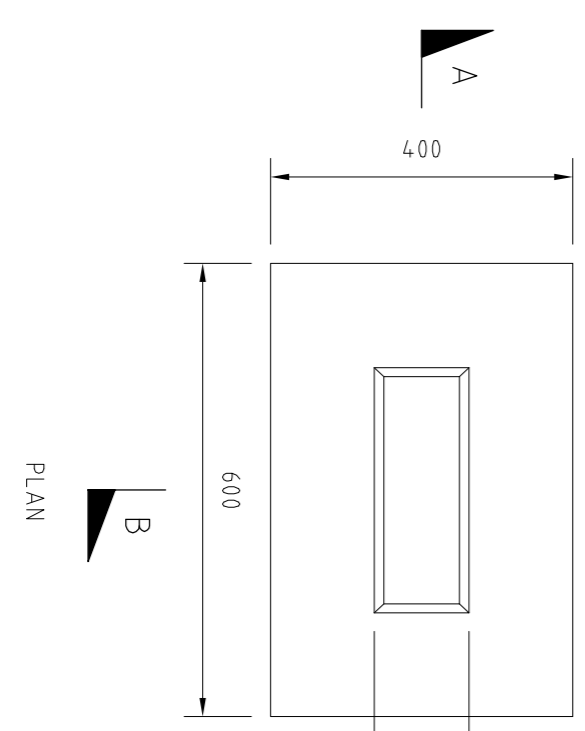
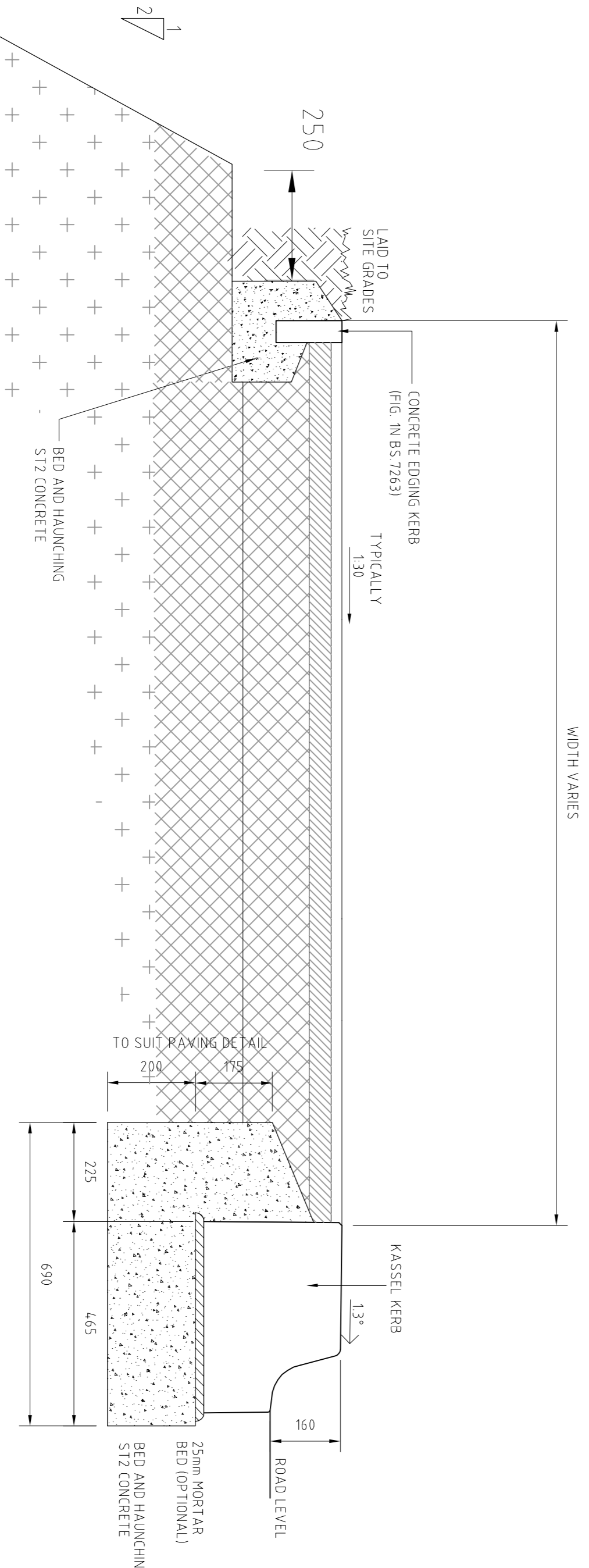


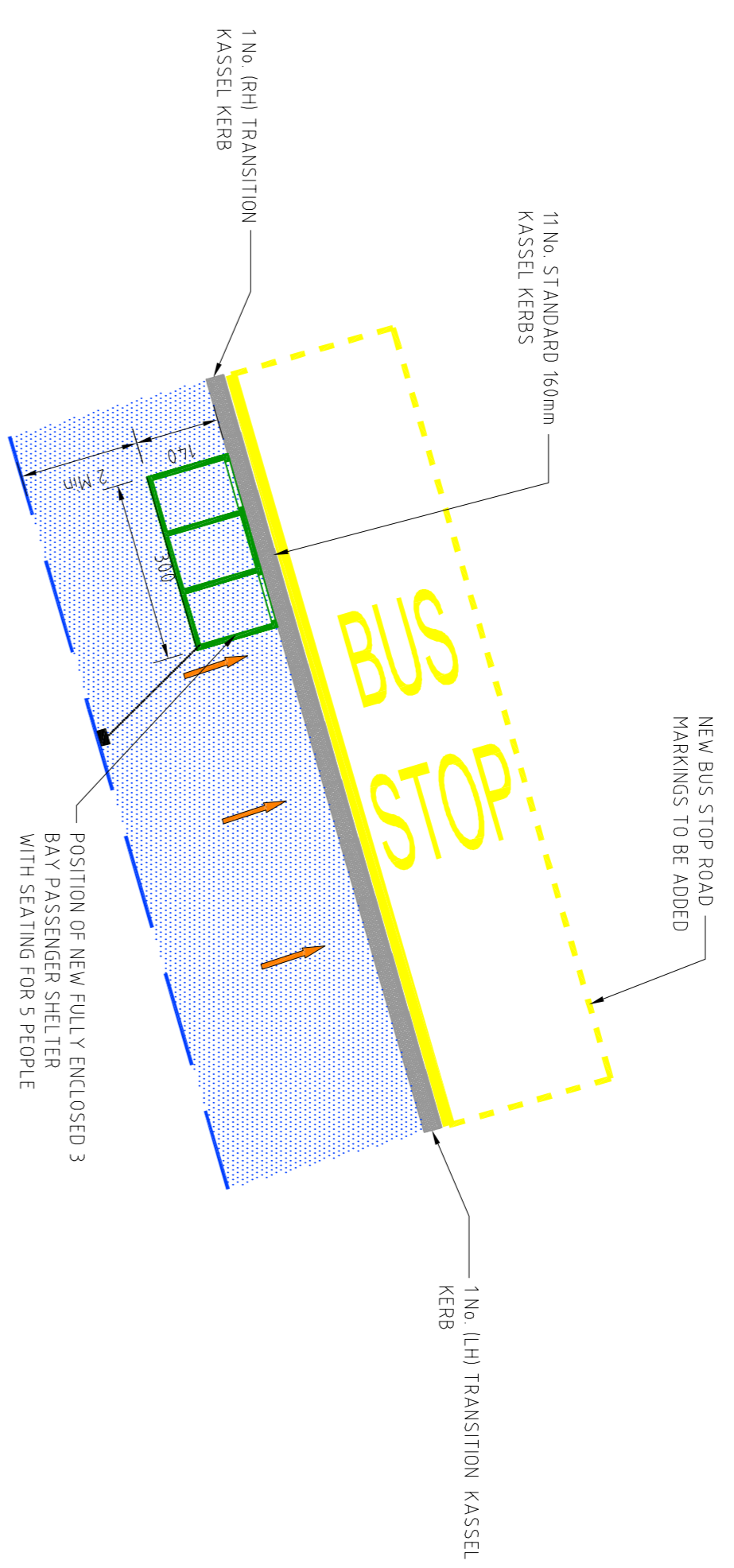
ELEVATION OF KASSEL KERB AND TRANSITION DETAIL  
SCALE 1:10

PLAN OF KASSEL KERB TYPICAL LAYOUT  
SCALE 1:10

TYPICAL SECTION THROUGH FOOTWAY AT KASSEL KERBS  
SCALE 1:10



HIGHWAY BOUNDARY MARKER DETAIL  
SCALE 1:10



TYPICAL BUS STOP LAYOUT  
SCALE 1:10

- NOTES
1. WITHIN GRASSED AREAS THE LIMIT OF ADAPTIBLE HIGHWAY IS TO BE INDICATED ON SITE BY MEANS OF CONCRETE HIGHWAY BOUNDARY MARKERS POSITIONED AT INTERVALS OF EXACT 5m
  2. THE MARKERS SHOULD BE PLACED IN A CENTRAL POSITION ON THE HIGHWAY BOUNDARY
  3. WHERE THE HIGHWAY LIMITS ARE NOT READY IDENTIFIABLE IN UNGRASSED AREAS, I.E. WHERE THE HIGHWAY BOUNDARY MEETS A PRIVATE DRIVE AND THERE IS NO CHANGE IN SURFACE MATERIAL, A MARKER SHOULD BE PLACED IN A CENTRAL POSITION ON THE HIGHWAY BOUNDARY
  4. FOR LOCATION OF MARKERS REFER TO DRAWING NO UA001881/72L0

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FOR CONSTRUCTION	
Issue	Date
B	28/02/14
A	09/08/13
01	MAR 12

Original Size	Author
A1	MILLEMELIN
0.5	W PEARSON
0.5	S DAVIES

Grid	Client
OS	azdominion

Project	Title
BICESTER ECO DEVELOPMENT EXEMPLAR SITE	EXEMPLAR SITE S38 CONSTRUCTION DETAILS SHEET 2 OF 2

Hyder Consulting (UK) Limited	Hyder Consulting
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Drawing No	Project No	Issue
7272	UA001881	B

