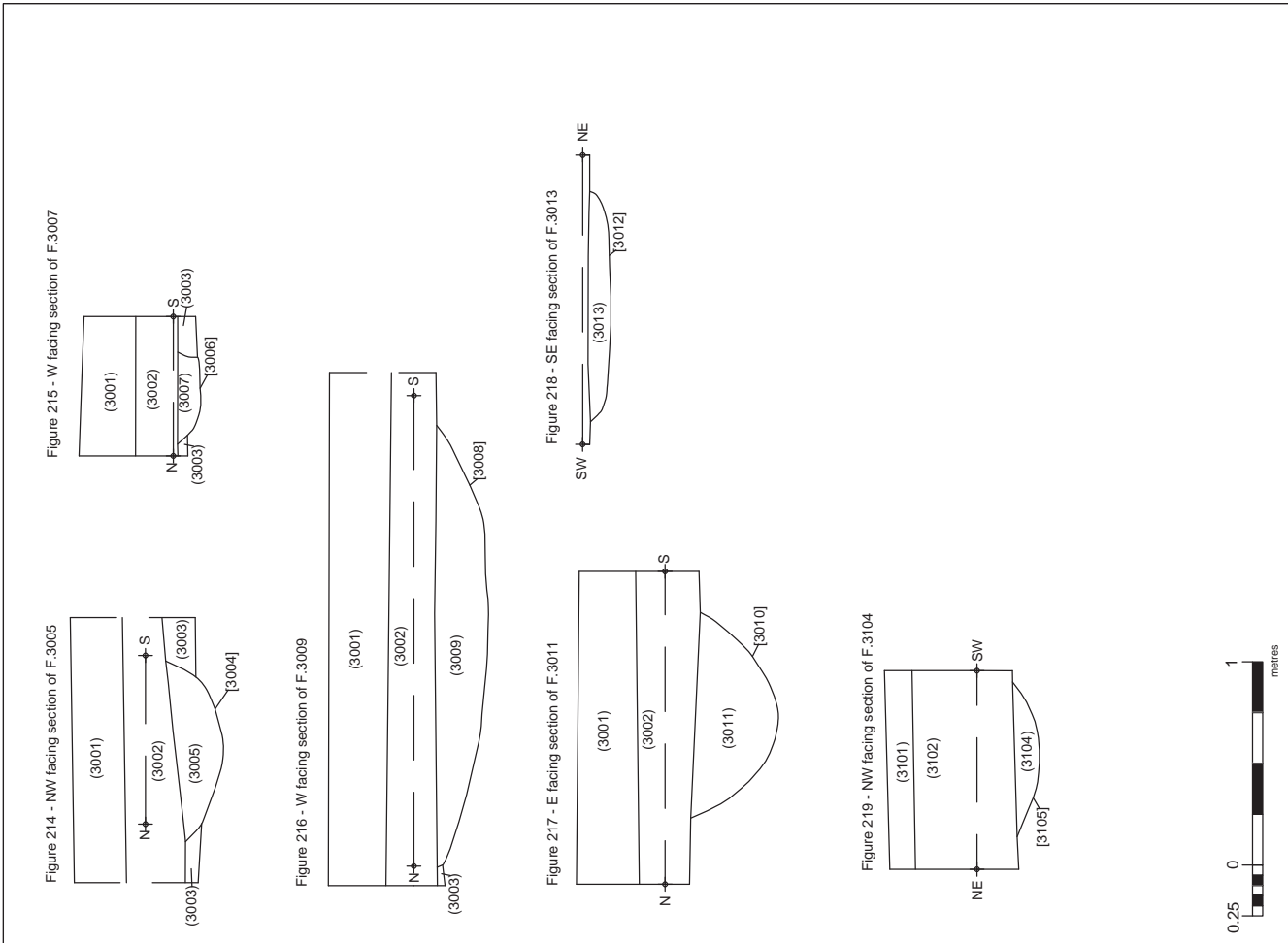


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Figure 227 - SE facing section of F.5205

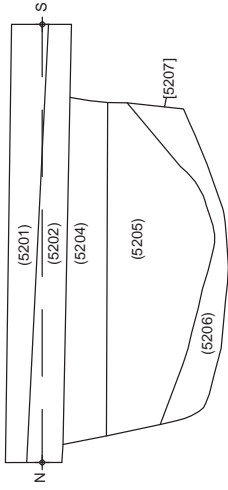


Figure 228 - SE facing section of F.5208

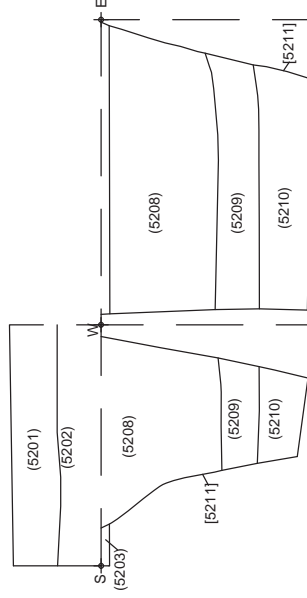


Figure 229 - NE facing section of F.5305

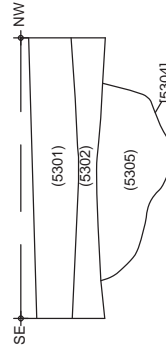
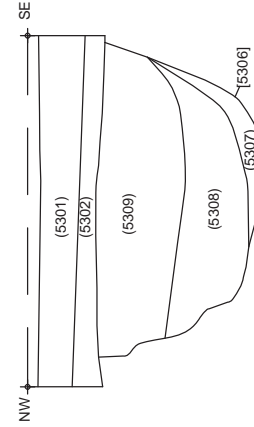


Figure 230 - SW facing section of F.5309



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Feature Sections  
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Figure 224 - SE and NW facing section of F.4706

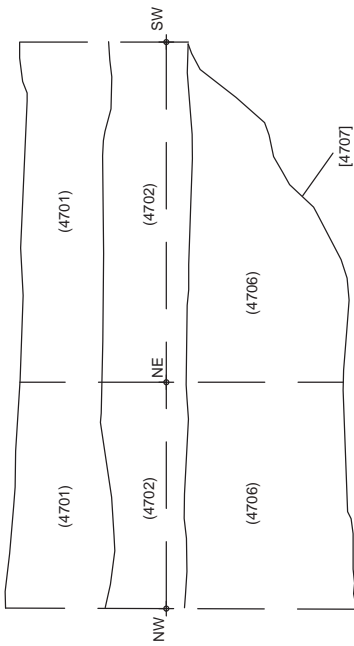


Figure 225 - SE and SW facing section of F.4705

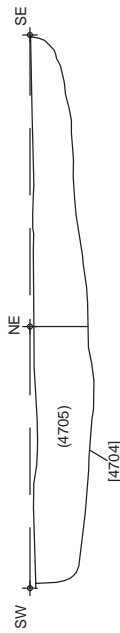
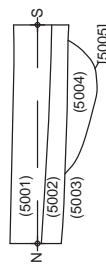


Figure 226 - E facing section of F.5004



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Figure 231 - Sections of F.5409

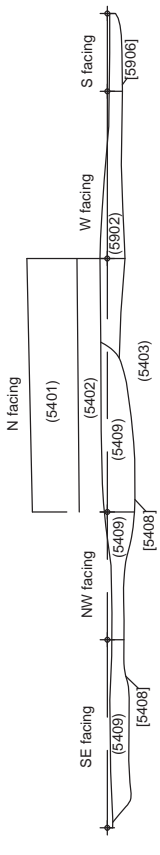


Figure 232 - SE facing section of F.5405

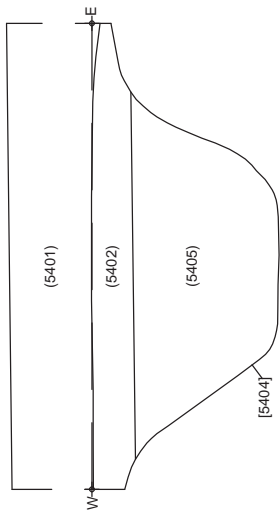


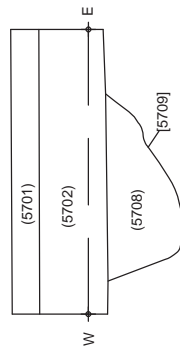
Figure 233 - S facing section of F.5704



Figure 234 - S facing section of F.5706



Figure 235 - N facing section of F.5708



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Figure 236 - S facing section of F.5804, F.5806 & F.5808

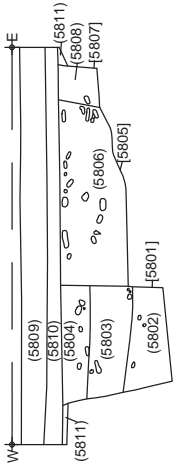


Figure 237 - N facing section of F.5813

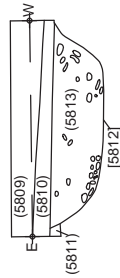


Figure 238 - W facing section of F.5904

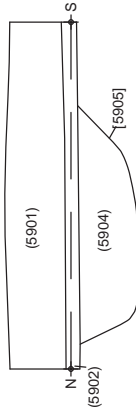


Figure 239 - W facing section of F.5906

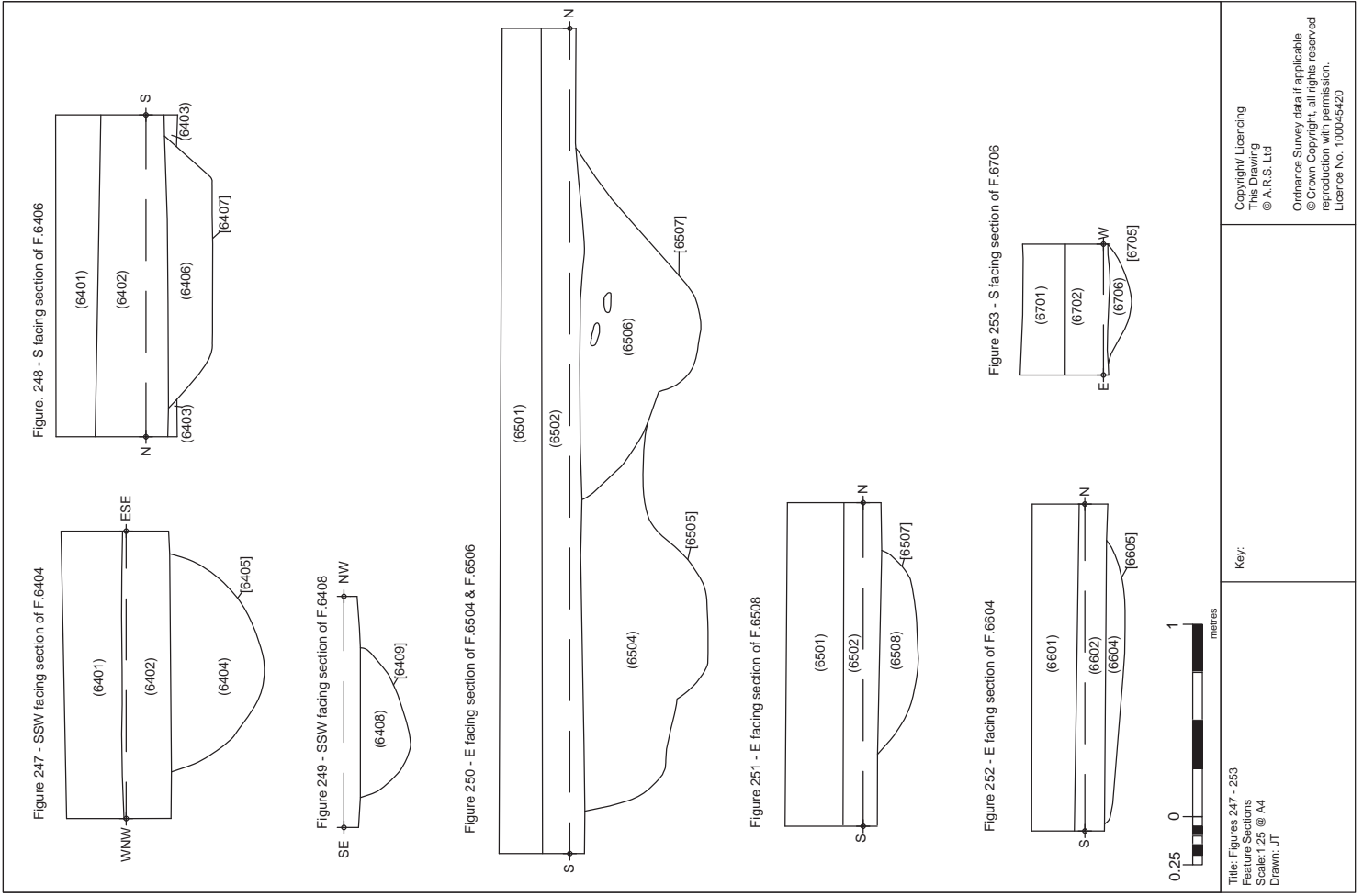


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Feature Sections  
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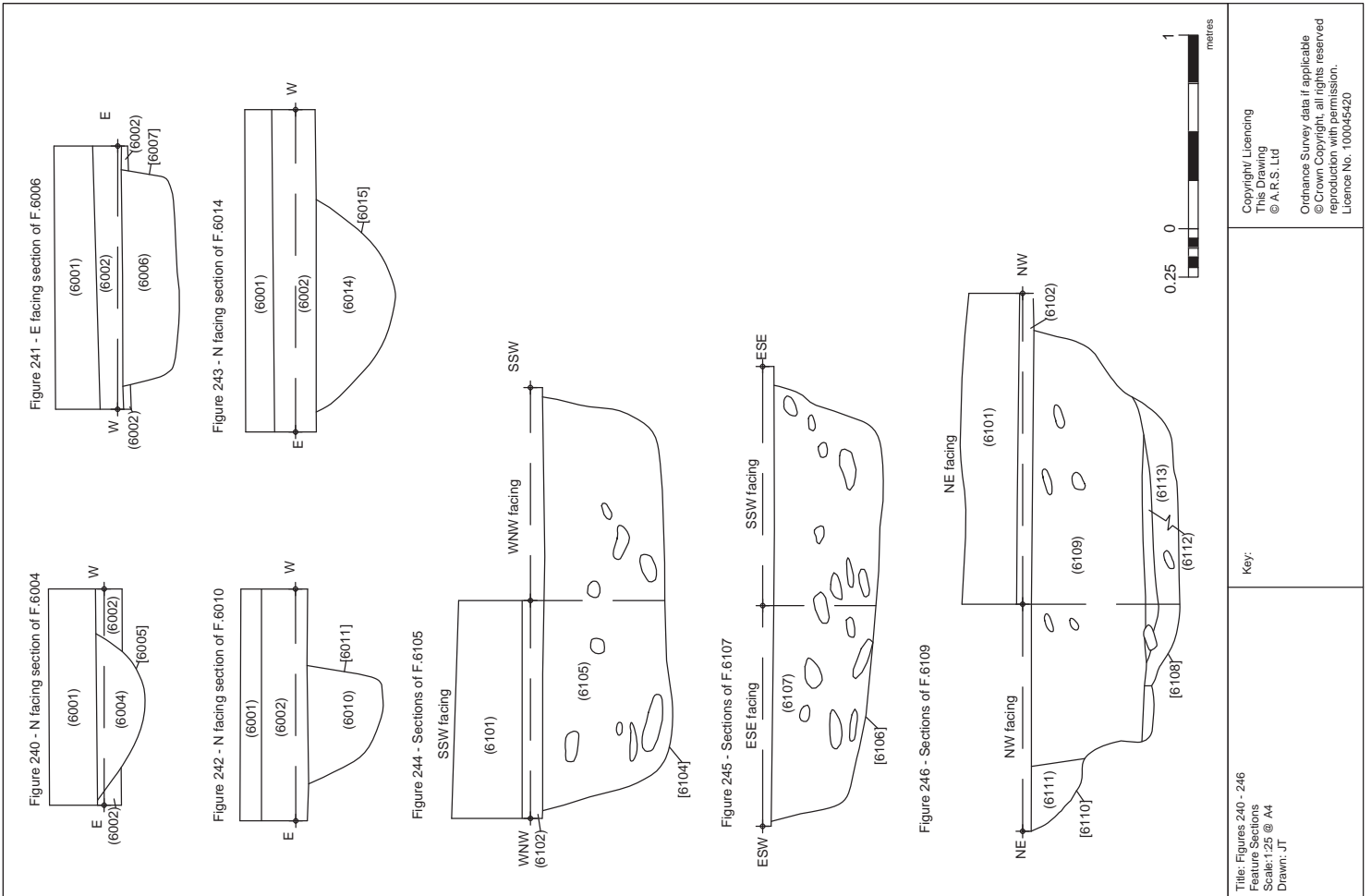
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Figure 254 - E facing section of F.6804



Figure 255 - E facing section of F.6804



Figure 256 - S facing section of F.6806

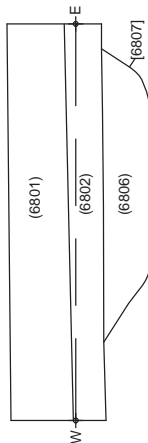
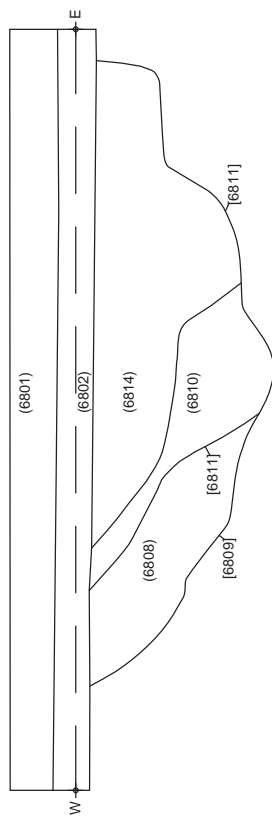


Figure 257 - SE facing section of F.6812



Figure 258 - SE facing section of F.6808 & 6810



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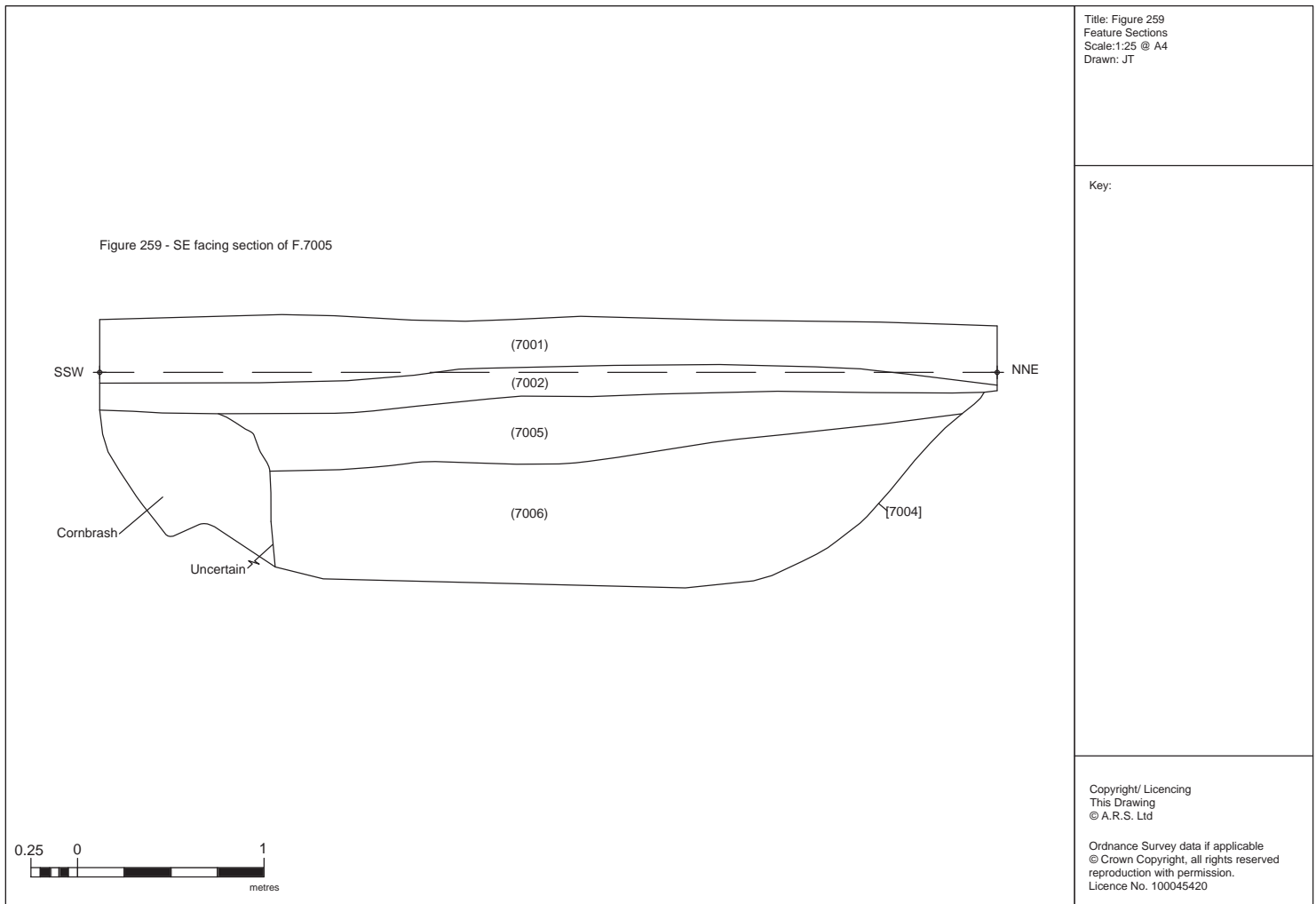


Figure 260 - SSE facing section of F.7104

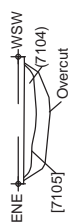
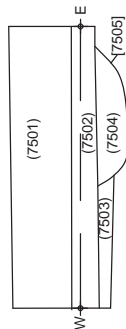


Figure 261 - N facing section of F.7504



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				roots										
5806	I mammal	long bones fgs	-	-	diaps	-	9	2	-	re, ?gnawing	-	-	poor	lbrown
5806	m mammal	long bone fg	-	-	diap	-	3.2	1	-	re	F breaks, longit split F	-	poor	lbrown
5806	s/g/roe deer	scapula	R	4,5	6,7	-	9.7	3	subadult (size)	re	-	-	poor	lbrown
5808	bird	long bone	-	-	diap	-	1.2	1	-	weath.	-	-	poor	lbrown
5808	unid	long bone fg	-	-	diap	-	1.1	1	-	weath.	-	-	poor	lbrown
5813	I mammal	long bones fgs	-	-	diaps	-	13.3	2	-	re	-	-	poor	lbrown
5813	s/g	mandible+M3	L	crown	1	-	7.5	2	TWS:j=MWS:47-49=11y1m+	re	-	-	poor	lbrown
5813	I mammal	unid	-	-	cancellous	-	4.3	1	-	weath.	-	-	poor	lbrown
5813	s/g	mandible	R	-	1	-	5.9	2	-	weath.	-	-	poor	lbrown
5813	I mammal	unid	-	-	-	-	11.4	2	-	-	-	-	poor	lbrown
5906	I mammal	unid	-	-	-	-	3.2	3	-	weath.	-	-	poor	lbrown
6004	equus sp	?deciduous upper incisor	L	crown	-	-	4.8	2	not in wear, staining suggesting erupting	-	-	-	good	mixed
6004	I mammal	unid	-	-	-	-	6.6	2	-	weath.	-	-	poor	lbrown
6010	I mammal	rib	R	-	-	-	11.3	4	-	re	-	-	poor	lbrown
6010	s/g	MTC	-	-	5,6,7,8	-	3.2	1	-	re,R	-	-	poor	lbrown
6010	pig	lower I	R	crown	-	-	1.6	1	-	-	-	-	moderate	lbrown
6010	I mammal	unid	-	-	-	-	1.6	-	-	re	-	-	poor	lbrown
6010	bird	long bone fg	-	-	diap	-	0.6	1	-	Oir	-	-	moderate	lbrown
6010	unid	maxilla+tooth	R	-	-	-	1.1	1	-	-	-	-	moderate	lbrown
6105	pig	upper M3	L	crown	-	-	10	4	subadult, M3:roots 1/4 developed, not in wear, still erupting as per discolouration	-	-	-	moderate	brown
6105	bird	long bone	-	-	diap	-	3.7	1	-	re	-	-	moderate	dbrown
6105	cattle	pelvis	R	3	1,8	-	34.4	1	fused acetabulum	re, weath.	-	-	poor	mixed
6105	dog/wolf	mandible	R	1,3	6	-	27	2	-	re, weath., abraded	-	-	poor	mixed
6105	I mammal	ribs fgs	-	2	-	-	9.9	1	-	re, abraded	-	-	poor	lbrown
6105	I mammal	long bones fgs	-	-	diaps	-	17.3	4	-	re, weath.	-	-	poor	lbrown
6105	pig	P4?	L	crown, roots	-	-	2.2	2	slight wear or damage to the occlusal aspect	-	-	-	moderate	cream
6105	pig	dp4?	-	crown, roots	-	-	1.9	1	in wear	-	-	-	moderate	cream
6107	cattle	humerus	R	3,4,5,6,7,8	-	-	98.1	4	fused	For,R,re	-	-	moderate	brown
6107	s/g	femur	R	7,8,9,10,11	6	-	5.2	2	unfused distally, epiphysis present	-	-	-	moderate	lbrown
6107	s/g	tibia	R	7,8	9	-	4.3	1	unfused proximally, eps absent	-	-	-	moderate	lbrown
6107	s/g	radius	R	6,7,8,9	5	-	5.1	1	unfused distally, epiphysis	-	-	-	moderate	lbrown

## Appendix 3- Skeletal Material

ctx	taxon	element	side	zone >50%	zone <50%	meas. (mm)	weight (gr)	fragm. count	age	taphonomy	butchery	pathology	preserv.	colour
5205	equus sp	pelvis	R	1,2,3,4,5,6,8,9,10,11	-	Sb:21.55/ LA:66.50/ LAR:59.24	182.1	1	fused acetabulum	re	-	-	moderate	lbrown
5205	cattle	tibia	L	5,6,9,10	-	Bd:59.29	114.8	2	fused distally	re	-	-	moderate	lbrown
5205	s/g	humerus	L	3,4,5,6,7,8	-	Bd:26.33/ BT:25.70	12.6	1	fused distally	-	12 xparallel cuts transverse on 8 and 6/ F breaks mid shaft	-	good	lbrown
5205	cattle	radius	R	1,2,5	7	-	37.4	3	fused prox, ulna not fused not present	re	-	-	moderate	lbrown
5205	s/g	MTC	-	-	5,6,7,8	-	5.4	1	fused medio-laterally	-	F breaks mid shaft	-	moderate	lbrown
5205	I mammal	long bones fg	-	-	diaps	-	51	3	-	re, weath.	-	-	poor	mixed
5205	cattle	zygomaticus	L	frontal process	-	-	6.2	1	-	-	-	-	poor	lbrown
5205	I mammal	long bones fg	-	unid	-	-	4.6	2	-	re, weath.	-	-	poor	lbrown
5208	equus sp	lower M3	R	crown, roots	-	width:30.27/ height:17.42	11.8	1	20y+(Levine), well in wear	weath.	-	-	poor	cream
5208	I mammal	long bone fg	-	-	diap	-	16.6	1	-	re, weath.	-	-	poor	lbrown
5208	I mammal	unid	-	-	-	-	2	1	-	re, weath.	-	-	poor	lbrown
5210	I mammal	long bones fgs	-	-	diaps	-	26.6	12	-	abraded, weath.	-	-	poor	lbrown
5210	I mammal	unid	-	-	cancellous	-	16	4	-	abraded, weath.	-	-	poor	lbrown
5308	cattle	tibia	L	5,6,8,9,10	-	Bd:54.32/ Dd:43.09	140.5	1	fused dist	re	F breaks mid shaft	porosity calcaneal aspect	moderate	lbrown
5308	s/g/roe deer	tibia	L	7,8,9,10	-	-	18.3	1	-	?gnawing	F breaks	-	good	lbrown
5308	m mammal	tibia	R	7,8,9,10	-	-	5.1	1	-	re	-	-	good	lbrown
5308	I mammal	long bones fgs	-	-	diaps	-	28.7	8	-	re, severe weath., abraded	-	-	poor	mixed
5308	s/g/roe deer	zygomaticus	L	body	frontal and temporal process	-	1.5	1	-	abraded	-	-	moderate	lbrown
5308	?bird	long bone fg	-	-	diap	-	0.9	1	-	-	-	-	poor	mixed
5802	I mammal	rib	-	-	2	-	3.6	2	-	weath.	-	-	poor	lbrown
5806	pig	humerus	L	3,4,5,6,7,8	-	Bd:34.01	25.1	1	fusion line still but barely visible	re	1xcut lateral z6 (medial condyle)	-	poor	lbrown
5806	cattle	radius	R	-	2,5	-	20	2	fused prox	re and gnawing	-	-	poor	lbrown
5806	cattle	m1/2 upper	R	crown,	-	w:23.12	7.8	1	in wear	-	-	-	moderate	cream

									<7mos(Popkin)						
6109	cattle	pelvis	L	3	1,8	-	18.1	1	fused acetabulum	re	-	-	porosity within acetabulum	moderate	lbrown
6109	I mammal	pelvis fgs	-	-	acetab. & unid	-	15.9	2	fused acetabulum	-	-	-	good	brown	
6109	I mammal	ribs fgs	-	2	-	-	28.6	1	-	re	-	-	good	brown	
6109	cattle	humerus	L	5,6	3,4,7,8	-	83.9	4	fused dist	re	F breaks	-	good	brown	
6109	cattle	radius + ulna	R	3,4,8,9,10,J	6,7	Bd:62.58 (including J)	98.5	1	fused dist and with dist ulna	re,gnawing	-	NBF on frontal aspect - z10	good	brown	
6109	s/g	mandible+m4, M2	L	-	1	-	9.7	1	m4:g, M2:c=MWS:19 (12-15mos- old subadult - Greenfield et al 2008)	re	-	-	good	brown	
6109	I mammal	long bones fg	-	-	diaps	-	80.9	14	-	-	F breaks on some	-	moderate	brown	
6109	m mammal	long bone fg	-	-	diap	-	1.4	1	-	re,gnawing	unob	unob	poor	dbrown	
6109	I mammal	unid	-	-	-	-	12.8	8	-	-	-	-	moderate	brown	
6109	I mammal	skull fgs	-	-	-	-	17.4	3	-	-	-	-	good	brown	
6109	m mammal	varia	-	-	acetab. unid	-	3.6	2	fused acetabulum	-	-	-	moderate	lbrown	
6504	I mammal	unid	-	-	cancellous	-	12	8	-	re	-	-	moderate	lbrown	
6506	I mammal	long bone	-	-	diap	-	6.2	3	-	weath.	-	-	poor	lbrown	
6808	I mammal	mandible	-	-	1	-	26.5	2	-	re,weath.	-	alveolar aspects - NBFplaques	poor	lbrown	
6808	I mammal	pelvis	-	-	5	-	32	1	-	re,gnawing	1x transverse cut	-	poor	mixed	
6808	I mammal	long bones fgs	-	-	diaps	-	64	7	-	re,gnawing	F breaks	-	poor	mixed	
6808	s/g	mandible+M1, M2, M3	L	1	-	withd:M1:9.85/ M2:12.75/ M3:20.29	12.2	10	TWS(M1):m,(M2):h,(M3):k MWS:42+=6y+	re,R	-	-	moderate	lbrown	
6808	I mammal	rib	L	2	-	-	15.8	1	-	re,R	-	-	moderate	lbrown	
6814	I mammal	mandible	-	-	6	-	5.3	1	-	weath.	-	-	poor	lbrown	
6814	I mammal	unid	-	-	cancellous	-	5.6	2	-	weath.	-	-	poor	lbrown	
6814	m mammal	long bones fgs	-	-	diaps	-	2.7	3	-	weath.	-	-	poor	lbrown	
7005	I mammal	long bones fgs	-	-	diaps	-	9.2	3	-	weath.	-	-	poor	lbrown	
7005	I mammal	?maxilla	-	-	alveolar portion	-	1.4	1	-	weath.	-	-	poor	lbrown	

				10					absent					
6107	s/g	tibia	L	8,9,10	7	-	6.4	2	>7mos(Popkin et al 2012) unfused distally, ep absent	-	-	-	moderate	lbrown
6107	s/g	pelvis	R	5	7	-	2.2	1	subadult appearance	re	-	-	moderate	lbrown
6107	s/g	pelvis	L	5	7	-	1.4	1	subadult appearance	re	-	-	moderate	lbrown
6107	s/g	mandible + P3,P4,M1	L	1	2	-	8.2	1	TWS(P4):g, (M1):h MWS:29-40 - young adult	re	-	-	moderate	lbrown
6107	mammal	ribs fgs	-	-	2	-	0.8	3	-	-	-	-	moderate	lbrown
6107	?cattle	ulna	R	C	-	-	6.4	1	fused	re	-	-	good	lbrown
6107	cattle	lower M1/2	L	crown	-	width M2:25.56	13.1	1	subadult, brown discoloration probably still erupting, subadult, TWS:a = MWS:13-16=9-15mos	-	-	-	good	cream
6107	s/g/roe deer	mitt	L	5,6,7,8	-	-	5.8	1	-	re, F sawirr	-	-	moderate	lbrown
6107	?cattle	pelvis	L	5	1	-	44.1	1	fused	re,gnawing	-	-	poor	mixed
6107	?equus sp	pelvis	L	8	9	-	23	1	-	re	-	-	moderate	lbrown
6107	m mammal	mandible	R	-	1	-	4.3	1	-	re	-	-	moderate	brown
6107	I mammal	long bone fg	-	-	diaph	-	9.6	1	-	Fspreg, Ftrreg	-	-	moderate	dbrown
6107	s/g	humerus	R	7	8	-	1.9	1	>7mos (Popkin et al 2012) unfused distally, ep absent	re	-	-	moderate	lbrown
6107	mammal	unid	-	-	-	-	3	2	-	re	-	-	moderate	brown
6107	equus sp	scapula	L	1,2,3,4,5	-	SB:21.55/ LA:66.50/ LAR:59.24/ SLC:57.27	121.8	1	fused, unfused tuber???	re,?gnawing	all on lateral aspect: 2xparallel hack15mm from tuber to spine z1, 1xhack on border between 4 and 5, parallel to spine 32.62mm,1xcut parallel to glenoid between 1 and 2	-	moderate	brown
6107	I mammal	scapula	-	unid fgs	-	-	36.8	2	-	re	-	-	moderate	brown
6107	?cattle	scapula	R	4	6	-	12.4	3	-	re	-	-	moderate	brown
6109	s/g	mandible+M3, M2, M1, P3	R	1,2,5,7	3,6	-	36.6	4	M3:g, M2:g=MWS:36-41 (3-6y - young adult - Greenfield et al 2008)	-	-	-	moderate	cream
6109	s/g	radius	R	5,6,7,8,9,10	-	-	10	1	unfused and not present epiphyses = <4mos (Noddle 1974)/ <7mos (Popkin)	re	-	-	good	brown
6109	s/g	humerus	R	7,8,9,10	-	-	7.6	1	unfused and not present dist epiphysis = <9mos(Noddle 1974)/	re	-	-	good	brown

**Appendix 4- Written Scheme of Investigation**



**Land Off Salt Way, Banbury, Oxfordshire**

**Written Scheme of Investigation for Archaeological Evaluation  
Trenching**

May 2014

**Compiled By:**

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## 1. INTRODUCTION

1.1 This Written Scheme of Investigation (WSI) has been prepared by Archaeological Research Services Ltd (ARS Ltd) on behalf of Gladman Developments Ltd. It provides a WSI for an archaeological evaluation of three fields with an area of c.18 ha which are to be the subject of a planning application for a housing development (Figure 1).

1.2 The site is situated adjacent to the southern edge of Banbury and the north-western edge of Bodicote, Oxfordshire (centred on NGR SP 456 383). The site covers an area of c.18ha and is presently used as arable land, with a plot of allotments in the south-west corner and a plot of managed open grassland in the east of the site, part of which is fenced off to provide an access easement to Bodicote Cricket Club in the south. A small plot of scrubland is located on the north boundary of the site.

1.3 The site has been the subject of an archaeological desk-based assessment (DBA) and geophysical survey (reports deposited with the County Historic Environment Record). The DBA identified that there is a moderate to high potential for previously unknown archaeological remains to be present on site. Prehistoric activity including a Neolithic causewayed enclosure and Bronze Age round barrow monuments were identified in close proximity to the site boundary. Also, Iron Age settlements were identified to the west and east of the site, and a Roman Road runs along the southern boundary of the site. However, little evidence for medieval and post-medieval activity was identified apart from ridge and furrow.

1.4 The subsequent geophysical survey identified the westernmost field (Field 1) as an area of high archaeological potential, with clear evidence of settlement activity. The archaeology has been interpreted as a late Iron Age/Romano-British settlement, possibly a farm complex which could be of regional importance. Also within Field 1, further possible archaeological remains in the form of field boundary ditches, possible pits and extensive evidence of more than one phase of agricultural activity were identified.

1.5 In the central field (Field 2), the possible remains of four circular features were identified. The largest feature is approximately 40m in diameter and could date from the Neolithic or Bronze Age. The three smaller features, with diameters of approximately 15m, may indicate the surviving remains of Iron Age round houses or Bronze Age barrows. Further evidence of field boundary ditches and possible pits were also recorded within the field.

1.6 In field 3, a well-defined, three-sided anomaly corresponding to a crop mark was recorded in the western of the field. The anomaly was interpreted as representing an archaeological feature of unknown origin although a more modern cause cannot be discounted. In the remainder of field 3, a number of minor anomalies were recorded without any particular form or context and are therefore thought unlikely to be significant. A narrow strip in the south of field 3, forming an access easement to Bodicote Cricket Club, was surveyed but was mainly paved and landscaped and has not revealed any evidence of archaeological remains.

1.7 This WSI covers the programme of archaeological evaluation, the scope of which has been agreed with the Oxfordshire County Council (OCC) Planning Archaeologist. The evaluation is to comprise 80 no. 30m by 1.5m trenches, which equates to a 2% sample of the entire site, with a further 1% sample held in reserve as a contingency.

1.8 The archaeological evaluation will be carried out in compliance with the Institute for Archaeologists (IfA) Codes of Conduct (2012) and will follow the IfA's Standard and Guidance for Archaeological Evaluation (2009a).

1.9 This evaluation programme has been prepared in line with the National Planning Policy Framework (NPPF) paragraph 128: "Where a site on which development is proposed includes or has the potential to include heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation" (DCLG 2012, 30).

## 2. AIMS AND OBJECTIVES

### 2.1 Aim

- To establish the presence/absence, extent, condition, character and date of any archaeological deposits within the area affected by invasive development.
- Gather sufficient evidence to establish, supplement, improve and make available information about the archaeological resource existing within the areas of investigation.
- Use the evidence as the basis of any proposals for appropriate mitigation measures that may seek to limit the damage to significant archaeological deposits, and aim to define any research priorities that may be relevant should further investigation be required

### 2.2 Objectives

- Excavation of trenches by machine targeting potential buried features identified by the magnetometry survey as well as apparently 'blank' areas and covering 2% of the proposed development site down to the surface of any archaeological deposits and investigation of these deposits in order to ascertain their nature and date (following standard excavation methodologies). Should significant archaeological deposits be encountered, particularly in areas where the geophysical survey has not recorded many anomalies, further trenching may be required. A contingency of up to a further 1% sample size may be utilised within these areas. Any use of the contingency will be agreed by the Planning Archaeologist at OCC and the archaeological contractor on behalf of the site owners.

- Appropriate post-excavation assessment, analysis, reporting, archiving and dissemination which will aim to define any research priorities that may be relevant should further field investigation be required.

## 3. METHODOLOGY

3.1 The evaluation trenching targeting potential buried features identified by the magnetometry survey and covering 2% of the proposed extraction site will be undertaken in accordance with the following methodology.

### Objectives

3.2 The objective of the evaluation trenching is to identify and assess archaeological features within the area of the proposed development in order to inform:

- the location, nature and date of any archaeological features encountered;
  - and potential significance of buried archaeology on the site.
- 3.3 All elements of the archaeological evaluation will be carried out in accordance with the Institute for Archaeologists (IfA) *Standards and Guidance for field evaluation* (2009a) and with the IfA *Code of Conduct* (2012).
- 3.4 Any changes to the agreed trenching plan will be discussed with, and agreed with, the OCC Planning Archaeologist before implementation.

#### Excavation by machine

3.5 Topsoil and unstratified modern material will be removed mechanically by a machine equipped with a wide toothless ditching blade under the supervision of a qualified archaeologist. The topsoil will be removed down to the first significant in situ archaeological horizon or the natural horizon, whichever is encountered first, in successive level spits. The exposed surface would be cleaned using appropriate hand tools for the purpose of identifying any archaeological remains. The trenches and all exposed features and deposits will be drawn in plan and located on a general site plan compiled at an appropriate scale.

#### Excavation by hand

3.6 Archaeological features will generally only be sample-excavated sufficiently to characterise and date them. Full excavation of features should not be undertaken at this stage.

3.7 Sufficient of the archaeological features and deposits identified will be excavated by hand through a specified or agreed sampling procedure to enable their date, nature, extent and condition to be described. No archaeological deposits should be entirely removed unless this is unavoidable. It is not necessarily expected that all trial trenches will be fully excavated to natural subsoil, but the depth of archaeological deposits across the whole site will be assessed. The stratigraphy of all trial trenches will be recorded even where no archaeological deposits have been identified. Spoil heaps shall be monitored to allow analysis of the spatial distribution of artefacts.

3.8 The site will be recorded using in accordance with the ARS Ltd field recording manual. A full and proper record (written, graphic and photographic as appropriate) will be made for all work, using a single context planning system with pro-forma record sheets and text descriptions appropriate to the work in accordance with the ARS Ltd field recording manual. Accurate scale plans and section drawings will be drawn at 1:50, 1:20 and 1:10 scales as appropriate. A photographic record of all contexts will be taken in colour high resolution digital format and will include a clearly visible, graduated metric scale. A register of all photographs will be kept.

#### Treatment of finds

3.9 All finds processing, conservation work and storage of finds will be carried out in compliance with the IfA *Standard and Guidance for the collection, documentation, conservation and research of archaeological materials* (2008) and those set out by UKIC (1990).

3.10 Artefact collection and discard policies will be appropriate for the defined purpose. All retained artefacts and ecofacts will be cleaned and packaged in accordance with the requirements of the recipient museum. Bulk finds which are not discarded will

be washed and, with the exception of animal bone, marked. Marking and labelling will be indelible and irremovable by abrasion. Bulk finds will be appropriately bagged, boxed and recorded. This process will be carried out no later than two months after the end of the excavation.

3.11 All small finds will be recorded as individual items and appropriately packaged (e.g. lithics in self-sealing plastic bags and ceramic in acid-free tissue paper). Vulnerable objects will be specially packaged and textile, painted glass and coins stored in appropriate specialist systems. This process will be carried out within two days of the small find being excavated.

3.12 Metal finds will be sampled, processed and analysed in line with *Centre for Archaeology Guidelines: Archaeometallurgy* (English Heritage 2001), and *Guidelines on the X-radiography of archaeological metalwork* (English Heritage 2006a). Any waterlogged artefacts or ecofacts will be sampled, processed and analysed using *Waterlogged Wood: Guidelines on the Recording, Sampling, Conservation and Care of Waterlogged Wood* (English Heritage 2010) and *Waterlogged Organic Artefacts: Guidance on their Recovery, Analysis and Conservation* (English Heritage 2012).

3.13 Artefacts, ecofacts and deposits suitable for dating purposes will be identified and obtained in line with *Dendrochronology: Guidelines on producing and interpreting dendrochronological dates* (English Heritage 1998), *Archaeomagnetic Dating: Guidelines on producing and interpreting archaeomagnetic dates* (English Heritage 2006b), and *Luminescence Dating: Guidelines on using luminescence dating in archaeology* (English Heritage 2008a).

3.14 Any surface finds will be collected, recorded and processed in line with *Our Portable Past: a statement of English Heritage policy and good practice for portable antiquities/surface collected material in the context of field archaeology and survey programmes (including the use of metal detectors)* (English Heritage 2014) and any finds deemed to constitute 'treasure' under the terms of the *Treasure (Designation) Order 2002* will be dealt with in line with *The Treasure Act 1996 Code of Practice (England and Wales)* (DCMS 2008).

3.15 During and after the excavation all objects will be stored in appropriate materials and storage conditions to ensure minimal deterioration and loss of information (including controlled storage, correct packaging, and regular monitoring, immediate selection for conservation of vulnerable material). All storage will have appropriate security provision.

3.16 All finds will be exposed, lifted, cleaned, conserved, marked, bagged and boxed in accordance with the United Kingdom Institute for Conservation (UKIC) *First Aid For Finds* (1990), The Institute for Field Archaeologists *Standard and Guidance for the collection, documentation, conservation and research of archaeological materials* (IfA 2008) and the recipient museum's guidelines. All artefacts will be collected with consideration of what material should be retained or discarded being made after post-excavation assessment and following the advice from the relevant specialist(s).

3.17 Any deposits relating to funerary/ritual activities, such as burials and cremation deposits, will initially be left *in situ*. However, should it be deemed necessary to remove any such human remains, this will be undertaken in line with best practice (English Heritage 2004a; English Heritage and The Church of England 2005; APABE/English Heritage 2013; Brickley and McKinley 2004). Discovery of any human remains will be

reported to the coroner and excavated following receipt of the appropriate Ministry of Justice Guidelines.

3.18 Domestic/industrial activity (such as walls, postholes, floors, hearths) will be sufficiently excavated to understand their form and function and to recover potential dating evidence and artefact and ecofact assemblages.

3.19 All finds which may constitute 'treasure' under the Treasure Act 1997 will be removed to a safe place and reported to the local Coroner. Where removal cannot take place on the same working day as discovery, suitable security will be taken to protect the finds from theft.

3.20 The deposition and disposal of artefacts will be agreed with the legal owner and the appropriate museum prior to the work taking place. All finds except treasure trove are the property of the landowner.

#### Report

3.21 Following the fieldwork ARS Ltd will prepare a report in accordance with The Institute for Field Archaeologists Standard and Guidance for the collection, documentation, conservation and research of archaeological materials (IFA 2008) that will include the following.

- An abstract containing the essential elements of the results will precede the main body of the report.
- A digital location plan showing all the excavated areas (as dug), tied into the Ordnance Survey Data.
- Section drawings at a scale of 1:10 showing depth of deposits including present ground level with Ordnance Datum, vertical and horizontal scale.
- A stratigraphical description of features and deposits.
- A table summarising any descriptive text showing, per trench, the features, classes and numbers of artefacts located and their interpretation.
- A reconsideration of the methodology used, i.e. a confidence rating.
- A consideration of the archaeological evidence from within the site set in its broader landscape setting.
- Photographic record of the site and detail of archaeological features.
- Any specialist assessments. This will include a report on any flintwork, pottery and/or metalwork discovered *in situ* in an archaeological context or that pre-dates the 19<sup>th</sup> century. A specialist report on any animal or human bone discovered within an archaeological context will also be included.
- A concise non-technical summary of the project results.

3.22 The report will not give an opinion on whether preservation or investigation is considered appropriate. (However, the client may wish to commission separately ARS Ltd's opinion on an appropriate treatment of the archaeological resource.)

3.23 On completion of the work a digital copy of the summary report in pdf format shall be supplied to the office of the County Archaeological Officer; for verification and assessment by the CAO or his representative; when the report has been agreed the final digital copy will then be lodged with the County Historic Environment Record (HER) on the understanding that it will become a public document after an appropriate period of time (generally not exceeding six months).

3.24 At the start of work (immediately before fieldwork commences) an OASIS online record <http://ads.ahds.ac.uk/project/oasis/> will be initiated and key fields completed on Details, Location and Creators forms. All parts of the OASIS online form will be completed for submission to the HER. This will include an uploaded pdf version of the entire report. Should any archaeological remains uncovered through this work be deemed to be of special significance, discussions will be held involving the client and the Planning Archaeologist at Oxfordshire County Council about a suitable means of further dissemination or publication.

#### Archive

3.25 An archive, consisting of all written records and materials recovered, drawn and photographed records will be prepared. It will be quantified, ordered, indexed and internally consistent. It will contain a site matrix, site summary and brief written observations on the artefactual and environmental data. The archive will also be prepared in line with UKIC Guidelines for the preparation of excavation archives for long term storage (1990), *The Institute for Field Archaeologists Standard and Guidance for the creation, compilation, transfer and disposition of archaeological archives* (IFA 2009b) and the recipient museum's guidelines.

3.26 Arrangements for the deposition of the full site archive will be made with the Oxfordshire County Museum Service, which will be consulted at the outset of the post-excavation phase concerning their requirements.

3.27 The archive will be presented to the archive curator within six months of completion of the fieldwork, unless alternative arrangements have been agreed in writing with the OCC Planning Archaeologist and the archive curator.

## 4. STANDARDS AND PROJECT MANAGEMENT

4.1 ARS Ltd is a Registered Organisation with the Institute for Archaeologists (IFA). Registered Organisations are continuously assessed to ensure that the highest standards of work are carried out, in line with the *Code of Conduct* of the IFA (2012). In addition to our key management staff, who have achieved the highest grade of corporate IFA membership, many of our field staff also hold corporate grade membership.

4.2 All staff employed on the project will be suitably qualified and experienced for their respective project roles and have practical experience of archaeological excavation and recording. All staff will be made aware of the archaeological importance of the area surrounding the site and will be fully briefed on the work required by this specification. Each member of staff will be fully conversant with the aims and methodologies and will be given a copy of this WSI to read. All members of staff employed by ARS Ltd are fully qualified and experienced archaeologists, this will ensure that appropriate decisions regarding excavation and sampling will be made in the field.

#### 4.3 Project Team

Project management: Chris Scott MIFA (ARS Ltd)  
 Fieldwork supervisor: Scott Williams (ARS Ltd)  
 Post-fieldwork and reporting: Scott Williams (ARS Ltd)

Flint specialist:  
 Ceramic specialists:  
 Metalwork specialist:  
 Plant macrofossils and charcoals:  
 Human remains:  
 Faunal remains:  
 Finds Conservation:

Dr Robin Holgate MIFA (ARS Ltd)  
 Dr. Clive Waddington MIFA (ARS Ltd), Dr Jane Timby, Mike Wood MIFA  
 Conservation Laboratory  
 Laura Trafford AIFA (ARS Ltd)  
 Milena Grzybowska (ARS Ltd)  
 Milena Grzybowska (ARS Ltd)  
 Dr Jenny Jones (Durham University)

## 5. MONITORING

5.1 The archaeological evaluation work, and the subsequent post-excavation and report preparation, will be monitored by the Oxfordshire County Council Archaeological Services (Directorate Environment & Economy), or their representative, by means of project updates and/or site visits. Prior notification of a site visit is required from Oxfordshire County Council to ARS Ltd in their role as agent for Gladman's Development Ltd.

5.2 Reasonable access to the site will be allowed to the Planning Archaeologist at Oxfordshire County Council or their nominee for the purpose of monitoring the archaeological evaluation.

## 6. GENERAL ITEMS

### Health and Safety

6.1 All work will be carried out in accordance with The Health and Safety at Work Act 1974. Specific health and safety policies exist for all our workplaces and all staff employed will be made aware of the policy and any relevant issues. The particular risks involved with this project will be assessed, recorded and relevant mitigation measures put in place as part of a full risk assessment, which will be compiled in advance of fieldwork. ARS Ltd retains Peninsula as its expert health and safety consultants.

### Insurance Cover

6.2 ARS Ltd has full insurance cover for employee liability, public liability, professional indemnity and all-risks cover.

## 7. ADJUSTMENTS TO THE WRITTEN SCHEME OF INVESTIGATION

7.1 Changes to the approved methodology or programme of works will only be made after discussion and with written approval of the Planning Archaeologist at Oxfordshire County Council.

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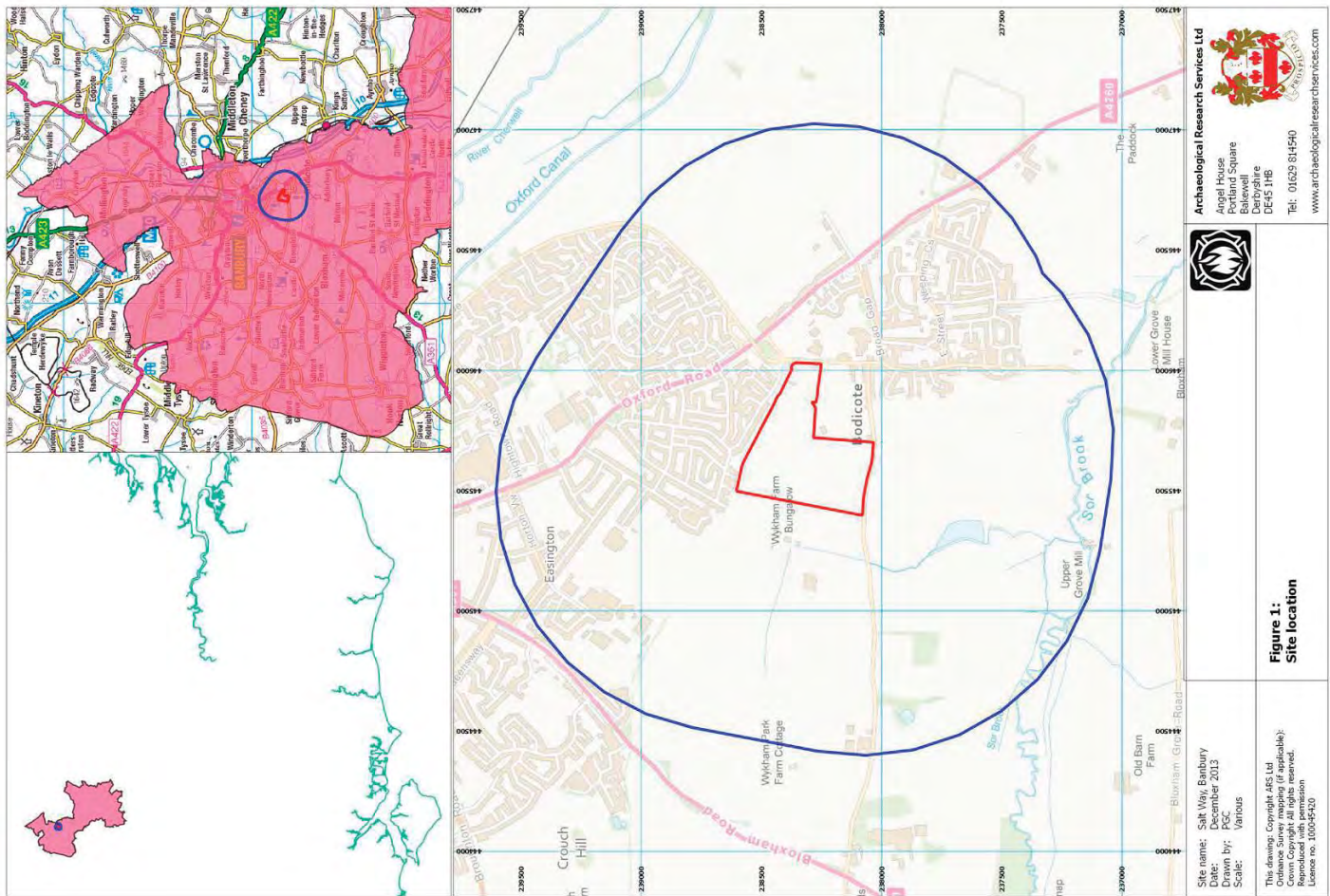
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Site name: Salt Way, Banbury  
 Date: 15 November 2013  
 Drawn by: PCJ  
 Scale: Various



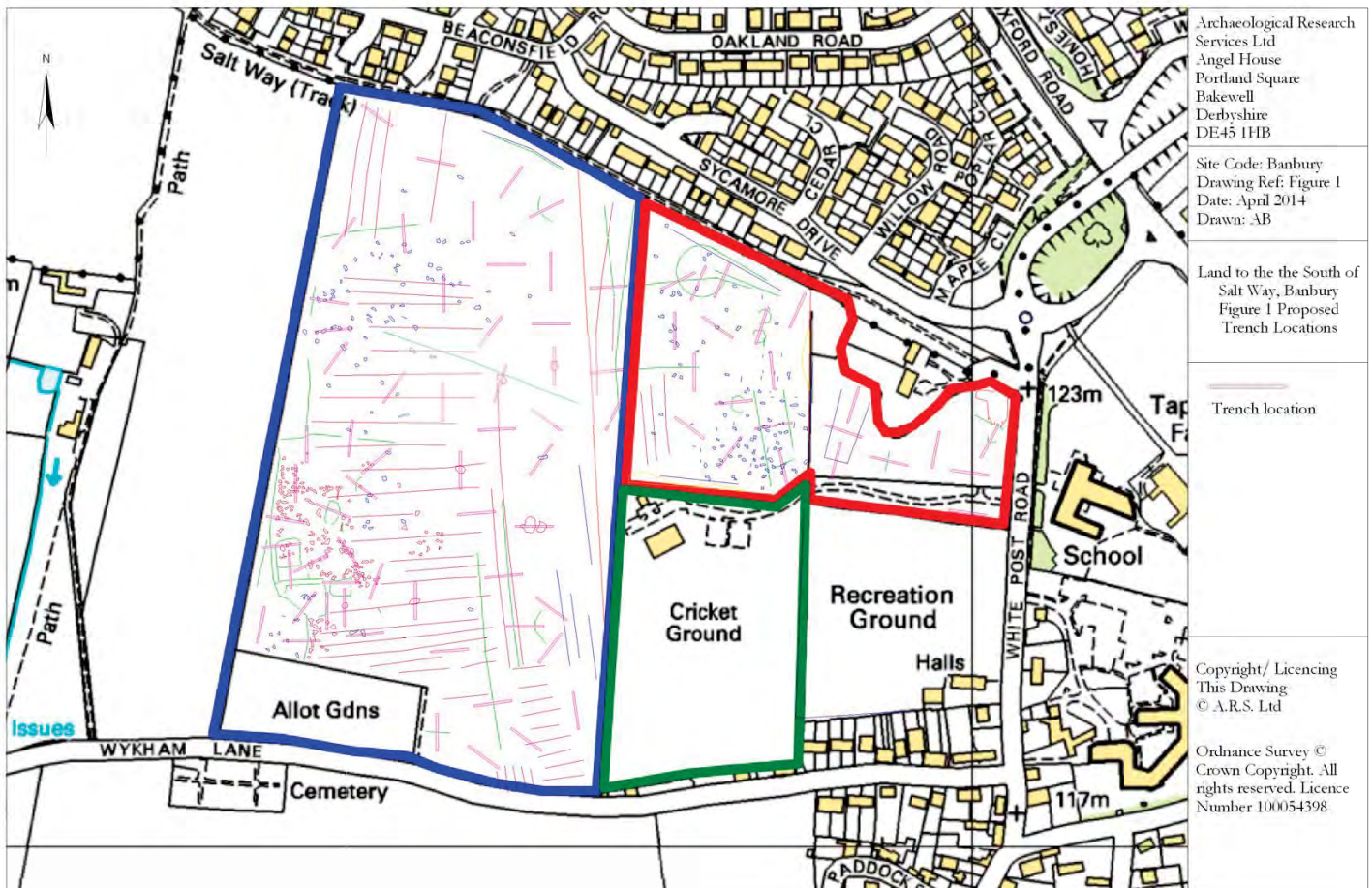
Archaeological Research Services Ltd  
 Angel House  
 Portland Square  
 Banbury  
 Oxfordshire  
 OX15 1HE  
 Tel: 01629 614540  
 www.archaeologicalresearchservices.com

**Figure 1:**  
**Site location**

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Appendix 5- Oasis Report

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# OASIS DATA COLLECTION FORM: England

[List of Projects](#) | [Manage Projects](#) | [Search Projects](#) | [New project](#) | [Change your details](#) | [HER coverage](#) | [Change country](#) | [Log out](#)

Entered by Joseph Tong (joseph@archaeologicalresearchservices.com)  
Entered on 7 October 2014

[Printable version](#)

## OASIS:

**OASIS ID:** [archaeo05-191966](#)

Please e-mail [English Heritage](mailto:English.Heritage@oasis.ac.uk) for OASIS help and advice  
© ADS 1996-2012 Created by Jo Gilham and Jen Mitcham, email [Last modified](mailto:Last.modified@oasis.ac.uk) Wednesday 9 May 2012  
Site only: <http://www.oasis.ac.uk/form/print.cfm> For this page

### Project details

**Project name** An Archaeological Evaluation at land south of the Saltway, Banbury  
**Short description of the project** In August 2014, Archaeological Research Services Ltd (ARS Ltd) was commissioned by Gladman Developments Ltd to undertake an archaeological evaluation at land south-west of the Salt Way, Banbury, Oxfordshire. The evaluation was carried out in advance of a proposed housing development project. The purpose of the evaluation was to determine the nature of a series of anomalies identified by geophysical survey conducted by Archaeological Research Services Ltd in 2014 and the extent to which these features place archaeological constraints upon the proposed development. The archaeological evaluation was comprised of eight trenches, measuring 1.8m x 30m. Trenches 22, 23, 25, 34, 35 and 36 were not excavated due to the presence of overhead power-lines within close proximity to the afore-mentioned trench locations. Additionally, Trench 1 was unexcavated due to the high potential for damage to private property likely to be caused by the use of a mechanical excavator during trench excavation.

**Project dates** Start: 01-01-1901 End: 02-01-1901

**Previous/future work** Not known / Not known

**Type of project** Field evaluation

**Monument type** DITCH Late Prehistoric

**Significant Finds** NONE None

**Methods & techniques** "Targeted Trenches"

**Development type** Not recorded

**Prompt** General structure plan/local plan/minerals plan guidance

**Position in the planning process** Not known / Not recorded

### Project location

**Country** England

**Site location** OXFORDSHIRE CHERWELL BANBURY Saltway Banbury

**Study area** 0 Hectares

**Site coordinates** SP 446245 237857 51.9103794465 -1.35120504756 51 54 37 N 001 21 04 W Point

## **A7 Historic Environment**

### Appendix 7.4 Heritage Statement

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## Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire



**ARS Ltd Report: 2014/43**  
March 2014

**Compiled By:**  
Dr Victoria Park

Archaeological Research Services Ltd  
Angel House  
Portland Square  
Bakewell  
Derbyshire  
DE45 1HB

**Checked By:**  
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## Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire

### Archaeological Research Services Ltd Report 2014/43

March 2014



© Archaeological Research Services Ltd 2014  
Angel House, Portland Square, Bakewell, DE45 1HB  
[www.archaeologicalresearchservices.com](http://www.archaeologicalresearchservices.com)

Prepared on behalf of: Gladman Developments Ltd  
Date of compilation: March 2014  
Compiled by: Dr Victoria Park  
Checked by: Chris Scott MIFA  
Local Authority: Cherwell District Council

## EXECUTIVE SUMMARY

Archaeological Research Services Ltd (ARS Ltd) was commissioned to produce a Heritage Statement in relation to the impact of a proposed development at Salt Way, Banbury, Oxfordshire.

The study established that there are 39 Listed Buildings within a 1km study area. Thirty eight of these are Grade II Listed Buildings, and one is Grade II\*. Many of these lie within the Bodicote Conservation Area to the south-east of the proposed development area.

The study concluded that the majority of these assets will be unaffected by the proposed development due to their location within the compact village centre of Bodicote. The study found that the impact of the proposed development on the setting of built heritage assets at the north and west of Bodicote village, and to the west of the proposed development area would be **neutral or slight**. The setting of Bodicote Conservation Area, with its rural feel would largely be maintained due to the retention of the key views across fields and open land to the south and west as well as the retention of the cricket ground and recreation ground which would maintain open space between Bodicote and Banbury. The potential effect on the archaeological setting of the buried Neolithic Causewayed Enclosure and Bronze Age Barrows to the west of the proposed development site is considered to be **slight to moderate**, but further archaeological investigation within the proposed development area would be needed to establish this.

As such this effect does not constitute any direct or “substantial harm”, but rather “less than substantial harm” to its wider context, and this is not considered grounds for the refusal of consent under the National Planning Policy Framework (para. 133).

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**1 INTRODUCTION****1.1 Project and Planning Background**

Archaeological Research Services Ltd (ARS Ltd) was commissioned to produce a Heritage Statement on behalf of Gladman Developments Ltd in relation to the impact on the setting of built heritage of the proposed development at land to the south-west of Salt Way, Banbury, Oxfordshire.

**1.2 Site Description**

The proposed development area (PDA) is situated adjacent to the southern edge of Banbury and the north-western edge of Bodicote, Oxfordshire (centred on NGR: SP 456 383) (Figure 1 and 2). The site covers an area of c.18ha, bounded to the north by the Salt Way (an unmetalled track), on the west by an arable plot, on the south by Wykham Lane, on the south-east by Bodicote Cricket Club and on the east by White Post Road. The site is presently used as arable land with a plot of allotments in the south-west corner and a plot of managed open grassland in the east of the site. Part of this is fenced off to provide an access easement to Bodicote Cricket Club in the south. A small plot of scrubland is located at the north boundary of the site.

**1.3 Location and Geology**

The solid geology of the east and west parts of the development area consists of ferruginous limestone and ironstone of the Marlstone Rock Formation. The drift geology of the south and centre parts of the development area consists of siltstone and mudstone of the Dyrham Formation (British Geological Survey, 2014).

**2 AIMS AND OBJECTIVES**

A full desk-based assessment (DBA) was conducted for the site in 2013 by Archaeological Research Services Ltd (Clarke 2013). The 2013 study drew conclusions about the potential for buried archaeology within the site, and identified potential setting issues. This Heritage Statement draws upon this, and focuses on the setting of the nearby Listed Buildings and Bodicote Conservation Area, whilst also taking cognisance of the impact on setting of two buried non-designated archaeological sites to the west of the proposed development site.

**3 POLICY AND GUIDANCE****3.1 National Planning Policy Framework (NPPF)**

This assessment was carried out under the National Planning Policy Framework (NPPF) (DCLG 2012). The NPPF sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that it is relevant, proportionate and necessary to do so. The purpose of the NPPF is to contribute to



the achievement of sustainable development, which includes “...contributing to protecting and enhancing our natural, built and historic environment...” (DCLG 2012, 30).

Section 12 of the NPPF deals with government policy in relation to conserving and enhancing the historic environment and its role in sustainable development.

Paragraph 126 states that Local Authorities must undertake to, “recognise that heritage assets are an irreplaceable resource and conserve them in a manner appropriate to their significance”. In developing their strategy, local planning authorities should take into account:

- ◆ the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- ◆ the wider social, cultural, economic and environmental benefits that conservation of the historic environment can bring;
- ◆ the desirability of new development making a positive contribution to local character and distinctiveness; and
- ◆ opportunities to draw on the contribution made by the historic environment to the character of a place.

Paragraph 128 states that, “In determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. The level of detail should be proportionate to the assets’ importance and no more than is sufficient to understand the potential impact of the proposal on their significance. As a minimum the relevant historic environment record should have been consulted and the heritage assets assessed using appropriate expertise where necessary. (DCLG 2012, 30).

Paragraph 129 states that, “Local planning authorities should identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting the setting of a heritage asset) taking account of the available evidence and any necessary expertise. They should take this assessment into account when considering the impact of a proposal on a heritage asset, to avoid or minimise conflict between the heritage asset’s conservation and any aspect of the proposal” (DCLG 2012, 30).

In determining planning applications, planning authorities should take account of:

- ◆ other relevant books and journals that are identified in the course of the data collection; and
- ◆ the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- ◆ the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and



- ◆ the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraphs 132 – 141 provide guidance on the approach to be adopted by local authorities in weighing the impact of development against the conservation of heritage assets and their setting and significance (DCLG 2012, 31-32). Paragraph 132 states that “When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset’s conservation. The more important the asset, the greater the weight should be. Significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. As heritage assets are irreplaceable, any harm or loss should require clear and convincing justification. Substantial harm to or loss of a grade II listed building, park or garden should be exceptional.

Substantial harm to or loss of designated heritage assets of the highest significance, notably scheduled monuments, protected wreck sites, battlefields, grade I and II\* listed buildings, grade I and II\* registered parks and gardens, and World Heritage Sites, should be wholly exceptional” (DCLG 2012, 31).

Paragraph 141 states the Government requirement for the planning process to contribute to public understanding of the historic environment. The online Planning Practice Guidance (DCLG 2014) provides guidance on the practical implementation of the NPPF, and is consequently a much more in-depth document than the policy statement itself.

#### Setting Policy and Guidance

The NPPF Paragraph 128 states that “In determining applications, local planning authorities should require an applicant to describe the significance of any heritage assets affected, including any contribution made by their setting. As defined in the Planning Practice Guidance, setting is the surroundings in which an asset is experienced (CLG/DCMS/EH 2014). Paragraph 013 of the Planning Practice Guidance (CLG/DCMS/EH 2014) considers setting extensively. It states that “setting is the surroundings in which an asset is experienced, and may therefore be more extensive than its curtilage”. While views are an important part of setting “the way in which we experience setting is also influenced by other environmental factors such as noise, dust and vibration from other land uses in the vicinity, and by our understanding of the historic relationship between places”.

In the document, Conservation Principles: Policy and Guidance (English Heritage 2008) it is noted that ‘places where significance stems essentially from the coherent expression of their particular cultural heritage values can be harmed by interventions of a radically different nature’ (English Heritage 2008, 58). English Heritage’s guidance document, The Setting of Heritage Assets (2011) refers to an older practice guide for PPS5, although the wording is identical in the new Planning Practice Guidance Annex 2. In particular it refers to the statement: “All heritage assets have a setting, irrespective of the form in which they survive and whether they are designated or not. Elements of a setting may make a positive or negative





contribution to the significance of an asset, may affect the ability to appreciate that significance, or may be neutral” (CLG/DCMS/EH 2010, 34).

### 3.2 Local Planning Policies

In addition to referring to the national policies outlined above, the Cherwell Council Saved Planning Policies include the following in relation to Historic Buildings and Conservation Areas.

*Policy C23, Paragraph 9.52 states “The conservation areas have distinct visual characteristics and it is important that these are recognised when proposals or new development are formulated. Formality or informality of building line, the methods of boundary enclosure, homogeneity of building materials, local architectural styles and building practice, the importance of undeveloped space within the settlement structure, and the setting of existing buildings, are some of the matters that should be taken into account”.*

In relation to the setting of monuments and archaeological sites, Policy C25 states *“In considering proposals for development which would affect the site or setting of a Scheduled Ancient Monument, other nationally important archaeological sites and monuments of special local importance, the council will have regard to the desirability of maintaining its overall historic character, including its protection, enhancement and preservation where appropriate”.*

*Paragraph 9.56 states that “It must be acknowledged that the character and setting of an archaeological site or monument which may include historic landscapes, parks and gardens may be damaged or even destroyed by certain forms of development. In such cases policy C25 will apply. 9.57 Some ancient monuments are scheduled by the Secretary of State under the Ancient Monuments and Archaeological Areas Act 1979. The scheduled ancient monuments in this District are listed in Appendix D. In addition to planning permission granted by the District Council, the Secretary of State’s consent is required in the form of scheduled monument consent for any development likely to affect the site of a scheduled monument. PPG16 states that “where nationally important archaeological remains, whether scheduled or not, and their settings, are affected by proposed development there should be a presumption in favour of their physical preservation”*

## 4 METHOD STATEMENT

This Heritage Statement was undertaken in accordance with Government Policy on the historic environment contained within the National Planning Policy Framework (NPPF).

### 4.1 Study Area

The study area adopted for this assessment comprised the PDA together with a 1km buffer zone surrounding it. The extent of the 1km buffer zone is depicted by a blue



polygon on Figure 1. Two additional Grade I listed buildings which were outside of the 1km study area, were taken into consideration during this study due to the DBA (Clarke 2013) identifying that these had sweeping views across the area, and therefore had the potential to be affected by the development.

### 4.2 Information Sources

The following sources of information were consulted to inform this assessment:

- ◆ An archaeological desk-based assessment by Archaeological Research Services Ltd (Clarke 2013). This assessment produced a detailed study of the archaeological potential of the site and was thoroughly reviewed. It is referred to throughout this document;
- ◆ the online National Heritage List for England (NHLE) maintained by English Heritage for information regarding designated assets, as well as their downloadable GIS datasets of Scheduled Monuments, Listed Buildings, Registered Parks and Gardens of Historical Interest, Registered Battlefields, and World Heritage Sites;
- ◆ the British Geological Survey onshore digital maps at 1:50 000 scale (DIGMapGB-50 – WMS);
- ◆ other relevant books and journals that are identified in the course of the data collection;
- ◆ a site walkover of the entire PDA and environs. This was undertaken on 14 March 2014 by Dr Victoria Park.

### 4.3 Impact Assessment Methodology

In order to address the significance of, and impact on, heritage assets as required by the NPPF, this study adopts the methodology for assessing predicted changes to the historic environment from the guidance provided in the Highways Agency’s (DfT 2007) *Design Manual for Roads and Bridges* (DMIRB). This methodology was designed for the assessment of impacts on heritage assets resulting from road construction, but it is also a useful approach to the assessment of other development schemes. The methodology was developed in consultation with the key historic environment stakeholders in the UK, including English Heritage, Historic Scotland, Cadw, The Environment and Heritage Service of Northern Ireland, and the Institute for Archaeologists (IfA). The methodology has also been adapted to take cognisance of more recent guidance concerning assessment of significance and impacts to setting (English Heritage 2008; English Heritage 2011).

The methodology identifies three cultural heritage ‘sub-topics’, each with its own assessment methodology: Archaeological Remains, Historic Buildings and Historic Landscape. The Historic Buildings and Historic Landscapes methodologies are used here.



4.3.1 The Value of Heritage Assets

In order to assess the significance of the different magnitudes of change resulting from the proposed development, the above factors have to be weighed against the value of each cultural heritage asset. This 'value' is broadly equivalent to an asset's *significance* in National Planning Policy Framework (NPPF) terminology, but the term 'value' has been retained here in order that this is not confused with the *significance of effects* which is discussed in section 4.3.3 below. In addition 'heritage values' were also assessed in accordance with the guidance contained within *Conservation Principles* (English Heritage 2008). The different criteria used for establishing the value for each type of asset within this study are tabulated in Tables 1 and 2 below:

Table 1: Factors for assessing the value of Historic Buildings

Value	Criteria
Very High	<ul style="list-style-type: none"> <li>♦ Standing structures inscribed as of universal importance as World Heritage Sites</li> <li>♦ Other buildings of recognised international importance</li> </ul>
High	<ul style="list-style-type: none"> <li>♦ Scheduled Monuments with standing remains</li> <li>♦ Grade I and Grade II* Listed Buildings</li> <li>♦ Other listed buildings that can be shown to have exceptional qualities in their fabric or historical association</li> <li>♦ Conservation Areas containing very important buildings</li> <li>♦ Undesignated structures of clear national importance</li> </ul>
Medium	<ul style="list-style-type: none"> <li>♦ Grade II Listed Buildings</li> <li>♦ Historic unlisted buildings that can be shown to have exceptional qualities in their fabric or historical associations</li> <li>♦ Conservation Areas containing buildings that contribute significantly to its historic character</li> <li>♦ Historic Townscape or built-up areas with important historic integrity in their buildings, or built set tings (e.g. including Street furniture and other structures)</li> </ul>
Low	<ul style="list-style-type: none"> <li>♦ 'Locally Listed' buildings</li> <li>♦ Historic (unlisted) buildings of modest quality in their fabric or historical association</li> <li>♦ Historic Townscape or built-up areas of limited historic integrity in their buildings, or built settings (e.g. including Street furniture and other structures)</li> </ul>
Negligible	<ul style="list-style-type: none"> <li>♦ Buildings of no architectural or historical note; buildings of an intrusive character</li> </ul>
Unknown	<ul style="list-style-type: none"> <li>♦ Buildings with some hidden (i.e. inaccessible) potential for historical significance</li> </ul>

Table 2: Evaluating Historic Landscape Character

Value	Criteria
Very High	<ul style="list-style-type: none"> <li>♦ World Heritage Sites inscribed for their historic landscape qualities</li> <li>♦ Historic landscapes of international value, whether designated or not</li> <li>♦ Extremely well preserved historic landscapes with exceptional coherence, time-depth, or other critical factor(s)</li> </ul>
High	<ul style="list-style-type: none"> <li>♦ Designated historic landscapes of outstanding interest</li> </ul>



4.3.2 The Magnitude of Change

The scale and magnitude of change to cultural heritage assets can be assessed using the five tier grading system presented below in Tables 3 and 4:

Table 3: Factors in the assessment of the magnitude of change for Historic Buildings

Magnitude	Description
Major	<ul style="list-style-type: none"> <li>♦ Changes to key historic building elements such that the resource is totally altered</li> <li>♦ Comprehensive changes to setting, resulting in a loss of significance.</li> </ul>
Moderate	<ul style="list-style-type: none"> <li>♦ Changes to many key historic building elements, such that the resource is significantly modified</li> <li>♦ Changes to the setting of an historic building, such that it is significantly modified, resulting in a loss of significance.</li> </ul>
Minor	<ul style="list-style-type: none"> <li>♦ Changes to key historic building elements, such that the asset is slightly different</li> <li>♦ Changes to the setting of an historic building, such that it is noticeably changed, resulting in a loss of significance.</li> </ul>
Negligible	<ul style="list-style-type: none"> <li>♦ Slight changes to historic building elements or setting that hardly affect it.</li> </ul>
No Change	<ul style="list-style-type: none"> <li>♦ No change to fabric or setting.</li> </ul>

4.3.2 The Magnitude of Change

The scale and magnitude of change to cultural heritage assets can be assessed using the five tier grading system presented below in Tables 3 and 4:

Table 3: Factors in the assessment of the magnitude of change for Historic Buildings

Magnitude	Description
Major	<ul style="list-style-type: none"> <li>♦ Changes to key historic building elements such that the resource is totally altered</li> <li>♦ Comprehensive changes to setting, resulting in a loss of significance.</li> </ul>
Moderate	<ul style="list-style-type: none"> <li>♦ Changes to many key historic building elements, such that the resource is significantly modified</li> <li>♦ Changes to the setting of an historic building, such that it is significantly modified, resulting in a loss of significance.</li> </ul>
Minor	<ul style="list-style-type: none"> <li>♦ Changes to key historic building elements, such that the asset is slightly different</li> <li>♦ Changes to the setting of an historic building, such that it is noticeably changed, resulting in a loss of significance.</li> </ul>
Negligible	<ul style="list-style-type: none"> <li>♦ Slight changes to historic building elements or setting that hardly affect it.</li> </ul>
No Change	<ul style="list-style-type: none"> <li>♦ No change to fabric or setting.</li> </ul>

Table 4: Factors in the assessment of the magnitude of change for Historic Landscapes

Magnitude	Description
Major	<ul style="list-style-type: none"> <li>♦ Change to most or all key historic landscape elements, parcels or components</li> <li>♦ Extreme visual effects</li> <li>♦ Gross change of noise or change to sound quality</li> <li>♦ Fundamental changes to use or access: Resulting in total change to historic landscape character unit.</li> </ul>



Magnitude	Description
Moderate	<ul style="list-style-type: none"> <li>Changes to many key historic landscape elements, parcels or components</li> <li>Visual change to many key aspects of the historic landscape</li> <li>Noticeable differences in noise or sound quality</li> <li>Considerable changes to use or access:</li> <li>Resulting in moderate changes to historic landscape character.</li> </ul>
Minor	<ul style="list-style-type: none"> <li>Changes to few key historic landscape elements, parcels or components</li> <li>slight visual changes to few key aspects of historic landscape</li> <li>limited changes to noise levels or sound quality</li> <li>slight changes to use or access:</li> <li>Resulting in limited changes to historic landscape character.</li> </ul>
Negligible	<ul style="list-style-type: none"> <li>Very minor changes to key historic landscape elements, parcels or components</li> <li>Virtually unchanged visual effects</li> <li>Very slight changes in noise levels or sound quality</li> <li>Very slight changes to use or access:</li> <li>Resulting in a very small change to historic landscape character.</li> </ul>
No Change	<ul style="list-style-type: none"> <li>Very minor changes to key historic landscape elements, parcels or components</li> <li>No visual or audible changes</li> <li>No changes arising from amenity or community factors.</li> </ul>

#### 4.3.3 The Significance of Effects

Using the *magnitude of change* as ascertained from Tables 1-2, and the assessment of *value* as indicated by Tables 3-4, Table 5 below indicates how an assessment of the *significance of effects* of the development proposals is reached.

**Table 5: Significance of effects matrix**

VALUE/SENSITIVITY	MAGNITUDE OF CHANGE				
	No change	Negligible	Minor	Moderate	Major
Very High	Neutral	Slight	Moderate or Large	Large or Very Large	Very Large
High	Neutral	Slight	Moderate or Slight	Moderate or Large	Large or Very Large
Medium	Neutral	Neutral or Slight	Slight	Moderate	Moderate or Large
Low	Neutral	Neutral or Slight	Neutral or Slight	Slight	Slight or Moderate
Negligible	Neutral	Neutral	Neutral or Slight	Neutral or Slight	Slight

## 5 IMPACT ASSESSMENT

### 5.1 Heritage Assets

#### 5.1.1.1 On-Site

There are no Listed Buildings, or extant built heritage within the PDA.

#### 5.1.1.2 Off-Site

Within the 1km study area lie 39 Listed Buildings, the majority of which fall within the Bodicote Conservation Area (Table 6). Thirty eight of these are listed Grade II, while one, St John the Baptist Church, is Grade II\*. Those within the 1km study area are listed in Table 6, and are illustrated on Figure 3. Further details about the assets can be found in Appendix 1.

Table 6: Listed Buildings within the 1km study area

NHLE No.	Name	Grade	Grid Reference
1046877	WYKHAM FARMHOUSE	II	SP 45093 38274
1200199	HORTON GENERAL HOSPITAL, MAIN ENTRANCE BLOCK FRONTING OXFORD ROAD	II	SP 45562 39575
1248370	BRISTOW HOUSE	II	SP 46080 37982
1248383	HERBAL HOUSE	II	SP 46036 37671
1248384	WESTWAY	II	SP 46036 37655
1248385	HOLLY TREE COTTAGE	II	SP 46033 37601
1248386	BAKER'S ARMS PUBLIC HOUSE	II	SP 46010 37724
1248399	HEADSTONE APPROXIMATELY 15 METRES SOUTH EAST OF PORCH OF CHURCH OF ST JOHN THE B	II	SP 46011 37661
1248400	HEADSTONE DATED 169? APPROXIMATELY 15 METRES SOUTH OF PORCH OF CHURCH OF ST JOHN	II	SP 45999 37657
1248401	ROW OF 3 HEADSTONES APPROXIMATELY 5 METRES SOUTH OF PORCH OF CHURCH OF ST JOHN T	II	SP 45994 37663
1248674	HEADSTONE TO LEFT OF HEADSTONE DATED 169? APPROXIMATELY 15 METRES SOUTH OF PORCH	II	SP 45999 37654
1248677	DRAYCOT HOUSE	II	SP 46116 37766
1248678	CORNER COTTAGE	II	
1248679	OLD BARN HOUSE	II	SP 45921 37782
1248702	TOWN FURLONG FARMHOUSE	II	SP 45932 37710
1248703	BODICOTE HOUSE	II	SP 46159 38071
1248705	BROWN THATCH	II	SP 46040 38095



NHLE No.	Name	Grade	Grid Reference
1248707	IVY COTTAGE	II	SP 46049 38039
1248721	THE OLD BAKERY	II	SP 46011 37929
1248722	THE CLOSE	II	SP 46010 37919
1248723	THE HERMITAGE	II	SP 46032 37769
1248732	YEW TREE COTTAGE	II	SP 46011 38071
1248733	43, HIGH STREET	II	SP 45978 37983
1248734	17 AND 19, HIGH STREET	II	SP 45989 37851
1248735	REAPER'S COTTAGE	II	SP 45991 37843
1248740	THE OVEN	II	SP 46003 37742
1248741	THE PADDOCKS	II	SP 45951 37942
1249071	BODICOTE MILL	II	SP 45900 37200
1277760	GOOSE COTTAGE	II	SP 45993 37830
1277764	THE OLD FARMHOUSE	II	SP 45964 37975
1277765	WALL TO FRONT AND TO LEFT OF THE PADDOCKS	II	SP 45952 37945
1277788	LODGE TO BODICOTE HOUSE	II	SP 46054 38184
1277795	WALL TO IVY COTTAGE FRONTING HIGH STREET	II	SP 46041 38028
1277796	12, HIGH STREET	II	SP 46008 37857
1277815	WEST HOUSE	II	SP 46104 37763
1277816	FARM PLACE	II	SP 46073 37732
1277817	OLD BARN COTTAGE	II	SP 45922 37752
1277943	GARDEN WALL TO NORTH AND SOUTH OF BRISTOW HOUSE	II	SP 46066 37989
1277948	CHURCH OF ST JOHN THE BAPTIST	II*	SP 45993 37683

The DBA in 2013 (Clarke 2013) also identified three non-designated built heritage assets within the 1km study area. These lie within the Conservation Area, but away from the PDA, and so are discussed as part of that section.

Using the criteria set out in Tables 1-4 in the methodology, as Listed Buildings, the heritage assets presented in the above table are of **high** (Grade I and II\*) and **medium** (Grade II) **value**. The Bodicote Conservation Area, which incorporates many of the Listed Buildings (Figure 4), is an area of **high to medium value**.

In addition to the built heritage outlined above, two archaeological features identified to the west of the PDA have the potential to have their wider setting impacted upon: the Neolithic causewayed enclosure (MOX4460) on Wykham Farm, and two Bronze Age Round Barrows (MOX12183) which were identified through



crop marks on aerial photographs in the 20<sup>th</sup> century. These are non-designated, but have the potential to contribute to regional, or in the case of the putative causewayed enclosure national, research objectives and therefore can be considered of **medium to high value**, although their condition of preservation is unknown and because they survive only as cropmarks they have all been evidently highly truncated by later land use.

The potential impacts on the setting of the above heritage assets were the focus of the site visit. Throughout the discussion, assets are referred to by their NHLE number or, if they are non-designated, by their local HER number.

## 5.2 Site Visit and Discussion of Setting

The previous DBA (Clarke 2013) identified that the proposed development would have the potential to impact on the setting of the Bodicote Conservation Area, and in particular on six of the listed buildings within it: Old Barn House (NHLE 1248679), Old Barn Cottage (NHLE 1277817), The Paddocks and Associated Walls (NHLE 1248741 and NHLE 1277765), Brown Thatch (NHLE 1248705) and the Church of St John the Baptist (NHLE 1277948). These Listed Buildings lie to the western and northern edges of the Conservation Area.

In addition to the buildings within Conservation Area, there are two other buildings within the 1km study area which have the potential to be affected by the PDA. The Lodge to Bodicote House (NHLE 1277788) lies immediately to the north of the Conservation Area, and Wykham Farmhouse (NHLE 1046877) lies to the west of the PDA and was identified by Clarke (2013) as having clear views towards the site.

Within the wider local area lie The Church of St Mary in Adderbury to the south-east of Bodicote and the Church of St Mary in Bloxham to the south-west of the PDA (Figure 5). These are both Grade I listed, and have views across the area.

A site visit was undertaken on 20 March 2014 by Victoria Park. The visit included walking over the PDA, as well as into the Conservation Area, and viewing the PDA to and from the locations of the Listed Buildings. Selected photographs are included in Appendix 2 (Figures 6 to 25).

### Bodicote Village Listed buildings.

The site walkover confirmed the compact nature of the village of Bodicote. This means that many of the Listed Buildings have restricted views to the surrounding areas, and their setting is largely focused within the village (Figures 6 and 7). Therefore, the proposed development would have **no change** to the setting of the majority of the Listed Buildings within the village. The setting of the Conservation Area as a whole, which contains these buildings, is discussed later in this section. The Listed Buildings to the north and west of the village which have been identified as having the potential to be affected are discussed below.



*Church of St John the Baptist (NHLE 1277948)*

The Church of St John the Baptist lies to the west of the main road (Figure 8), towards the southern part of the Conservation Area, and is included as part of the 'Main Road' area in the Conservation Area appraisal (Cherwell District Council 2008). The earliest parts of the church date to the 13<sup>th</sup> century, but it was largely rebuilt in 1843-4 and this was when the new tower was built.

Only the top of the church tower is visible from the proposed development area (and therefore from the existing houses in Banbury). From a distance, the tower lies amongst the village rooftops and trees (Figure 21). Bodicote Conservation Area appraisal notes that 'the vistas across the valley towards Bloxham as seen from the churchyard...are very beautiful and give an impression of the topographical position of the village overlooking Sor Brook'. This vista (to the south-west) would be unchanged by the proposed development (to the north-west). Additionally, as the site visit established, the views towards the PDA from the church and churchyard are obscured by other existing buildings and foliage (Figure 9). It was not possible during the site visit to assess the views from the top of the church tower towards the PDA. It is likely that it would be visible, however, as will be discussed in more detail later, the views north-west towards the PDA already take in modern housing at the edge of Banbury and the proposed development would only serve to bring this closer.

It can therefore be concluded that the proposed development would have **negligible to no change** on the setting of the Church of St John the Baptist.

*Old Barn House (NHLE 1248679) and Old Barn Cottage (NHLE 1277817), Goose Lane*  
Goose Lane lies at the western side of the village, in an area referred to as the 'Village Lanes' within the Conservation Area appraisal (Cherwell District Council 2008). Old Barn House lies at the north-west corner of Goose Lane (Figures 10 and 11), while the Grade II Listed Old Barn Cottage lies just south of this.

Bodicote Conservation Area appraisal comments that 'houses at the far ends of these lanes [Goose Lane, Malthouse Lane and Paddock Farm Lane] enjoy beautiful views across the valley to Bloxham'. Bloxham lies to the south-west, away from the PDA.

While it was not possible to fully access the rear of the properties to establish the exact views towards the PDA, it was possible to get a glance from Goose Lane across the driveway and garden of Old Barn House towards the PDA (Figure 10). This indicated that the main views from the back of the house are out to the west, rather than north-west. As Figure 12 demonstrates, only the south-western part of the PDA has the potential to be visible from the end of Goose Lane, due to more modern buildings along Wykhham Lane and at the ends of Malthouse Lane and Paddock Farm Lane shielding part of the PDA.

The views back towards to Goose Lane from the PDA (Figure 21) also suggest that while there may be a degree of visibility between the two, it would be minimal due



to the distance and extensive tree cover, which, although it will vary seasonally, will still offer a degree of screening.

The Conservation Area appraisal highlights that the key views from Goose Lane are from the High Street west down the lane towards Old Barn House (which is noted as being 'particularly impressive'), and at the end of Goose Lane looking west-south-west. As Figure 10 shows, the view down Goose Lane will not be affected and the overall feel of the lane with its narrow, compact nature would not be impacted upon.

It can therefore be concluded that the proposed development would have **negligible to no change** on the setting of Old Barn House and Old Barn Cottage on Goose Lane.

*The Paddocks and Wall to front, Paddock Farm Lane (NHLE 1248741 and 1277765)*

As with Goose Lane above, Paddock Farm Lane (Figure 13) is also grouped within the 'Village Lanes' area in the Conservation Area appraisal, and the comments above relating to views out to the west and across to Bloxham apply.

There are a number of Listed Buildings on this street, however the compact nature of the lane, and the surrounding buildings means that only two Grade II Listed Buildings (The Paddock, and the Wall to its front) were identified in the DBA (Clarke 2013) as having the potential to be impacted upon. This was confirmed during the site visit. The Paddock and associated Wall lie part way down the south side of Paddock Farm Lane (Figure 14). While most of the views from the building lie into Paddock Farm Lane, it should be noted that the building is three stories high, and therefore the views from the third story window may be more extensive and incorporate some of the PDA.

In addition to the general views at the end of all the lanes at this end of the village, the Conservation Area appraisal also notes that there is a 'positive' view looking north-west down the lane from the High Street. As was the case with Goose Lane, and as Figure 11 shows, the view down Paddock Farm Lane will not be affected. The overall feel of the lane given its narrow, compact nature would not be impacted upon. The houses at the very end of Paddock Farm Lane are modern and not listed (Figure 15). These buildings lie outside the Bodicote Conservation Area and also have the effect of shielding the Listed Buildings from the PDA (Figures 20 and 21).

It can therefore be concluded that the proposed development would have **negligible to no change** on the setting of The Paddocks and associated Wall.

*Brown Thatch and (NHLE 1248705) and Lodge to Bodicote House (NHLE 1277788)*

Brown Thatch lies at the very north of the Bodicote Conservation Area, while the Lodge to Bodicote House lies just outside, to the north, along White Post Lane. These two Listed Buildings lie to the east of the PDA, but are separated from it by land at the cricket ground and recreation ground.



The Bodicote Conservation Area appraisal already notes that the view from Brown Thatch north-west towards the PDA is a negative view (Figure 15), whereas the view slightly further to the north, down White Post Road, is noted as being positive. The site walkover confirmed this (Figures 15 and 16). While the view directly north incorporates some of the PDA, this is largely screened by trees at the present time, and only a small portion of the eastern part of the PDA would be visible. Therefore, the current setting of the building would be largely unaffected by the proposed development.

The Lodge to Bodicote House is outside the Conservation Area, to the east of White Post Road. The lodge currently lies at the edge of Cherwell District Council offices and adjacent to Bishop Loveday Church of England Primary School. Although it is shielded by heavy tree cover, these nearby buildings have already impacted on its setting. The view west from the Lodge to Bodicote House is directly towards the PDA (Figure 18), and so it would be visible. However, the cricket ground and recreation ground would remain as a buffer of open space between the two.

It can therefore be concluded that the proposed development would have **negligible to no change** on the setting of Brown Thatch and the Lodge to Bodicote House.

#### *Bodicote Conservation Area*

Several of the key parts of the Conservation Area which have the potential to be affected have been discussed in the above subsections. There are, however, a number of additional points that the Conservation Area appraisal highlights which also need to be addressed.

The Conservation Area appraisal notes a number key positive views within the village. The above discussion has established that many of these will be unaffected by the proposed development due to their containment within the village or focus out from the village in other directions. One positive view noted in the appraisal that has not already been addressed is west down Wykhams Lane. The site walkover determined that the view west from the top of this lane incorporates more recent housing to the north, which would largely shield the view from the PDA, while the views of open fields are concentrated to the south and would be unaffected.

As already touched upon, the ends of two of the cul-de-sacs, Paddock Farm Lane, and Malthouse Lane, at the west of the village, are only partially within the Conservation Area. Their far ends consist of a small number of more modern dwellings, which lie outside the Conservation Area. This has the effect of surrounding the village at this side so that the historic core of the village is largely shielded from this side (Figure 21).

The 'rural' character of the village and Conservation Area is highlighted several times within the Conservation Area appraisal. It states that *'the fields around Bodicote have many well-used paths and are key to views into and out of the village'* and *'in particular those [footpaths] which cross the fields to the west and south, are key to maintaining the village's rural character'*. As already discussed in the above sections,



the key views out south and west of the village towards Bloxham will be unaffected by the development.

It was noted in the Conservation Area appraisal that planned developments at the north-east of the village meant that it was *'important to retain the rural setting of the village to the south and west'* and that *'it is key that Bodicote retains its identity as a village and does not merge completely with Banbury to the north'*. In considering the impact of the proposed development the current nature of the land between Bodicote and Banbury needs to be taken into consideration. The views towards Banbury from Bodicote already contain a substantial amount of modern housing (Figure 19) and the proposed development would serve to bring this closer to Bodicote. However, the cricket ground and recreational grounds would still exist and mean that the gap between Bodicote and Banbury is not closed at this edge. There would still be open spaces. In particular, the proposed development would only stretch half way along White Post Road, the main road between Bodicote and Banbury, and there would still be a sense of divide between the town and village.

An additional factor to consider is the large cricket pavilion which lies at the inside edge of the proposed development (Figures 2 and 20). This large structure, and associated landscaping at the recreation ground, means that the majority of the eastern and northern parts of the PDA are screened from the Bodicote Conservation Area.

It can therefore be concluded that the proposed development would have a **minor** change to the Bodicote Conservation Area.

#### *Wykhams Farmhouse (NHLE 1046877)*

Outside of the main concentration of Bodicote Village and the Conservation Area lies the Grade II listed Wykhams Farmhouse (Figure 22). It lies to the west of the PDA.

The main views from the farmhouse (from the front elevation) are to the south and do not take in the PDA. From the PDA, although the side of the farmhouse is slightly visible, at the present time tree coverage provides a level of screening (Figures 22 and 23). There are also more modern barn conversions both immediately to the east of the Listed Building (within its yard), and also mid-way to the PDA (Figure 24). These reinforce the point made above that the key views are to the south. Although there will be some impact on the view up the driveway to the farmhouse, a substantial amount of land will still separate it from the PDA. The proposed development will have a negligible impact on the current setting of the Listed Building.

It can therefore be concluded that the proposed development would have a **negligible** change to the setting of Wykhams Farmhouse

*Neolithic Causewayed Enclosure and Bronze Age Round Barrows*



The Neolithic causewayed enclosure (MOX4460) on Wykham Farm and two Bronze Age Round Barrows (MOX12183) were identified through crop marks on aerial photographs taken in the 20<sup>th</sup> century and only survive as heavily truncated buried remains with no surface expression. These are non-designated, but have the potential to contribute to regional or national research objectives and therefore can be considered of medium to high value. Their current location to the west of the PDA means that the sites themselves will be physically unaffected by the proposed development, however there is a potential, as noted by Clarke (2013), that associated archaeological assets which may contribute to the understanding and significance of these monuments may lie under the PDA. As a result, the proposed development has the potential to have a **minor to moderate** change to their setting. The current lack of understanding of the nature, condition and significance of these monuments, could be resolved by limited evaluation trenching.

#### *Church of St Mary in Adderbury, and the Church of St Mary in Bloxham*

Clarke's 2013 DBA established that the Church of St Mary in Adderbury was not visible from the proposed development area, and the site walkover confirmed this. The spire of the Church of St Mary in Bloxham is visible from the PDA (Figure 25), however it is at a considerable distance (c.3.5km). Due to its location, the immediate views of the church, and from the churchyard towards the PDA, would be unaffected. Any potential wider views which incorporate the church and the area towards the PDA would also take in Banbury and the modern houses which lie immediately to the north of the PDA. Therefore, the proposed development would have a **negligible** change to the Church of St Mary in Bloxham.

### 5.3 Conclusion

The above data, site visit and discussion have established that there are built heritage assets of **medium to high value** in the surrounding area, with the Bodicote Conservation Area being of **medium value**.

Using the matrix shown in Table 5, the impact on the proposed development on the setting of these assets is considered to be largely **neutral or slight** (Table 7). Only the potential effect on the Neolithic Causewayed Enclosure and Bronze Age Barrows is considered to be **slight to moderate**, but further archaeological investigation would be needed to establish this.

Table 7: Summary of significance of effects

Asset	Value of asset	Magnitude of Impact	Significance of Effects
Bodicote Conservation Area	Medium	Negligible to Minor	Neutral or slight
Church of St John the Baptist	High	Negligible to no change	Neutral or slight
Old Barn House	Medium	None	Neutral
Old Barn Cottage	Medium	None	Neutral



Asset	Value of asset	Magnitude of Impact	Significance of Effects
The Paddocks and associated Wall	Medium	None	Neutral
Brown Thatch Lodge to Bodicote House	Medium	Negligible to no change	Neutral or slight
Wykham Farmhouse	Medium	Negligible to no change	Neutral or slight
Neolithic Causewayed Enclosure and Bronze Age round barrows	Medium to high	Minor to moderate	Slight to moderate
Church of St Mary in Bloxham	High	Negligible	Slight

### 6 IMPACT STATEMENT

Drawing upon results from the previous DBA and the current investigation, this study considers the impact of the proposed development on the setting of heritage assets, as required in the NPPF and Planning Policy Guidance, to be a neutral to slight adverse effect. Therefore, the effect constitutes "less than substantial harm", and is not considered grounds for the refusal of consent under the National Planning Policy Framework (para. 133).

### 7 STATEMENTS AND ACKNOWLEDGEMENTS

#### 7.1 Publicity, Confidentiality and Copyright

Any publicity will be handled by the client. Archaeological Research Services Ltd will retain the copyright of all documentary and photographic material under the Copyright, Designs and Patent Act (1988).

#### 7.2 Statement of Indemnity

All statements and opinions contained within this report arising from the works undertaken are offered in good faith and compiled according to professional standards. No responsibility can be accepted by the author/s of the report for any errors of fact or opinion resulting from data supplied by any third party, or for loss or other consequence arising from decisions or actions made upon the basis of facts or opinions expressed in any such report(s), howsoever such facts and opinions may have been derived.

#### 7.3 Acknowledgements

Archaeological Research Services Ltd would like thank Gladman Developments Ltd for commissioning the work. We would also like to thank the landowners for allowing access for the site walkover.



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## Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire

NHLE No.	HER ID	Name	Grade	Grid Reference
1248674	MOX13700	Headstone To Left Of Headstone Dated 169? Approximately 15 Metres South Of Porch Of Church St John The Baptist, Church Street	II	SP 45999 37654
1248400	MOX13698	Headstone Dated 169? Approximately 15 Metres South Of Porch Of Church Of St John The Baptist, Church Street	II	SP 45999 37657
		Late 19Th Century Agricultural Building	II	SP 46000 38300
1248740	MOX14522	No 1 (The Oven), High Street	II	SP 46003 37742
1277796	MOX12960	No 12 High Street	II	SP 46008 37857
1248386	MOX13696	Baker's Arms Public House, Church Street	II	SP 46010 37724
1248722	MOX14733	The Close, High Street	II	SP 46010 37919
1248399	MOX13697	Headstone Approximately 15 Metres South East Of Porch Of Church Of St John The Baptist, Church Street	II	SP 46011 37661
1248721	MOX13705	The Old Bakery, High Street	II	SP 46011 37929
1248732	MOX13707	Yew Tree Cottage, High Street	II	SP 46022 38035
1248723	MOX13706	The Hermitage, High Street	II	SP 46032 37769
1248385	MOX13695	Holly Tree Cottage & No 18, Church Street	II	SP 46033 37601
1248384	MOX14732	Westway, Church Street	II	SP 46036 37655
1248383	MOX13694	No 8 (Herbal House), Church Street	II	SP 46036 37671
	MOX4408	Wesleyan Methodist Chapel, East Street	II	SP 46040 37720
1277788	MOX13018	Lodge To Bodicote House, High Street	II	SP 46040 38094
1277795	MOX13745	Wall To Ivy Cottage Fronting High Street	II	SP 46041 38028
1248707	MOX13704	Ivy Cottage, High Street	II	SP 46049 38039
1248705	MOX13703	Brown Thatch, High Street	II	SP 46049 38057



## Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire

## APPENDIX 1: GAZETTEER OF BUILT HERITAGE

NHLE No.	HER ID	Name	Grade	Grid Reference
1046877	MOX13248	Wykham Farmhouse, Bodicote Road, Wykham	II	SP 45093 38274
	MOX4395	Upper Grove Mill Upper Grove Mill	II	SP 45300 37140
1200199	MOX14681	Horton General Hospital, Main Entrance Block Fronting Oxford Road	II	SP 45562 39575
		Two Late 19Th Century Buildings Associated With The Vicarage	II	SP 45600 38200
1249071	MOX4396	Bodicote Mill	II	SP 45860 37220
1248679	MOX14519	Old Barn House, Goose Lane	II	SP 45921 37782
1277817	MOX14267	Old Barn Cottage, Goose Lane	II	SP 45922 37752
1248702	MOX14520	Town Furlong Farmhouse, Goose Lane	II	SP 45932 37710
1248741	MOX13711	The Paddocks Paddock Farm Lane	II	SP 45951 37942
1277765	MOX14531	Wall To Front And To Left Of The Paddocks, Paddock Farm Lane	II	SP 45955 37961
1277764	MOX13016	The Old Farmhouse, Paddock Farm Lane	II	SP 45964 37975
1248733	MOX13708	No 43, High Street	II	SP 45978 37983
1248734	MOX13709	Nos 17 And 19, High Street	II	SP 45989 37851
1277948	MOX4423	St John The Baptist Church, Church Street	II*	SP 45990 37680
1248735	MOX13710	Reaper'S Cottage, High Street	II	SP 45991 37843
1277760	MOX12959	No 13 & Goose Cottage, High Street	II	SP 45993 37830
1248401	MOX13699	Row Of 3 Headstones Approximately 5 Metres South Of Porch Of Church Of St John The Baptist, Church Street	II	SP 45994 37663



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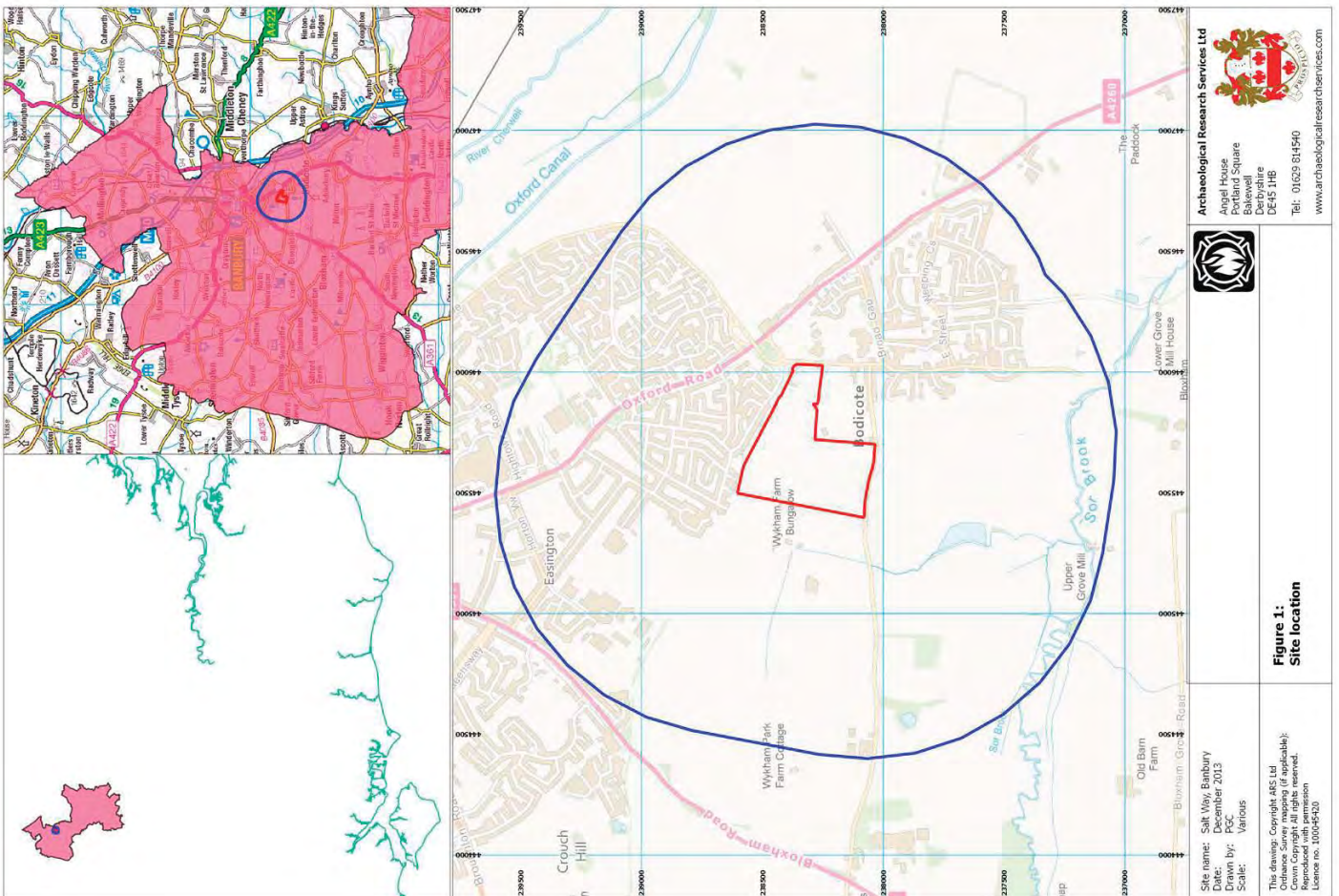
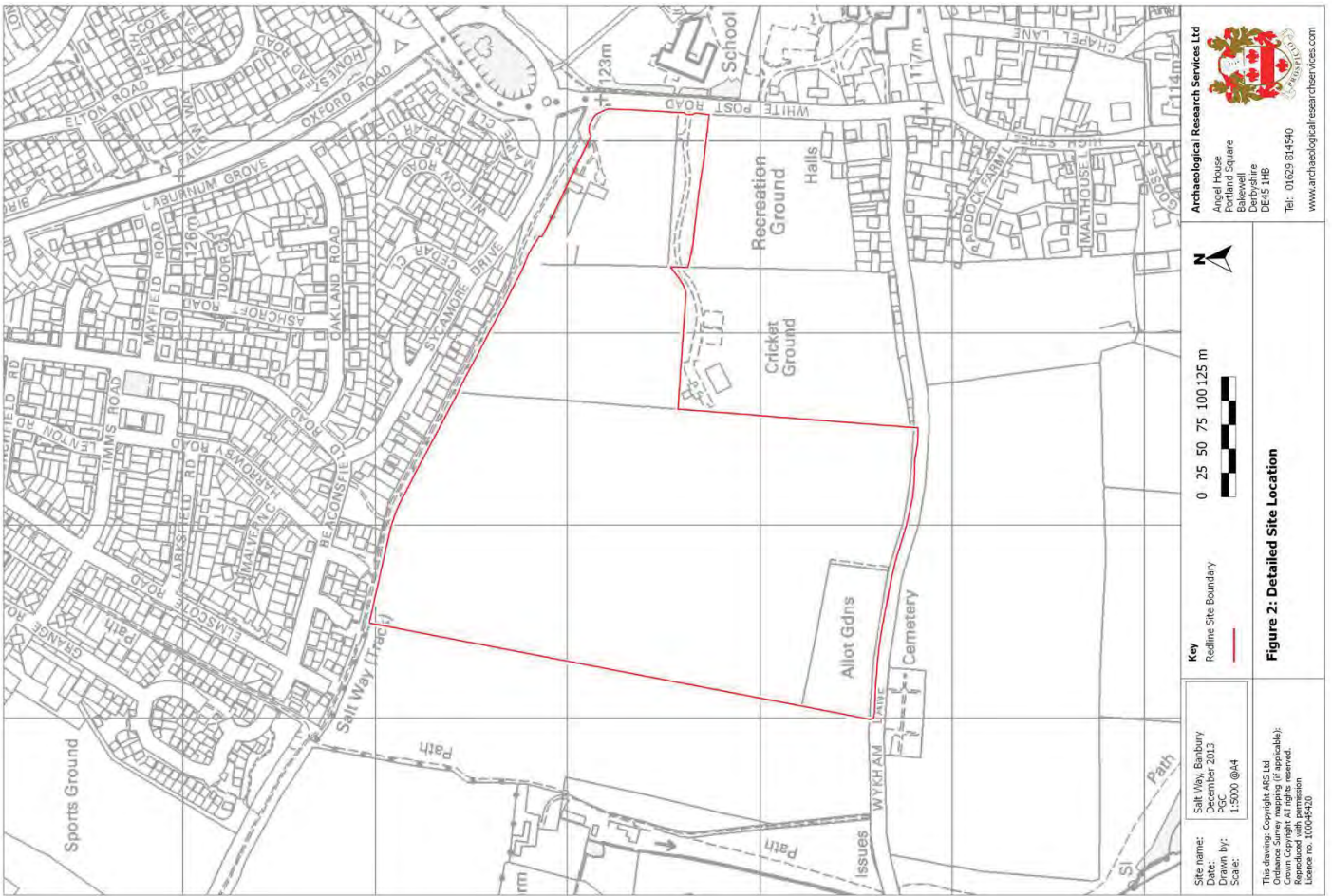
*Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire*

NHLE No.	HER ID	Name	Grade	Grid Reference
1277943	MOX12962	Garden Wall To North And South Of Bristow House, Chapel Lane	II	SP 46066 37989
1277816	MOX12961	Nos 2, 3 4, & 5 (Farm Place), East Street	II	SP 46073 37732
1248370	MOX14731	Bristow House, Chapel Lane	II	SP 46080 37982
1277815	MOX13746	West House, East Street	II	SP 46104 37763
1248677	MOX13701	Draycot House, East Street	II	SP 46116 37766
1248678	MOX13702	Corner Cottage And Thatch Stone, East Street	II	SP 46141 37765
1248703	MOX14521	Bodicote House, High Street	II	SP 46159 38071

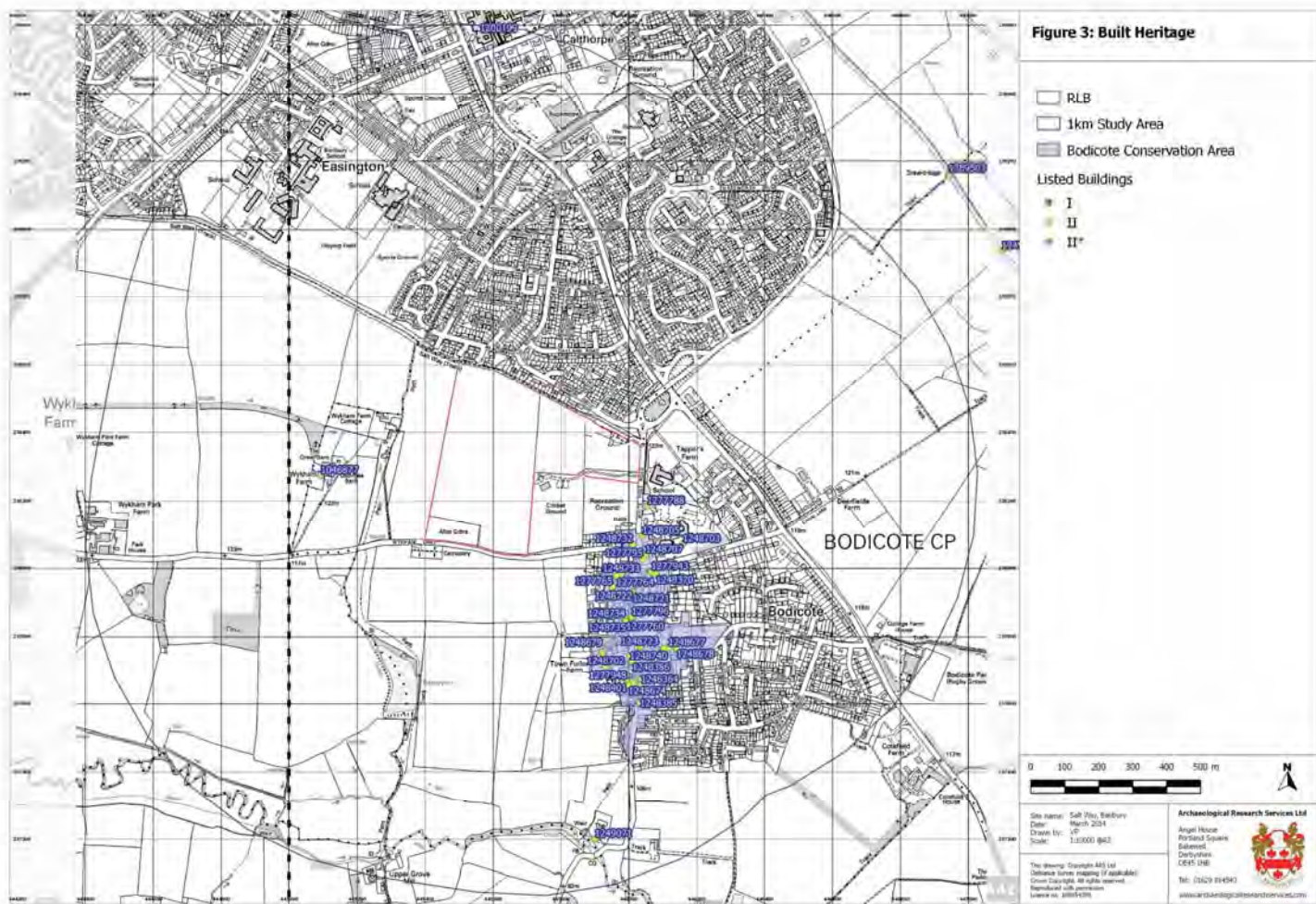


**APPENDIX 2: FIGURES**

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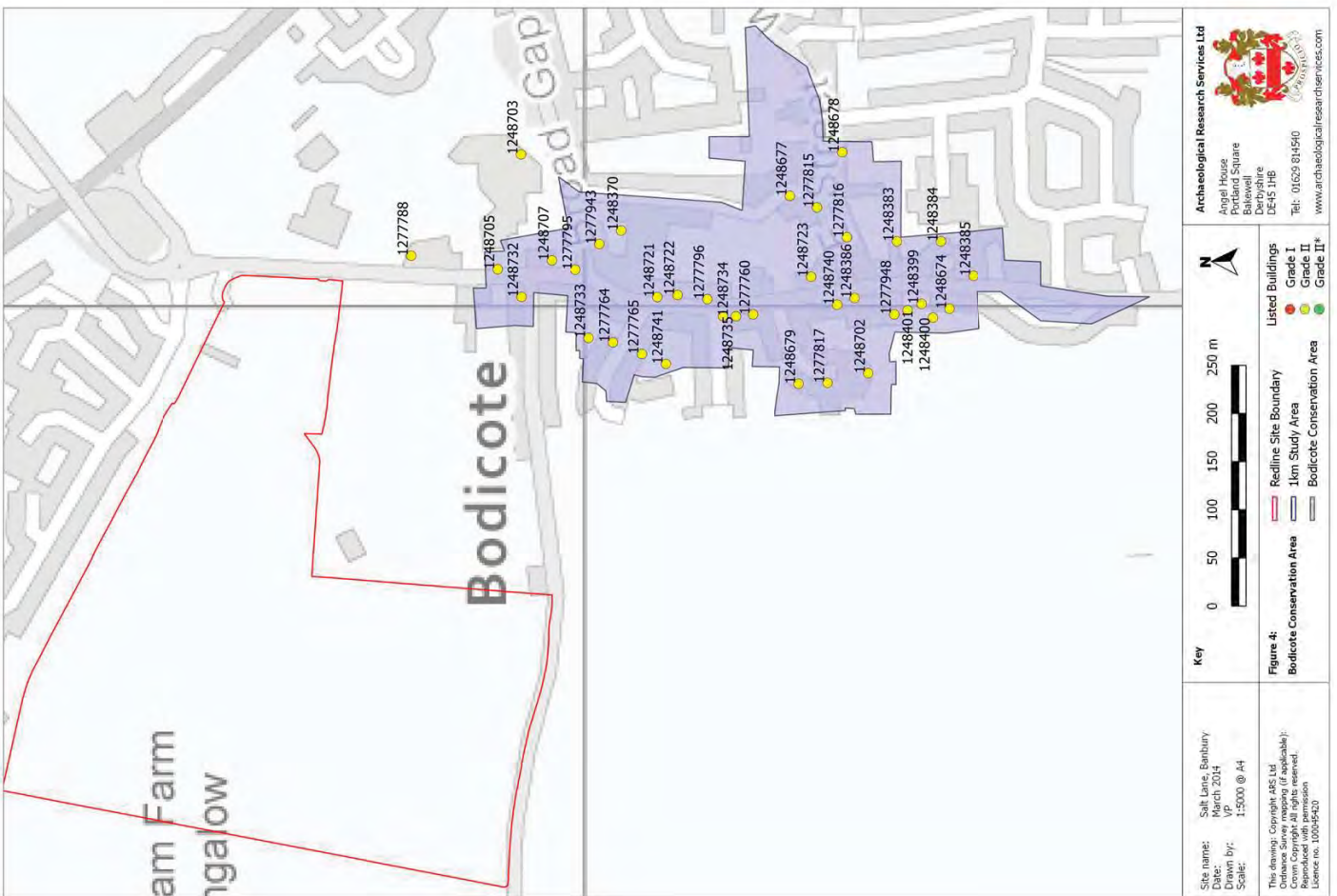
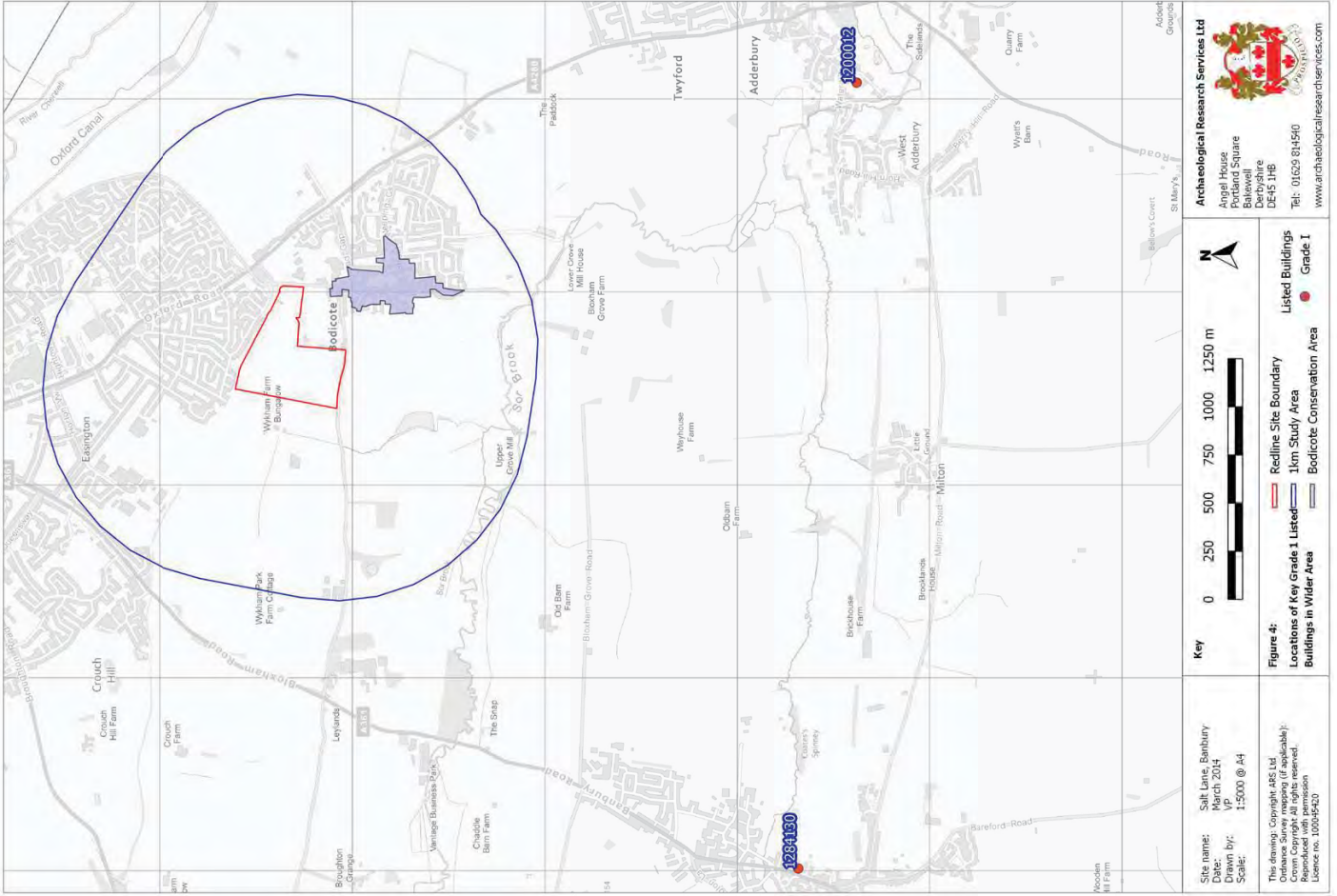




Figure 6: Bodicote High Street, view south

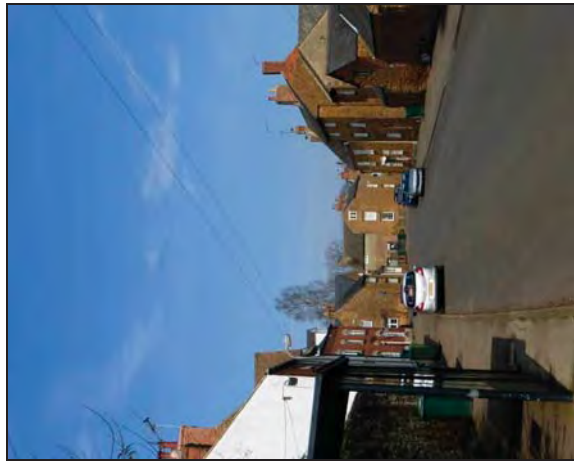


Figure 7: Bodicote High Street, view north



Figure 8: Grade II\* Church of St John the Baptist, Bodicote, looking north-west



Figure 9: View north-west from the rear of the Church of St John the Baptist. The PDA lies to the right, behind the visible buildings and foliage.



Figure 10: Goose Lane, looking west towards Old Barn House



Figure 11: Grade II Listed Old Barn House, looking north-west



Figure 12.: View north-west through drive and garden of Old Barn House, Goose Lane towards PDA



Figure 13: Paddock Farm Lane, looking west





Figure 14: The Paddocks and associated wall, looking east



Figure 15: End of Paddock Farm Lane, looking north-west towards proposed development area

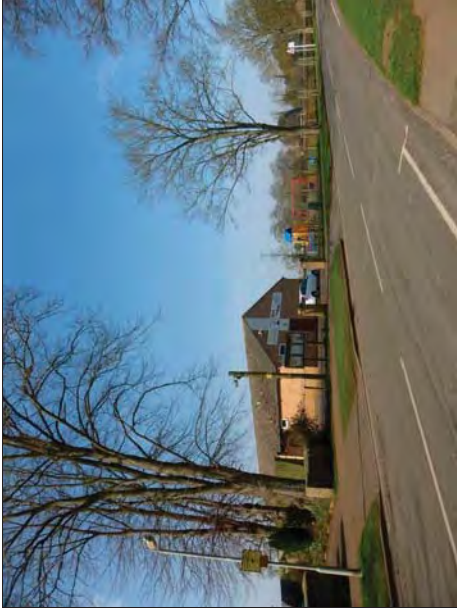


Figure 15: View from Brown Thatch, Bodicote, looking north-west towards PDA

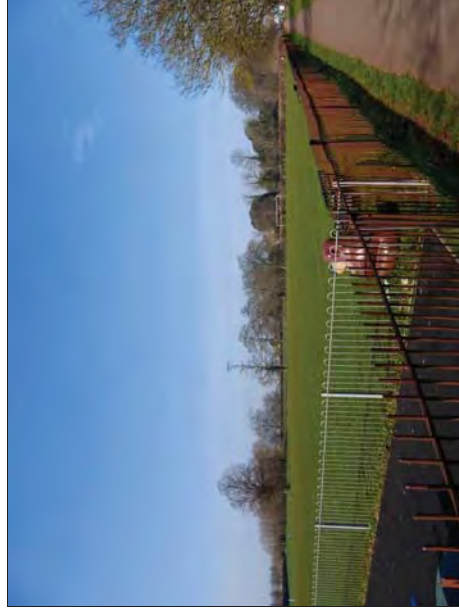


Figure 16: View along White Post Road, Bodicote, looking north-west towards PDA

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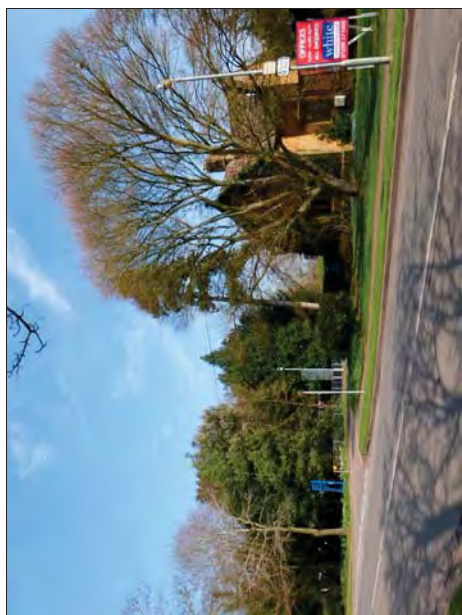


Figure 17: View north-east across White Post Road to the Lodge to Bodicote House

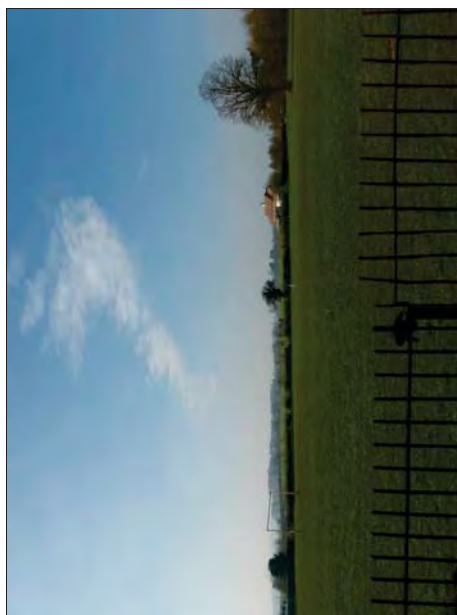


Figure 18: View from the Lodge to Bodicote House, looking east towards PDA

*Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire*



Figure 20: View south-east across the PDA towards Bodicote Village, with Banbury Cricket Pavillion in the foreground

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Figure 19: Panorama looking north from the southern edge of PDA (adjacent to Wykham Lane) towards Banbury (Cricket Club Pavillion to the east)

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*Heritage Statement for Land South of Salt Way, Banbury, Oxfordshire*



Figure 21: Annotated Bodicote Village from southern edge of PDA, adjacent to Wykam Road, showing modern housing along Wykam Road.



Figure 22: View towards Wykham Farmhouse from Wykham Lane

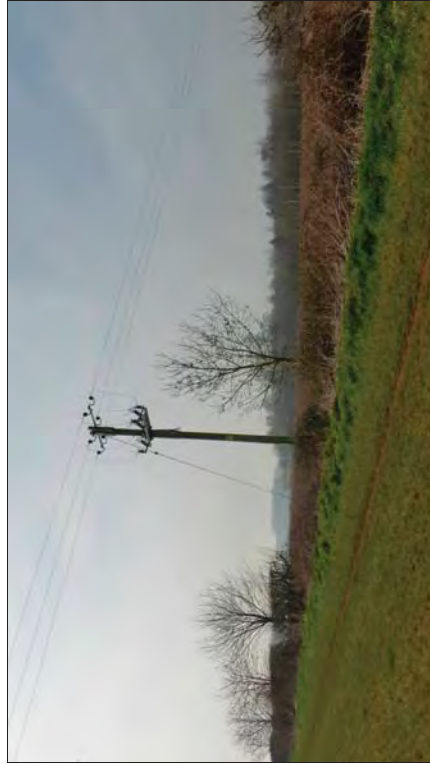


Figure 23: View south-west towards Wykham Farmhouse from PDA

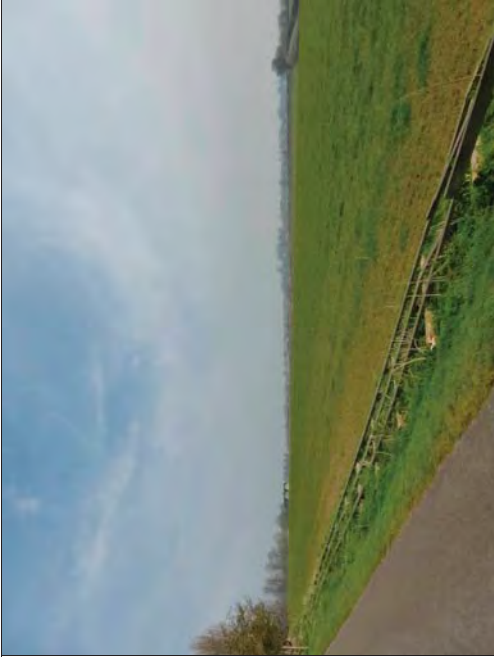


Figure 24: View north-east from lane at side of Wykham Farmhouse towards the PDA with modern barn conversions to left

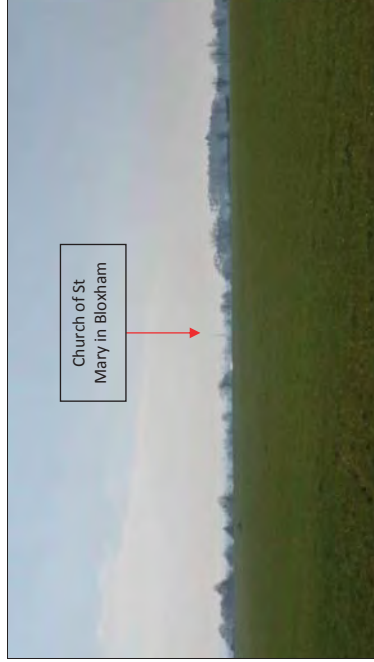


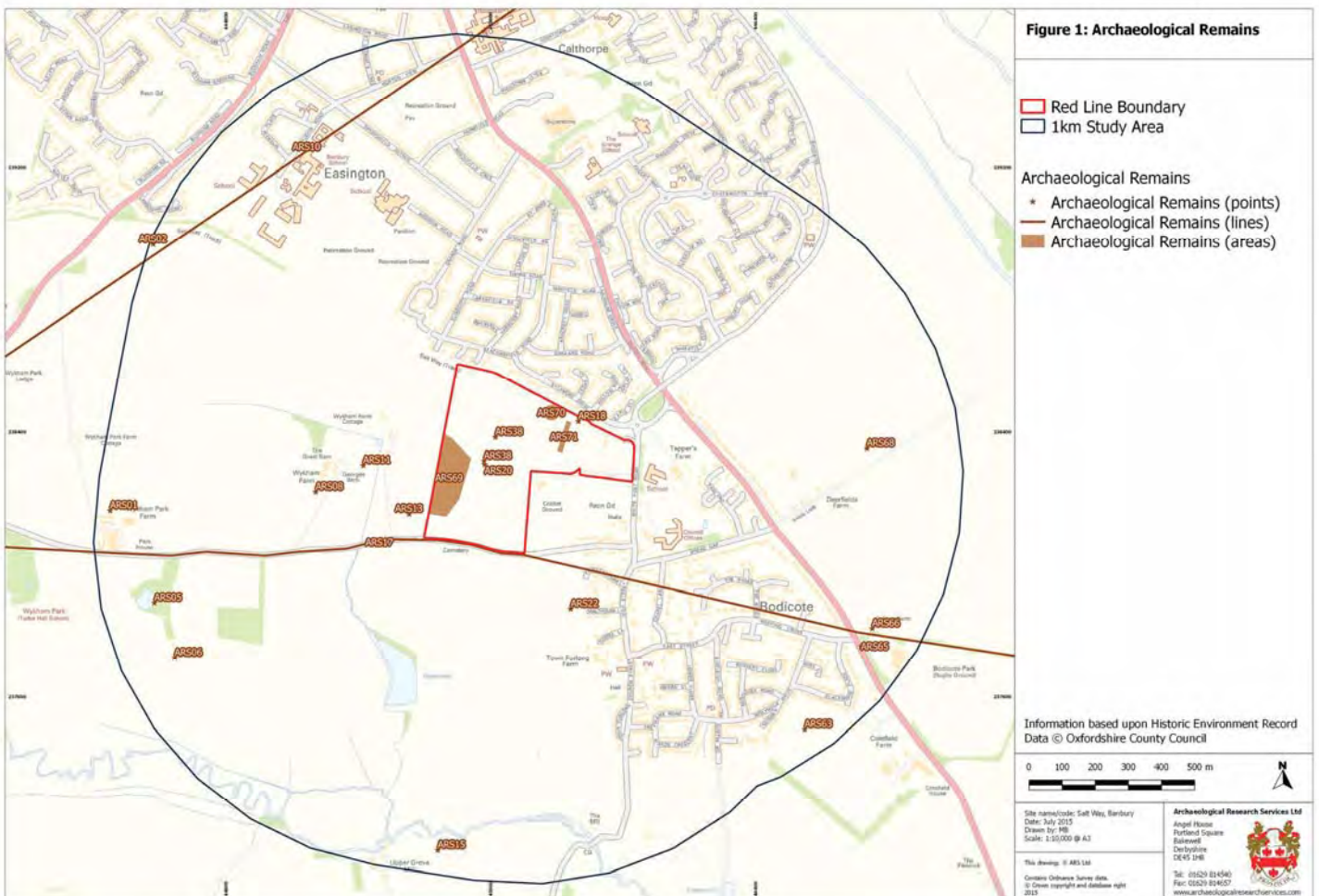
Figure 25: View north-east from lane at side of Wykham Farmhouse towards the PDA with modern barn conversions to left

## **A7 Historic Environment**

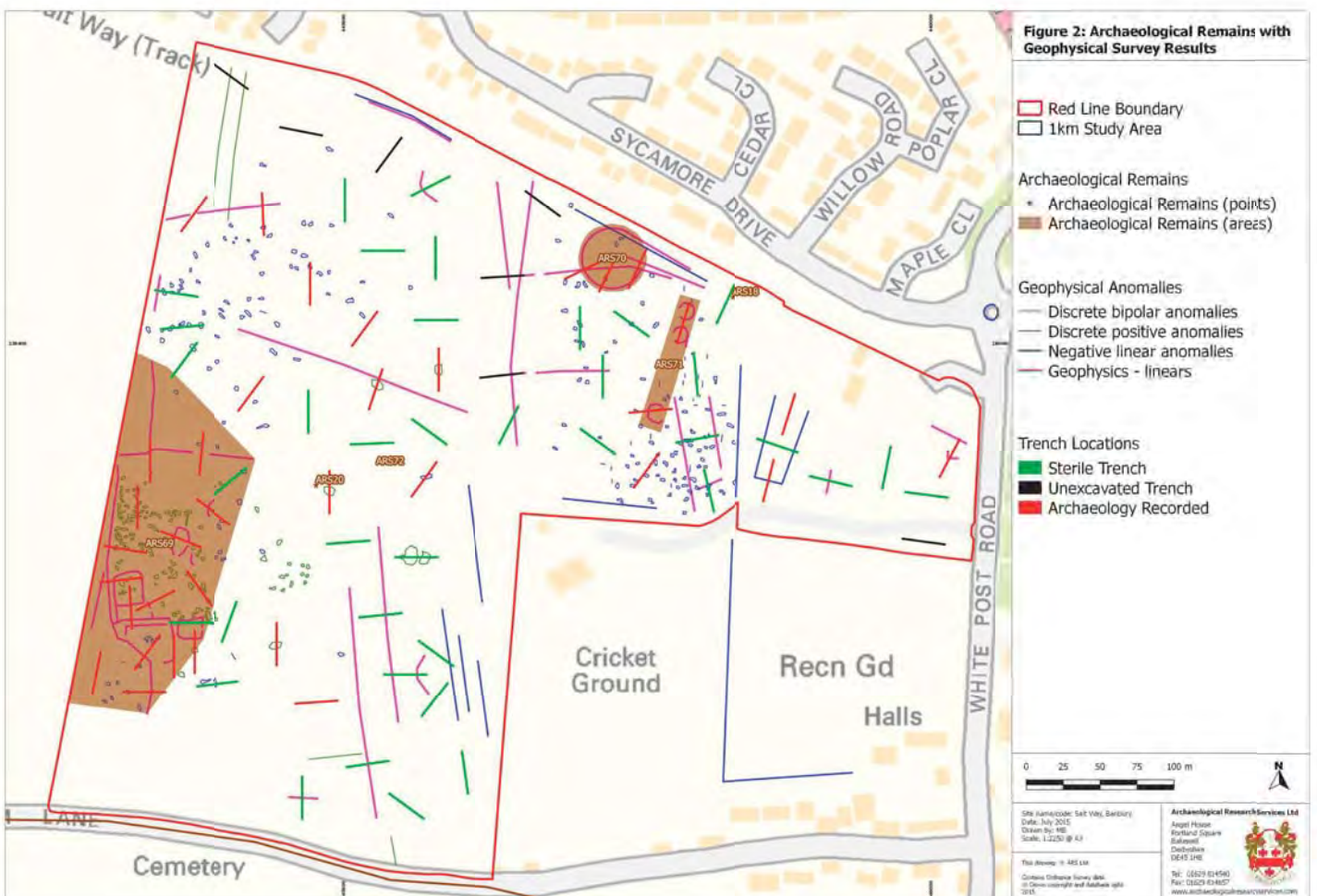
### Appendix 7.5 Historic Environment Figures

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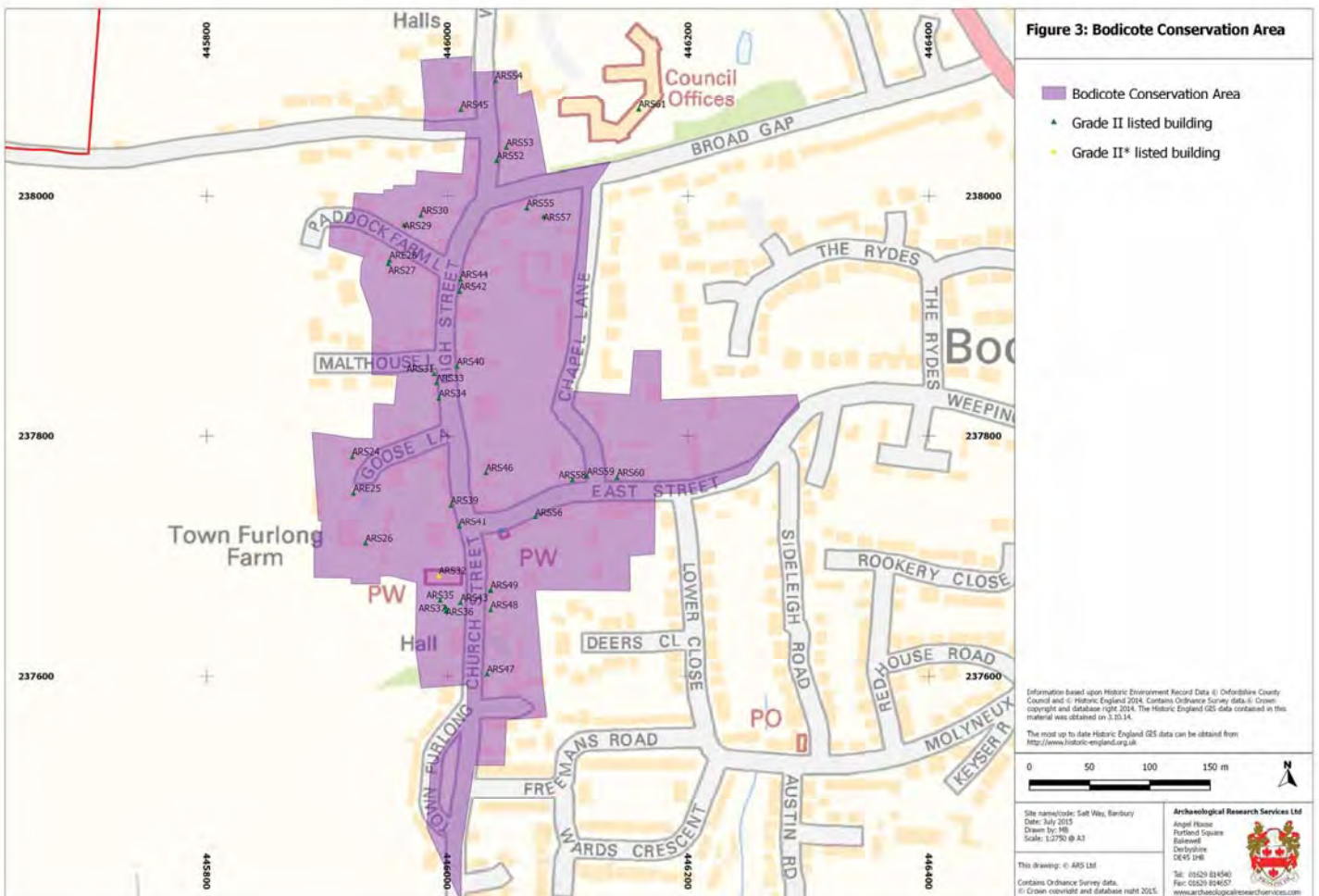


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## **A8 Traffic and Transport**

### Appendix 8.1 Transport Assessment

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**Transport Assessment**  
**Land West of White Post Road, Banbury**

Land West of  
White Post Road,  
Banbury

Client: Gladman Developments Ltd  
Report Ref: 136177/B  
Status: Final  
Date: July 2015

**Transport Assessment**

Report Prepared for  
Gladman  
Developments Ltd

July 2015  
Report Reference 136177/B



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# Transport Assessment

## Land West of White Post Road, Banbury

### 1 Introduction

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B Traffic Flows	
C Technical Filenote 1C	
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E Parking Survey	
F College Fields/Bankside Junction Improvements	
<b>Drawings</b>	
Dig No 1361/21	Proposed Site Access Arrangements
Dig No 1361/12/A	Indicative Site Access Roundabout
Dig No 1361/02-09 & 16-20	TA Study Junction Existing Arrangements

- 1.1 Ashley Helme Associates Limited (AHA) are appointed by Gladman Developments Ltd to prepare a Transport Assessment (TA) report to support the planning application for the residential development on land off White Post Road, Banbury (henceforth referred to as the Site). The local highway authority is Oxfordshire County Council (OCC). The location of the Site in the context of the local highway network is indicated on Figure 1.
- 1.2 The proposed development comprises the construction of up to 280 houses. The Site is presently vacant agricultural land.
- 1.3 The transport policy context for the proposed development is outlined in Chapter 2. The principles of the access strategy adopted for the proposed development are also discussed in Chapter 2, and this provides the means to achieve transport policy objectives. It is fundamental to the approach of the applicant, as represented in this TA, that a holistic view is taken of the consideration of access to the proposed development by all modes of transport.
- 1.4 The issues addressed within the TA fall broadly into the following areas:
- Accessibility by non-car modes, and
  - The vehicular traffic impact on the operational performance of the local highway network, assessed quantitatively for the TA defined study network.
- 1.5 The local highway network is described in Chapter 3. The proposed Site access arrangements are outlined in Chapter 4.
- 1.6 The transport sustainability of the proposed development is a key issue, as set out in NPPF. Accessibility issues are identified in Chapter 2, and an accessibility appraisal of the Site by non-car modes is presented in Chapters 5 (Walk & Cycle) and 6 (Public Transport), using an accessibility mapping methodology.

- 1.7 A Travel Plan forms a key element of the development proposal and the principles of this are outlined in Chapter 7. A summary indication is included in Chapter 7 of the Action Plan for the Travel Plan. This includes measures that are to be implemented prior to first occupation of the development, as well as subsequent and ongoing measures/initiatives. A separate Travel Plan document is submitted as part of the planning application, and is complementary to the TA report.
- 1.8 The estimation of the development generated traffic and associated With Development traffic flows is presented in Chapter 8. Modelling of the impact of development traffic on the highway network is described in Chapter 9.
- 1.9 The application Site forms part of a wider allocation for residential development in the Council's Submission Local Plan, which is currently awaiting the outcome of its examination. The application site and other residential schemes along Bloxham Road are known collectively as the Banbury 17 sites. OCC highways officers have requested that the TA considers the traffic implications of the development proposal coming forward in conjunction with the other Banbury 17 residential sites. This is set out in Chapter 10.
- 1.10 The conclusions of the TA are presented in Chapter 11.

## 2 Policies & Principles of Access Strategy

- 2.1 A holistic approach is adopted for the desired access strategy. Due cognisance is taken of a range of relevant policy/guidance documents and considerations that represent national/local policies and good practice. These include:
- National Planning Policy Framework (NPPF), March 2012,
  - Planning Practice Guidance (PPG), March 2014,
  - Emerging Cherwell Local Plan 2011-2031,
  - Cherwell Local Plan, 1996,
  - Oxfordshire Local Transport Plan 2011-2030,
  - Emerging Oxfordshire LTP4.
- 2.2 A general thrust of current national and local policies is to promote and deliver sustainable transport objectives, and this is a key factor in defining the access strategy for the proposed development.
- 2.3 There are a range of documents that provide advice and guidance identifying that the historic approach of adopting rigid highway design standards and considering this in isolation is not appropriate or desirable in today's world. This includes, for example, the Urban Design Compendium (UDC) and Manual for Streets (MfS) and the associated recent Manual for Streets 2 (MfS2).
- 2.4 **NPPF: CORE LAND-USE PLANNING PRINCIPLE**
- The Government's commitment to sustainable development is emphasised in NPPF. In respect of transport related issues, this includes the core land-use planning principle to:
- "actively manage patterns of growth to make the fullest possible use of public transport, walking and cycling, and focus significant development in locations which are or can be made sustainable" (para 17).*
- The proposed development respects and reflects this NPPF transport sustainability related objective.

## 2.5 NPPF: PROMOTING SUSTAINABLE TRANSPORT

2.5.1 NPPF sets out that:

"Transport policies have an important role to play in facilitating sustainable development but also in contributing to wider sustainability and health objectives. Smarter use of technologies can reduce the need to travel. The transport system needs to be balanced in favour of sustainable transport modes, giving people a real choice about how they travel." (para 29) , and identifies that

"...local planning authorities should therefore support a pattern of development which, where reasonable to do so, facilitates the use of sustainable modes of transport." (para 30).

2.5.2 NPPF further highlights that:

"Plans and decisions should take account of whether:

- the opportunities for sustainable transport modes have been taken up depending on the nature and location of the site, to reduce the need for major transport infrastructure;
- safe and suitable access to the site can be achieved for all people; and
- improvements can be undertaken within the transport network that cost effectively limits the transport impacts of the development. Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe." (para 32)

2.5.3 National policy guidance is provided about the transport aspect of the location and design of developments:

- "Therefore, developments should be located and designed where practical to accommodate the efficient delivery of goods and supplies;

- give priority to pedestrian and cycle movements, and have access to high quality public transport facilities;
- create safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians, avoiding street clutter and where appropriate establishing home zones;
- incorporate facilities for charging plug-in and other low emission vehicles; and
- consider the needs of people with disabilities by all modes of transport." (para 35)

2.5.4 The development proposal reflects and respects as appropriate all of the above.

## 2.6 PPG

2.6.1 The Department for Communities and Local Government (DCLG) launched the new planning practice guidance (PPG) web-based resource on 6 March 2014. The PPG includes advice on when transport assessments and transport statements are required, and what they should contain.

2.6.2 The PPG states that:

"Travel Plans, Transport Assessments and Statements can positively contribute to:

- encouraging sustainable travel;
- lessening traffic generation and its detrimental impacts;
- reducing carbon emissions and climate impacts;
- creating accessible, connected, inclusive communities;
- improving health outcomes and quality of life;
- improving road safety; and
- reducing the need for new development to increase existing road capacity or provide new roads."

2.6.3 With respect to Transport Assessments and Statements, PPG sets out that:  
 "The key issues to consider at the start of preparing a Transport Assessment or Statement may include:

- the planning context of the development proposal;
- appropriate study parameters (i.e. area, scope and duration of study);
- assessment of public transport capacity, walking/ cycling capacity and road network capacity;
- road trip generation and trip distribution methodologies and/ or assumptions about the development proposal;
- measures to promote sustainable travel;
- safety implications of development; and
- mitigation measures (where applicable) – including scope and implementation strategy."

2.6.4 With respect to Travel Plans, PPG sets out that:

"Travel Plans should set explicit outcomes rather than just identify processes to be followed (such as encouraging active travel or supporting the use of low emission vehicles). They should address all journeys resulting from a proposed development by anyone who may need to visit or stay and they should seek to fit in with wider strategies for transport in the area.

They should evaluate and consider:

- benchmark travel data including trip generation databases;
- information concerning the nature of the proposed development and the forecast level of trips by all modes of transport likely to be associated with the development;
- relevant information about existing travel habits in the surrounding area;
- proposals to reduce the need for travel to and from the site via all modes of transport; and
- provision of improved public transport services."

2.7 **EMERGING CHERWELL LOCAL PLAN 2011-2031**

2.7.1 The emerging Cherwell Local Plan was submitted to the Secretary of State for Communities and Local Government for formal examination on January 2014. In June 2014 the Inspector suspended the examination to allow the Council to undertake

main modifications in relation to meeting its objectively assessed housing needs over the plan period. The Council has since undertaken modifications along with further public consultation and has re-submitted these to the Inspector in October 2014. The Inspector's Report on Examination of the modified submission has been published and council officers are considering the report. It is intended that it will be presented to Members at meeting of the Council on 20 July 2015 with a recommendation for adoption.

2.7.2 Relevant policies include the following:

- Draft Policy SLE4: Improved Transport and Connections

2.7.3 Draft Policy SLE 4 sets out the council's aim for improved transport and connections in Cherwell. The draft policy states:

"The Council will support the implementation of the proposals in the Movement Strategies and the Local Transport Plan to deliver key connections, to support modal shift and to support more sustainable locations for employment and housing growth. We will support key transport proposals including:

- Transport Improvements at Banbury and Bicester and at Former RAF Upper Heyford in accordance with the County Council's Local Transport Plan and Movement Studies Strategies.
- Projects associated with East-West rail including new stations at Bicester Town and Water Eaton
- Rail freight associated development at Graven Hill, Bicester.
- Improvements to M40 junctions."

2.7.4 Para C.126 addresses the Council's strategy for Banbury and includes the following statement:

"Provide for new development in accessible locations which will provide good opportunities for improving and accessing public transport services, for delivering and using new cycleways, for travelling on foot and for minimising the impact on the highway network and traffic congestion."

- 2.8 **CHERWELL LOCAL PLAN, 1996**
- 2.8.1 The Cherwell Local Plan was adopted in November 1996. This document contains the most current adopted planning policies for Cherwell. There are a number of 'saved' transportation policies from the Cherwell Local Plan.
- 2.8.2 Saved policy TR1 sets out the Council's position with regard to accessibility to public transport for new developments. Policy TR1 states that:
- "Before proposals for development are permitted the council will require to be satisfied that new highways, highway improvement works, traffic-management measures, additional public transport facilities or other transport measures that would be required as a consequence of allowing the development to proceed will be provided."*
- 2.9 **OXFORDSHIRE LOCAL TRANSPORT PLAN 2011-2030**
- 2.9.1 The Oxfordshire Local Transport Plan (LTP) was adopted in 2011 and is the third adopted LTP. The LTP has been subject to two subsequent adopted amendments in 2012 and 2014. This document remains the most current LTP for Oxfordshire but the fourth LTP is currently in preparation, this is discussed in para 2.10.
- 2.9.2 Oxfordshire County Council (OCC) is the Local Highway Authority, and has responsibility for the development and delivery of the Local Transport Plan (LTP).
- 2.9.3 LTP3 sets out four local transport goals, as follows:
- to support the local economy and the growth and competitiveness of the county;
  - to make it easier to get around the county and improve access to jobs and services for all by offering real choice;
  - to reduce the impact of transport on the environment and help tackle climate change; and
  - to promote healthy, safe and sustainable travel.

- 2.9.4 The underlying theme and objectives of the third LTP are to promote policies and measures to foster and achieve improved opportunities for travel choices by non-car modes. This provides the context for specific local measures to be considered, promoted and introduced.
- 2.10 **EMERGING OXFORDSHIRE LTP4**
- 2.10.1 Following the adoption of the third LTP in 2011, there have been a number of amendments to policy. OCC notes that in order:
- "To ensure that the county's transport systems are fit to support population and economic growth, in 2014/15 we have developed a new Local Transport Plan, that will give Oxfordshire the best chance of success when bidding for projects and securing new infrastructure to support new development."*
- 2.10.2 OCC is in the process of developing the new LTP4 and expects to finalise the document for approval in summer 2015.
- 2.10.3 Chapter 5 of the emerging LTP4 sets out enhancements to the road network to support growth and economic vitality. Para 85 addresses local routes in Banbury and specifically notes the link road between A4260 Oxford Road and A361 Chipping Norton Road. Para 85 states:
- "The Banbury Area Strategy includes a new link road between the town and a large employment site to be developed east of M40 junction 11. A spine road to be built as part of development at Salt Way south of the town will link the A4260 Oxford Road and A361 Chipping Norton Road."*
- 2.10.4 The link road between A4260 Oxford Road and A361 Chipping Norton Road is also detailed in the emerging LTP4 Volume 2 Part II: Area Strategies. A420 Route Strategy and Freight Strategy. The additional infrastructure improvements are highlighted in Para 12, stating:
- "Additional infrastructure improvements will be delivered to support future regeneration of Banbury and the Local Plan Modifications development proposals:"*



- A361 Bloxham Road to A4260 Oxford Road Spine Road through the residential development south of Salt Way: The co-ordinated approach to development to the south of Banbury as proposed in the Local Plan Modifications (August 2014), will enable provision of essential infrastructure including delivery of an east-west link from the A361 Bloxham Road to join White Post Road and the A4260 Oxford Road. This road will support operation of commercially viable bus services through the development, increasing accessibility and long term sustainability of the development. The spine road will be built by the developer."
- 2.10.5 BAN1 in the emerging LTP4 notes the council will seek improvements to support regeneration and growth. BAN1 refers to the link road connection, stating:

"We will seek opportunities to deliver transport schemes which will support the regeneration and growth of Banbury to 2031 and protect the historically sensitive areas of the town through:

  - Provision of A361 Bloxham Road to A4260 Oxford Road Spine Road."
- 2.11 **PRINCIPLES OF THE ACCESS STRATEGY**
- 2.11.1 The access strategy for the development provides the means to achieve the identified policy objectives by optimising the opportunity for access to/from the Site by non-car modes. This is in accordance with all local, regional and national policies.
- 2.11.2 The accessibility of the Site for those travelling on foot and by cycle is reviewed in Chapter 5. The current accessibility of the Site by public transport is outlined in Chapter 6 herein. The proposed development takes account of the needs of the mobility impaired.
- 2.11.3 The access strategy for the development is cohesive, reflecting the need to appropriately consider and enable provision for the movement of people and goods. This includes considering, inter alia:

  - Permeability of the Site from/connection to the surrounding locality, for all modes of transport, motorised and non-motorised, reflecting the principles and good practice set out in the national guidance in Manual for Streets (MfS & MfS2).

- Access/routing: for the external linkage to the Site. The corresponding internal access/routing details are to be addressed by Reserved Matters application(s).

2.12

**SUMMARY**

In summary, the development proposal respects and promotes the principles of transport sustainability, and is consistent with national, regional and local transport policy objectives.

### 3 Highway Network

3.1 The location of the Site is indicated on Figure 1 in the context of the local highway network.

3.2 The Site has frontage on White Post Road, which is public highway.

#### 3.3 Study Network

3.3.1 Figure 2 identifies the TA network of study junctions, comprising:

REF	JUNCTION	CONTROL
SJ1	Site Access/White Post Road	refer Chapter 4
SJ2	Bankside/ Oxford Rd N'bound Slips/White Post Rd/Sycamore Drive	roundabout
SJ2A	Oxford Road/Northbound Slips	priority
SJ3	Oxford Road On & Off Slips/Bankside	priority/r'bout
SJ3A	Oxford Road/Southbound Slips	priority
SJ4	Broad Gap/Oxford Road/Canal Road	priority
SJ5	Weeping Cross/Oxford Road	traffic signals
SJ6	Broad Gap/High Street	priority
SJ7	Wykham Lane/White Post Road/High Street	priority
SJ8	Hightown Road/Oxford Road/Horton View	traffic signals
SJ9	Upper Windsor Street/Oxford Road	traffic signals
SJ10	Bloxham Road/South Bar Street/Oxford Road	traffic signals
SJ11	High Street/South Bar Street/ West Bar Street/Horse Fair	roundabout
SJ12	Castle Street/North Bar Street/Warwick Road/Southham Road	traffic signals
SJ13	Swan Close Road/Upper Windsor Street	traffic signals
SJ14	Bridge Street/Windsor Street/Cherwell Street	traffic signals
SJ15	Cherwell Street/Hennef Way	roundabout.

3.3.2 The local highway authority Oxfordshire County Council (OCC) is responsible for TA study junctions SJ1-SJ15.

#### 3.4 Existing Junction Layout Drawings

The existing layout of the study junctions are presented on the following drawings:

REF	JUNCTION	DRAWING
SJ2	Bankside/ Oxford Rd N'bound Slips/White Post Rd/Sycamore Drive	1361/02
SJ2A	Oxford Road/Northbound Slips	1361/03
SJ3	Oxford Road On & Off Slips/Bankside	1361/04
SJ3A	Oxford Road/Southbound Slips	1361/05
SJ4	Broad Gap/Oxford Road/Canal Lane	1361/14
SJ5	Weeping Cross/Oxford Road	1361/15
SJ6	Broad Gap/High Street	1361/16
SJ7	Wykham Lane/White Post Road/High Street	1361/16
SJ8	Hightown Road/Oxford Road/Horton View	1361/17
SJ9	Upper Windsor Street/Oxford Road	1361/06
SJ10	Bloxham Road/South Bar Street/Oxford Road	1361/07
SJ11	High Street/South Bar Street/ West Bar Street/Horse Fair	1361/08
SJ12	Castle Street/North Bar Street/Warwick Road/Southham Road	1361/09
SJ13	Swan Close Road/Upper Windsor Street	1361/18
SJ14	Bridge Street/Windsor Street/Cherwell Street	1361/19
SJ15	Cherwell Street/Hennef Way	1361/20.

#### 3.5 White Post Road

3.5.1 White Post Road forms the eastern boundary of the Site. White Post Road is a single carriageway road with footway on both sides. The footway is separated from the carriageway by grass verge. Along the Site frontage, White Post Road is 7.3m wide and is subject to a 30mph speed limit. Street lighting is present and there is an absence of waiting restrictions.

3.5.2 White Post Road provides access to Bishop Loveday C of E Primary School, opposite the Site. It also provides access to Cherwell District Council offices and a playing field, in the vicinity of the Site, and to Banbury Cricket Club, through the Site. There is a pelican crossing on White Post Road at the southern end of the Site frontage. White Post Road provides access to Bodicote village to the south of the Site

#### 3.6 SJ1: Site Access/White Post Road

The proposed Site Access/White Post Road junction is described in Chapter 4.

- 3.7 **SJ2: Bankside/Oxford Road Northbound Slips/White Post Road/Sycamore Drive**
- 3.7.1 White Post Road forms a four arm roundabout junction with Sycamore Drive, Bankside, and the Oxford Road Northbound Slips.
- 3.7.2 The existing SJ2 junction arrangements are presented on Drg No 1361/02.
- 3.7.3 The Oxford Road Northbound Slips (on and off) are referred to as SJ2A. The slips comprise a simple taper diverge and a simple taper merge. Give way markings have been added to the taper merge, which has the effect of turning the merge into a priority controlled junction. There are cycle lanes marked on both sides of Oxford Road in the vicinity of SJ2A. The existing layout of SJ2A is shown on Drg 1361/03.
- 3.8 **SJ3: Oxford Road Southbound Slips/Bankside**
- 3.8.1 To the east of SJ2, Bankside forms a 'triangle' of priority controlled junctions with the Oxford Road Southbound Slips.
- 3.8.2 The existing SJ3 junction arrangements are presented on Drg No 1361/04.
- 3.8.3 Planning permission has been granted for a mixed use development to the south of Bankside, comprising 1070 dwellings, employment and leisure uses, and a primary school. As part of the permitted development, SJ3 will be converted to a 4-arm roundabout junction providing access to part of the permitted development site. The proposed roundabout arrangements are presented on Colin Buchanan Figure 59, Appendix F.
- 3.8.4 The Oxford Road Southbound Slips (on and off) are referred to as SJ3A. The slips comprise a simple taper diverge and a simple taper merge. Give way markings have been added to the taper merge, which has the effect of turning the merge into a priority controlled junction. There are cycle lanes marked on both sides of Oxford Road in the vicinity of SJ3A. The existing layout of SJ3A is shown on Drg 1361/05.

- 3.9 **SJ4: Broad Gap/Oxford Road/Canal Lane**
- 3.9.1 SJ4 is the priority controlled junction of Broad Gap and Oxford Road. Broad Gap forms the minor arm of the junction.
- 3.9.2 The existing SJ4 arrangements are presented on Drg No 1361/14.
- 3.10 **SJ5: Weeping Cross/Oxford Road**
- 3.10.1 To the south of SJ4, Oxford Road forms a traffic signal controlled staggered crossroads junction with Weeping Cross. The fourth arm is the recently introduced access to serve the College Fields residential scheme.
- 3.10.2 The Weeping Cross/Oxford Road junction includes assisted pedestrian crossings on all approaches. The existing SJ5 arrangements are presented on Drg No 1361/15.
- 3.11 **SJ6: Broad Gap/High Street**
- 3.11.1 SJ6 is the priority controlled junction of Broad Gap and High Street. Broad Gap forms the minor arm of the junction.
- 3.11.2 The existing SJ6 arrangements are presented on Drg No 1361/16.
- 3.12 **SJ7: Wykham Lane/White Post Road/High Street**
- To the north of SJ6, High Street forms a priority controlled 'T' junction with Wykham Lane. This is SJ7 and the existing arrangements are presented on Drg No 1361/16.
- 3.13 **SJ8: Hightown Road/Oxford Road/Horton View**
- 3.13.1 SJ8 is the traffic signal controlled staggered crossroads junction of Oxford Road and Hightown Road/Horton View. The existing junction layout is presented on Drg No 1361/17. There are 2no lanes on Oxford Road for northbound and southbound movements. Both Hightown Road and Horton View are marked with a single approach lane.

- 3.13.2 There are assisted pedestrian crossings on Oxford Road. There are dropped kerbs and tactile paving which provide unassisted crossing on Hightown Road and Horton View.
- 3.14 **SJ9: Upper Windsor Street/Oxford Road**
- 3.14.1 To the north of SJ8, Oxford Road forms a signal controlled T-junction with Upper Windsor Street. There is an assisted pedestrian crossing on the Upper Windsor Street arm of the junction, and dropped kerbs, tactile paving, and pedestrian refuges on the Oxford Road arms.
- 3.14.2 There is an uphill gradient on Oxford Road, from north to south, in the vicinity of SJ9. There are 'no stopping' restrictions on the Oxford Road (N) and Upper Windsor Street arms of the junction. There are waiting restrictions on all arms of the junction.
- 3.14.3 The existing SJ9 junction arrangements are presented on Drg No 1361/06.
- 3.15 **SJ10: Bloxham Road/South Bar Street/Oxford Road**
- 3.15.1 To the north of SJ9, Oxford Road forms a signal controlled T-junction with Bloxham Road and South Bar Street. There are assisted pedestrian crossing facilities on the South Bar Street arm of the junction, and dropped kerbs, tactile paving and a pedestrian refuge on the Bloxham Road arm of the junction.
- 3.15.2 There is an uphill gradient towards the junction on the South Bar Street arm, and a downhill gradient towards the junction on the Oxford Road and Bloxham Road arms of the junction. There are waiting restrictions on all arms of the junction and guard rail is present.
- 3.15.3 The existing SJ10 junction arrangements are presented on Drg No 1361/07.
- 3.16 **SJ11: High Street/South Bar Street/West Bar Street/Horse Fair**
- 3.16.1 SJ11 is the roundabout junction at 'Banbury Cross' and is situated to the north of SJ9. There are assisted crossing facilities on all arms of the junction. These comprise zebra crossing facilities on the High Street, South Bar Street and West Bar Street arms, and a pelican crossing on the Horse Fair arm of the junction.

- 3.16.2 There is guard rail on all arms of the junction, and the Banbury Cross monument is situated in the central roundabout island.
- 3.16.3 The existing SJ11 junction arrangements are presented on Drg No 1361/08.
- 3.17 **SJ12: Castle Street/North Bar Street/Warwick Road/Southam Road**
- 3.17.1 To the north of SJ11 is the signalised crossroads junction of Southam Road, Castle Street, North Bar Street and Warwick Road.
- 3.17.2 There are assisted crossings and waiting restrictions on all arms of the junction. Guard rail is present on the Castle Street arm of the junction.
- 3.17.3 The existing SJ12 arrangements are shown on Drg No 1361/09.
- 3.18 **SJ13: Swan Close Road/Upper Windsor Street**
- 3.18.1 SJ13 is a 'T' junction operating under traffic signal control. All arms of the junction are located within a 30mph speed limit zone.
- 3.18.2 There are 2no approach lanes marked on each entry of the junction. The existing lane allocation is shown on Drg No 1361/18. The junction includes pedestrian crossings on Upper Windsor Street (S), which are assisted (ie with push button) and Swan Close Road, which are unassisted.
- 3.19 **SJ14: Bridge Street/Windsor Street/Cherwell Street**
- 3.19.1 To the north of SJ13, Windsor Street forms a traffic signal controlled crossroads junction with Bridge Street. The Windsor Street/Cherwell Street approaches are both marked with 3no lanes, comprising:
- Nearside: left turn,
  - Middle: straight ahead, and
  - Offside: right turn.

3.19.2 Bridge Street (W) is marked with a single approach lane. Bridge Street (E) has 2no lanes.

3.19.3 There are assisted (ie with push button) pedestrian crossings on all approaches with the exception of Bridge Street (W). On this arm, assistance is provided in the form of an island and dropped kerbs/tactile paving.

3.19.4 The existing SJ13 arrangements are presented on Drg No 1361/19.

### 3.20 SJ15: Cherwell Street/Hennef Way

To the north of SJ13, Cherwell Street forms a roundabout junction with Hennef Way. The existing SJ15 roundabout arrangements are presented on Drg No 1361/20.

### 3.21 ACCIDENT HISTORY

3.21.1 The three year accident records for the highway network in the vicinity of the Site and the TA study network are purchased from OCC. AHA requested records for the most recent three year period available at the time of purchase. The data provided by OCC covers the period 01 January 2012 to 31 April 2015, and therefore covers a period of 3 years and 4 months. The OCC accident records are presented in Appendix A.

3.21.2 A summary of the recorded accidents over the TA study network area is:

TA STUDY JUNCTION	2012	2013	2014	2015	TOTAL
White Post Road (near Site)	-	-	-	1	1
SJ2	1	-	-	-	1
SJ2A	-	-	-	-	0
SJ3	-	-	-	-	0
SJ3A	-	-	-	-	0
SJ4	-	1	-	-	1
SJ5	1	1	-	-	2
SJ6	-	-	-	-	0
SJ7	-	-	-	-	0
SJ8	-	2	-	1	3

SJ9	1	-	1	-	2
SJ10	2	1	1	-	4
SJ11	2	3	2	-	7
SJ12	-	-	1	-	1
SJ13	-	-	1	-	1
SJ14	2	1	3	-	6
SJ15	2	4	2	4	12
<b>Total</b>	<b>11</b>	<b>13</b>	<b>11</b>	<b>6</b>	<b>41</b>

3.21.3 At 13 of the study locations there are 3 or fewer recorded accidents. The junctions with the highest number of recorded accidents are SJ11, SJ14 and SJ15.

3.21.4 It is relevant to consider the level of development generated traffic that is predicted to be added to SJ11, SJ14 and SJ15.

REF	2025 BASE (pcu)		GENERATED TRAFFIC (pcu)		IMPACT (%)	
	AM	PM	AM	PM	AM	PM
SJ11	2527	2705	41	44	1.6	1.6
SJ14	3185	3269	62	70	1.9	2.1
SJ15	5807	5714	49	54	0.8	0.9

In all cases the proposed development is estimated to generate only about a 2.0% or less increase in traffic. Consequently, it is highly unlikely that the proposed development will materially alter the occurrence of accidents at the junctions.

### 3.21.5 Summary

It is concluded that the proposed development has **no** material impact on the occurrence of accidents on the study network, subject to the development access arrangements according with usual design standards.

## 4 Proposed Site Access Arrangements

- 4.1 A holistic approach to the Site access arrangements is at the core of the development Site access strategy. Thus, there is a cohesive 'package' of development access arrangements for differing modes of travel. Further information about walk and cycle modes is presented in Chapter 5, and about public transport in Chapter 6.
- 4.2 The proposed access strategy for the Site is:
- Vehicles: Single access on White Post Road,
  - Pedestrians/cycles: Access on White Post Road, Salt Way and Wykham Lane.
- 4.3 The proposed vehicular access arrangements are shown on Drg No 1361/21. This shows the formation of a priority controlled junction on White Post Road. The proposed junction geometry comprises:
- 6.75m wide Site access road,
  - 1.0m wide grass verge,
  - 3.0m wide shared footway/cycle way on the north side of the access
  - 2.0 wide footway on the south side of the footway,
  - 10.0m corner radii,
  - Visibility splays of 2.4m x 40m, based on guidance in MfS.

### 4.4 PARKING SURVEY

- 4.4.1 The application Site is located opposite to Bishop Loveday CE Primary School. A parking survey was undertaken on White Post Road to determine the existing on Street parking demand on White Post Road in the vicinity of the proposed Site access. A parking survey was undertaken on 25 September 2013 at the following times:
- 0800 – 0930,
  - 1445 – 1545.

The survey periods include the drop-off and pick-up times for the primary school.

4.4.2 The survey area comprises:

- White Post Road between the Sycamore Drive/Bankside roundabout junction (SJ2) and Broad Gap,
- Sycamore Drive between SJ2 and Willow Road.

4.4.3 The survey area is divided into 12 zones, labeled A-L, as shown on Drg 1361/PS/1, Appendix E.

4.4.4 The number of vehicles parked in each zone is recorded at 5 minute intervals. The results of the parking survey are presented in Table 1361/PS/1, Appendix E.

4.4.5 Review of Table 1361/PS/1 confirms that the peak morning parking accumulation occurred during the period 0850-0855. Drg 1361/PS/1, Appendix E, provides a visual summary of the parked vehicles for this period. It is clear that the section of White Post Road in the vicinity of the proposed Site access, (referred to as Zones C & J) is heavily parked on both sides of the road.

4.4.6 Review of Table 1361/PS/1 confirms the peak afternoon parking accumulation occurred during the period 1515-1520. Drg 1361/PS/2, Appendix E, provides a visual summary of the parked vehicles for this period. Zones C & J are heavily parked on both sides of the road, albeit to a lesser extent than in the morning worst case scenario.

4.4.7 Consequently, it is proposed that waiting restrictions are introduced in the vicinity of the proposed Site access to ensure that parked vehicles have no detrimental impact on the operation of the proposed Site access junction. It is also proposed that car parking is provided within the Site to accommodate parking displaced by the proposed waiting restrictions. The proposed parking improvements are presented on AHA Drg 1361/21.

#### 4.5 INTERNAL ROADS

4.5.1 The internal road layout for the outline application is to be the subject of reserved matters application(s). However, it is acknowledged that there is an aspiration to provide a 'link road' between Bloxham Road and White Post Road that is aimed primarily at providing access to development that will come forward within the Banbury 17 allocation area. Consequently, the applicant proposes to provide a road through the application Site between White Post Road and the western boundary of the Site comprising the following geometric standards:

- 6.75m wide carriageway,
- 1.0m wide grass verge,
- 3.0m wide shared footway/cycle way on the north side of the road
- 2.0 wide footway on the south side of the road.

4.5.2 The applicant has been liaising with the promoters of the Wykham Park Farm development. Resulting from this, a road connection location on the Site western boundary has been identified that satisfies both development proposals. Although the internal road network will be subject to reserved matters application(s) the masterplan layout that accompanies the planning application presents an indicative alignment for the 'link road'.

4.5.3 It is relevant to note that the level of geometry proposed for the 'link road' is greater than what would normally be required for a stand-alone residential cul-de-sac scheme of 280 dwellings.

#### 4.6 PEDESTRIANS

4.6.1 Pedestrian connectivity between the proposed residential development and the surrounding area is a key component of the development access strategy. This is converted into practice by a combination of:

- Well designed on-Site development layout that recognises pedestrian desire lines and provides enabling pedestrian facilities (eg footways, roads that discourage high speeds, a sense of place that is safe for pedestrians fostered by the layout of buildings, etc), and

- Pedestrian links between the Site and the surrounding area.

4.6.2 The former (on-Site layout) will be the subject of a reserved matters application, as the planning application is in outline. However, the principles of the pedestrian access strategy are established in this TA report (and the accompanying Travel Plan), and will form the basis of the detailed development Site layout for which reserved matters permission will be sought.

4.6.3 The existing pedestrian infrastructure around the Site and the surrounding area is described in Chapter 5. The development proposals for improvements to the existing pedestrian infrastructure and pedestrian linkages to/from the proposed development are also described in Chapter 5.

#### 4.7 CYCLE

The existing cycle infrastructure around the Site and the surrounding area is described in Chapter 5. The development proposals for improvements to the existing cycle infrastructure and cycle linkages to/from the proposed development are also described in Chapter 5.

#### 4.8 MOBILITY IMPAIRED

The needs of those with mobility impairment are an important component of the detailed design of the development. This is advocated in NPPF (eg NPPF para 35, refer para 2.5.3 above). The detailed design of the internal layout of the development, which must be the subject of reserved matters approval, will describe the facilities to be provided on Site to assist the mobility impaired, taking account of guidance and standards together with good practice and local/national policies.

## 5 Walk & Cycle

### 5.1 WALK

5.1.1 It is established and acknowledged that walking is the most important mode of travel at the local level, and offers the greatest potential to replace short car trips, particularly under 2km.

5.1.2 The ClHT provides guidance about journeys on foot. It does not provide a definitive view of distances, but does suggest a preferred maximum distance of 2000m for walk commuting trips. A 400m distance corresponds to a walk time of 5 minutes, based on upon a typical normal walking speed. Figure 3 presents the development 400m, 800m, 1200m and 2000m walk isochrones, (ie reflecting 5, 10, 15 and 25 minute walk journeys), and taking account of the pedestrian infrastructure.

5.1.3 The DfT National Travel Survey confirms that 78% of all trips less than a mile (1.6km) are carried out on foot.

5.1.4 The 'walkable neighbourhood' concept is set out in MfS1 and endorsed in MfS2. MfS1 explains that:

*"Walkable neighbourhoods are typically characterised by having a range of facilities within 10 minutes' (up to about 800 m) walking distance of residential areas which residents may access comfortably on foot. However, this is **not an upper limit** and .....that walking offers the greatest potential to replace short car trips, particularly those under 2 km."* (MfS para 4.4.1, my emphasis)

5.1.5 Indicated on Figure 3 are examples of local facilities near to the Site. This illustrates that there are a number of amenities within 800m of the Site, including:

- Nursery,
- Primary school,
- Convenience store,
- Community centre,
- Playgrounds/public open space,

- Cricket club,
- Salons,
- Public house,
- Church,
- Employment (Cherwell District Council),
- Allotments,
- Bus stops.

5.1.6 Within about 1200m of the Site (typically a 15 minute walk) there are a number of additional facilities, including:

- Secondary school/sixth form,
- Convenience Store,
- Supermarket,
- Post Office,
- Bank/ATM,
- Dentist, Pharmacy & Optician,
- Restaurants/takeaways.

5.1.7 Figure 3 demonstrates that there is a substantial range of amenities within 2000m of the Site. The edge of Banbury town centre is within 2000m of the Site and Horton Hospital provides a substantial employment destination within 2000m of the Site.

### 5.1.8 Public Rights of Way

5.1.8.1 Figure 4 presents the existing Public Rights of Way (PROW) near to the Site. This shows that there is a restricted byway along the northern boundary of the Site, and a bridleway close to the western Site boundary. There are a number of footpaths connecting the restricted byway to the residential area to the north of the Site.

5.1.8.2 There is an existing footpath within the Site between the restricted byway and Wykham Lane. This will be maintained and improved as part of the proposed development. Improvements may include, for example, a sealed surface, street lighting etc. The on-site layout will be subject of a reserved matters application.



5.1.8.3 Figure 4 shows that there is an extensive network of PROW in the vicinity of the Site providing convenient pedestrian routes to a range of local amenities.

## 5.2 CYCLE

5.2.1 It is recognised that cycling also has potential to substitute for short car trips, particularly those under 5km, and to form part of a longer journey by public transport.

5.2.2 The CIHT guidance 'Cycle Friendly Infrastructure' (2004) states that:

*"Most journeys are short. Three quarters of journeys by all modes are less than five miles (8km) and half under two miles (3.2km) (DOT 1993, table 2a). These are distances that can be cycled comfortably by a reasonably fit person."* (para 2.3)

5.2.3 Figure 5 indicates the 2km and 5km cycle isochrones for the Site, reflecting typically 10 minute and 25 minute journeys. Review of Figure 5 highlights that all of the built-up area of Banbury is within 5km of the Site. Additionally, all of Twyford, Adderbury, Broughton, North Newington, and most of Bloxham are accessible to Site residents by cycle.

5.2.4 Figure 6 indicates formally identified cycle routes in the locale of the Site. These include:

- National Cycle Network Route 5 (NCN 5): Reading to Holyhead via Oxford,
- A local signed on-road route linking the Site and NCN 5 to Banbury Rail Station.

5.2.5 In summary, the destination opportunities within a 5km cycle ride of the Site for residents of the development comprise a full range of amenity and employment locations within Banbury itself and beyond.

## 5.3 SUMMARY

Transport sustainability is a principle underlying the proposed development. Encouraging walk and cycle journeys is recognised as important. The location of the Site, provides a good context for journeys of residents to be undertaken on foot and

by cycle, to a range of local amenities. The development proposals include the package of measures to enhance the pedestrian and public transport accessibility.

## 6 Public Transport

- 6.1 The CIHT 'Guidelines for Planning for Public Transport in Developments' (March 1999) set out that, in considering public transport provision for development, three questions need to be addressed:
- *What is the existing situation with respect to public transport provision in and around the development?*
  - *What transport provision is required to ensure that the proposed development meets national and local transport policy objectives?*
  - *Are the transport features of the development consistent with the transport policy objectives, and if not, can they be changed to enable the policy objectives to be achieved?"* (para 4.18).
- 6.2 The CIHT Guidelines conclude that a broad methodology which allows the issues to be identified and debated comprises the following general principles:
- Investigate existing public transport provision.
  - Using this information, determine whether the Site is well served by public transport,
  - Where the Site is not well served or existing services are fully loaded, suggest measures to rectify this situation,
  - Define infrastructure for the proposed development which enables public transport to be operated effectively and make it attractive to potential passengers.
- 6.3 The proposed development affords opportunity for development generated public transport journeys to be made by bus and rail.

### 6.4 BUS

- 6.4.1 Figure 7 presents the bus stops within 400m, 800m and 1200m of the Site. The closest bus stops to the Site are on Sycamore Drive, within 400m. There are additional bus stops on White Post Road that are circa 500m from the centre of the Site. The bus stop on the southern side of Sycamore Drive closest to SJ2 has a shelter. All other bus stops on Sycamore Drive and White Post Road comprise a 'flag and pole'.
- 6.4.2 Table 1 presents the routes and frequencies of bus services calling at stops within 800m of the Site. The routes of services summarised in Table 1 are shown on Figure 8. Table 1 shows that the B1 service calls within 400m of the Site. The B1 operates at a 30 minute frequency, Monday – Saturday. The journey time between Sycamore Drive and Banbury bus station is 12-13 minutes. The earliest weekday departure from Sycamore Drive is 0702, and the latest weekday arrival at Sycamore Drive is at 1822.
- 6.4.3 The B2 service calls on White Post Road, circa 500m from the Site, and operates on the same frequency as the B1 service. There are additional services calling on A4260 within 800m of the Site. In a typical weekday situation there are circa 8 buses per hour calling within 800m of the Site, taking into account buses travelling in both directions along the road. Destinations include, among others, Banbury town centre, Bodicote, Easington, Oxford, Kings Sutton, Aynho, Evenly, Croughton, and Brackley.
- 6.4.4 All services calling within 800m of the Site also call at Banbury bus station, which is within 400m of Banbury Rail station. This provides opportunity for onward journeys by public transport to an extensive range of destinations.
- 6.4.5 **Bus Stop Improvements**
- 6.4.5.1 Improvements to bus infrastructure are to be provided as part of the proposed development. The aim of this is to promote bus travel to residents of the Site through providing better bus stop facilities. Proposed bus stop improvements are also of benefit to existing public transport users in the vicinity of the Site.
- 6.4.5.2 As mentioned in para 6.4.1, the bus stop on the southern side of Sycamore Drive closest to SJ2 has a shelter. All other bus stops on Sycamore Drive and White Post Road comprise a 'flag and pole'. The applicant proposes to:

- Upgrade the existing northbound and southbound stops on White Post Road, in the vicinity of the Site, to provide shelters;
- Introduce a bus stop(s) within the Site, with details to be agreed as part of a future reserved matters application.

6.4.5.3 The new/upgraded bus stops are to be to Quality Bus Standard providing a shelter with lighting, seating and timetable/routing information together with low floor access. It is recognised that the proposed bus stop locations will be subject to discussions with the highway authority, as part of a reserved matters application.

## 6.5 RAIL

6.5.1 Banbury Rail Station is circa 2.5km from the Site (refer Figure 3). This provides opportunity for residents to travel by rail, with the journey between the rail station and the Site by cycle or bus. Cycle storage is provided at the station, and all bus services calling close to the Site call within 400m of the rail station. Additionally, there are 795 car park spaces provided at the station. There are mobility impaired spaces available free of charge for blue badge holders.

6.5.2 The main services calling at Banbury Rail Station comprise:

OPERATOR	PRINCIPAL ROUTE	TYPICAL WEEKDAY FREQUENCY (mins)
Chiltern Railways	London - Birmingham:	30
Cross Country	Manchester - Bournemouth:	60
Cross Country	Newcastle – Reading:	60

Additionally, there are a number of less frequent services calling at Banbury, operated by Chiltern Railways and First Great Western. Typically, there are circa 10-11 services per hour calling at Banbury Station.

6.5.3 Services calling at Banbury provide frequent direct trains to a wide range of destinations including, among others, London, Birmingham, Manchester, Leeds, Sheffield, Newcastle, Southampton, Bournemouth, Coventry, Stoke-on-Trent, Derby, Doncaster and York.

6.5.4 Banbury rail station provides opportunity for residents undertake regular journeys (eg for work) to a wide range of destinations. Journey times between Banbury and key destinations that may be suitable for daily commuting are:

DESTINATION	APPROXIMATE JOURNEY TIME (mins)
Bicester	14
Leamington Spa	17
Oxford	21
Warwick	22
High Wycombe	32
Reading	45
Birmingham	53
London Marylebone	54 - 64

6.5.5 It is demonstrated that there is excellent opportunity for residents of the proposed development to undertake journeys by rail to an extensive range of destinations.

## 6.6 SUMMARY

It is established that public transport journeys to/from the Site can be made to a range of locations. The development proposals include a package of measures to improve bus accessibility and facilities. This is in accordance with the aims and objectives of current national and local policies.

## 7 Travel Plan

7.1 The Travel Plan (TP) report is submitted in support of the planning application, and complementary to this TA report. A summary of the key points in the TP are set out below.

7.2 The key objectives of the TP are to:

- Contribute to traffic reduction and other sustainable transport objectives set out in national, regional and local policies,
- Improve accessibility of the Site by sustainable modes of transport and address traffic and parking issues,
- Widen choice of travel mode for all those travelling to/from the Site.

7.3 Specific outcomes sought from the TP are to:

- Achieve the minimum number of additional single occupancy car traffic movements to/from the development,
- Address the access needs of site users, by supporting walking, cycling and public transport,
- Reduce the need for travel to/from the Site.

7.4 The TP explicitly considers accessibility by the sustainable travel modes of pedestrian, cycle, public transport and car share.

7.5 The TP target is set as: **maximum peak hour 2-way vehicle trip rate of 0.537 vehicles/hour/dwelling**, to be achieved within 5 years of first occupation of the development.

7.6 The residential developer will appoint a Travel Plan Coordinator (TPC), to introduce, manage, operate and monitor the TP. As part of the on-going management of the TP, the TPC will maintain a dialogue with the Council, and monitor emerging best practice information, to provide the most efficient platform for maximising the effectiveness of the TP.

7.7

The residential developer is required to finance the TP. A sufficient revenue budget will be identified to employ the TPC for a period of 5 years of first occupation of the development, on a sufficient basis to introduce and manage the TP initiatives, and thereafter as required to:

- Manage the initiatives,
- Finance the measures identified in this and subsequent TP Monitoring and Review reports and as agreed with the Council, and
- Enable the TPC postholder to carry out the duties identified above.

7.8

The TP Action Plan is set out in Chapter 10 of the TP. The TP Table 3 summarises identified measures that are proposed, and indicates the timing for the measures and funding information. This illustrates the holistic approach adopted for the TP, aimed at encouraging from the outset a positive sustainable transport awareness and culture for the development. The TP measures will be reviewed and amended as appropriate, in consultation with and requiring the agreement of the local authority, as part of the on-going dynamic monitoring and review process for the TP.