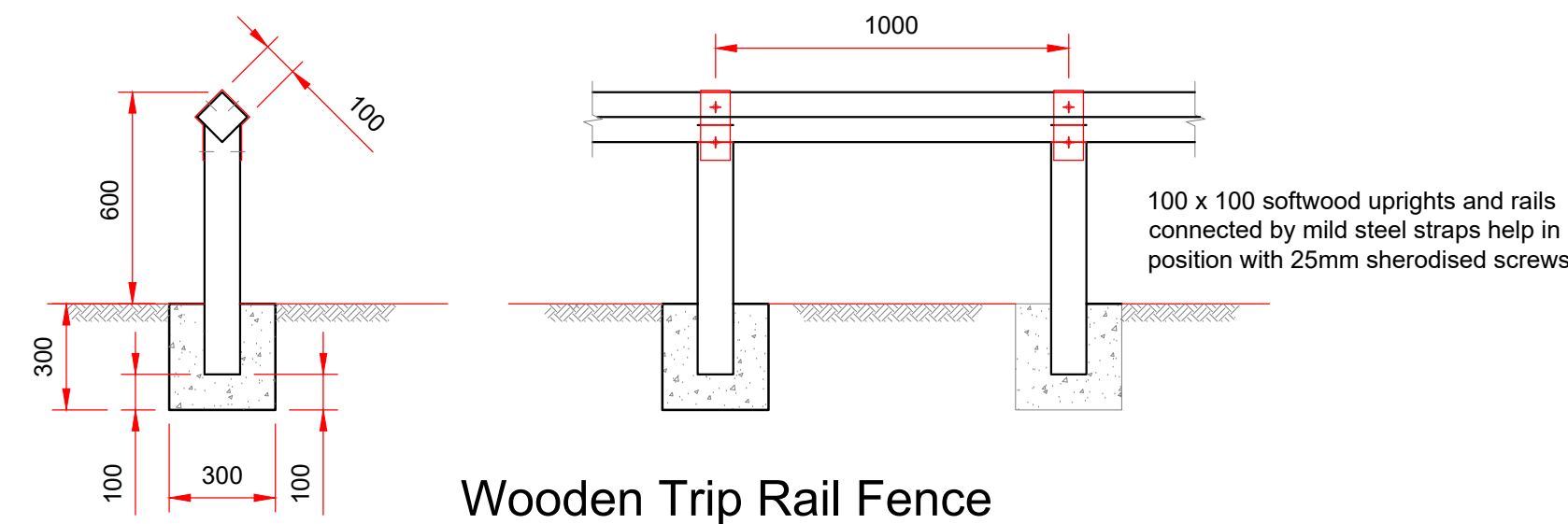
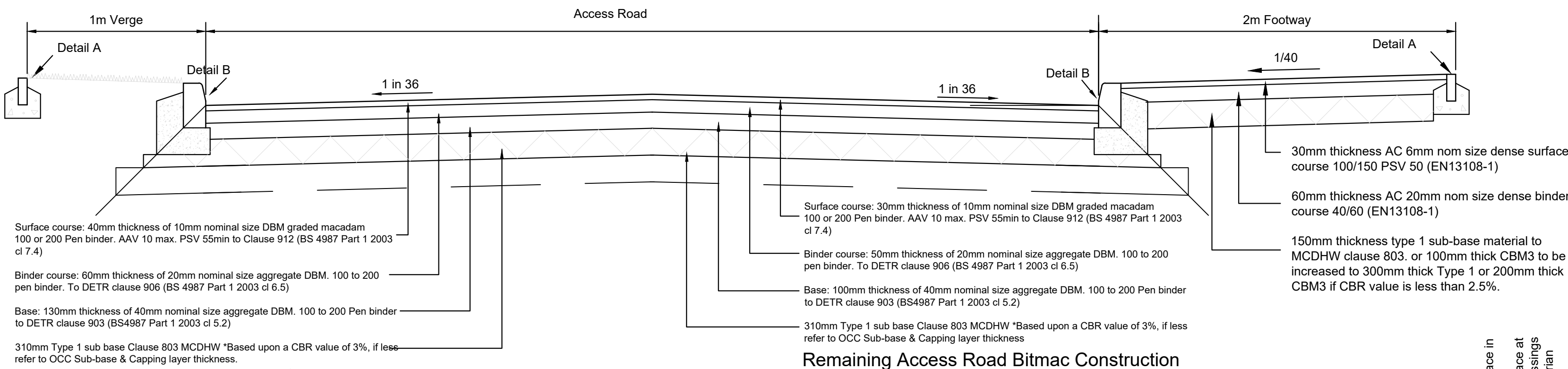


Typical Section Through Impermeable Block Paved Road

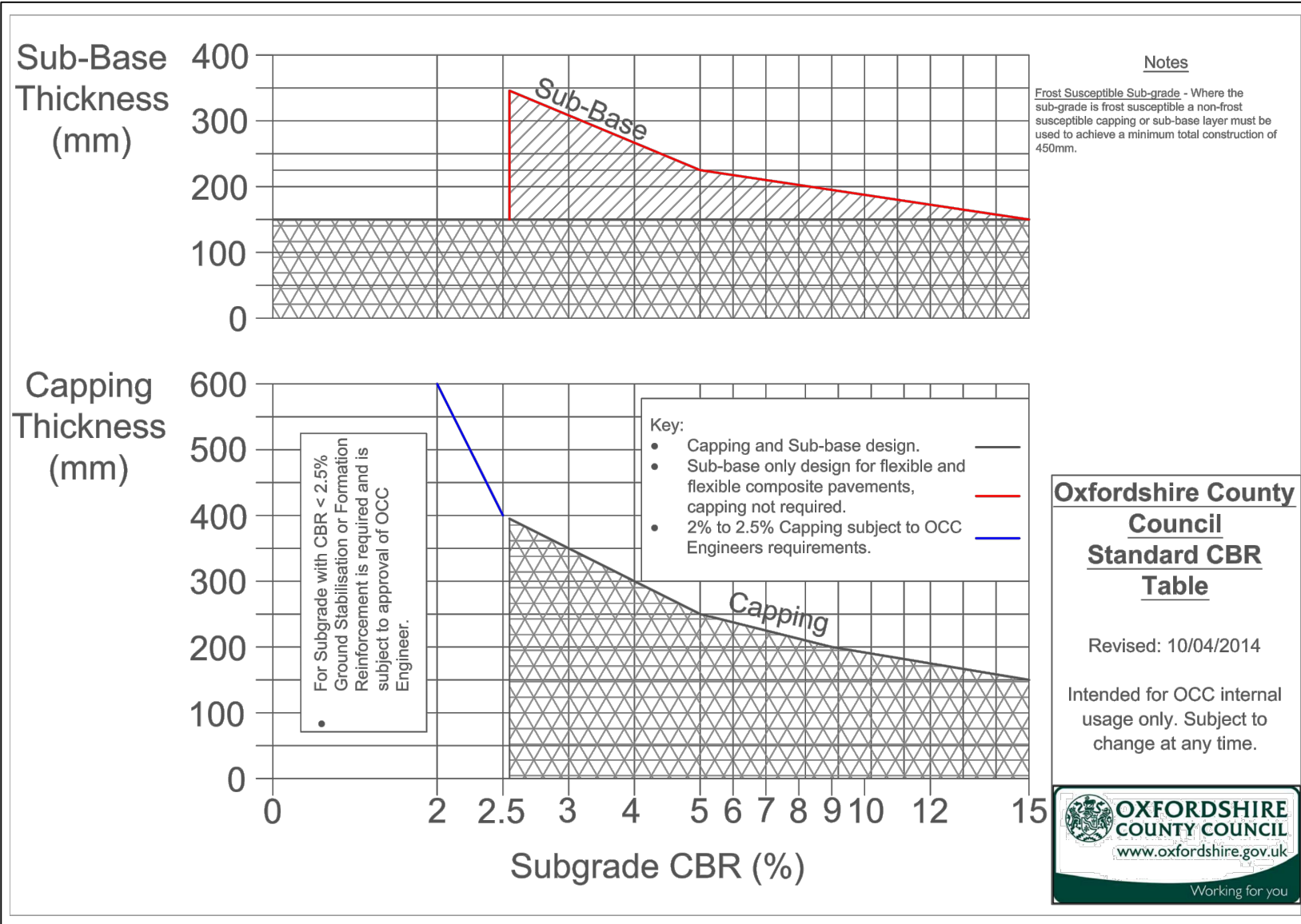


Wooden Trip Rail Fence

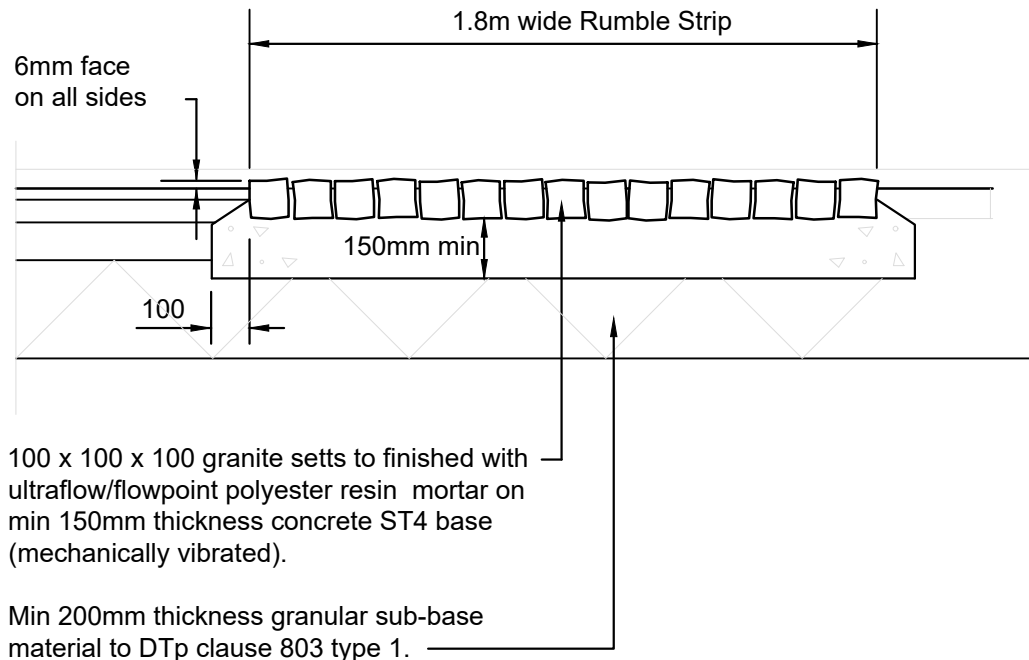


Access Road 1 ch 0 to ch 151 & ch 272 to 380 & Road 3 Bitmac Construction Only.

Access Road Bitmac Construction



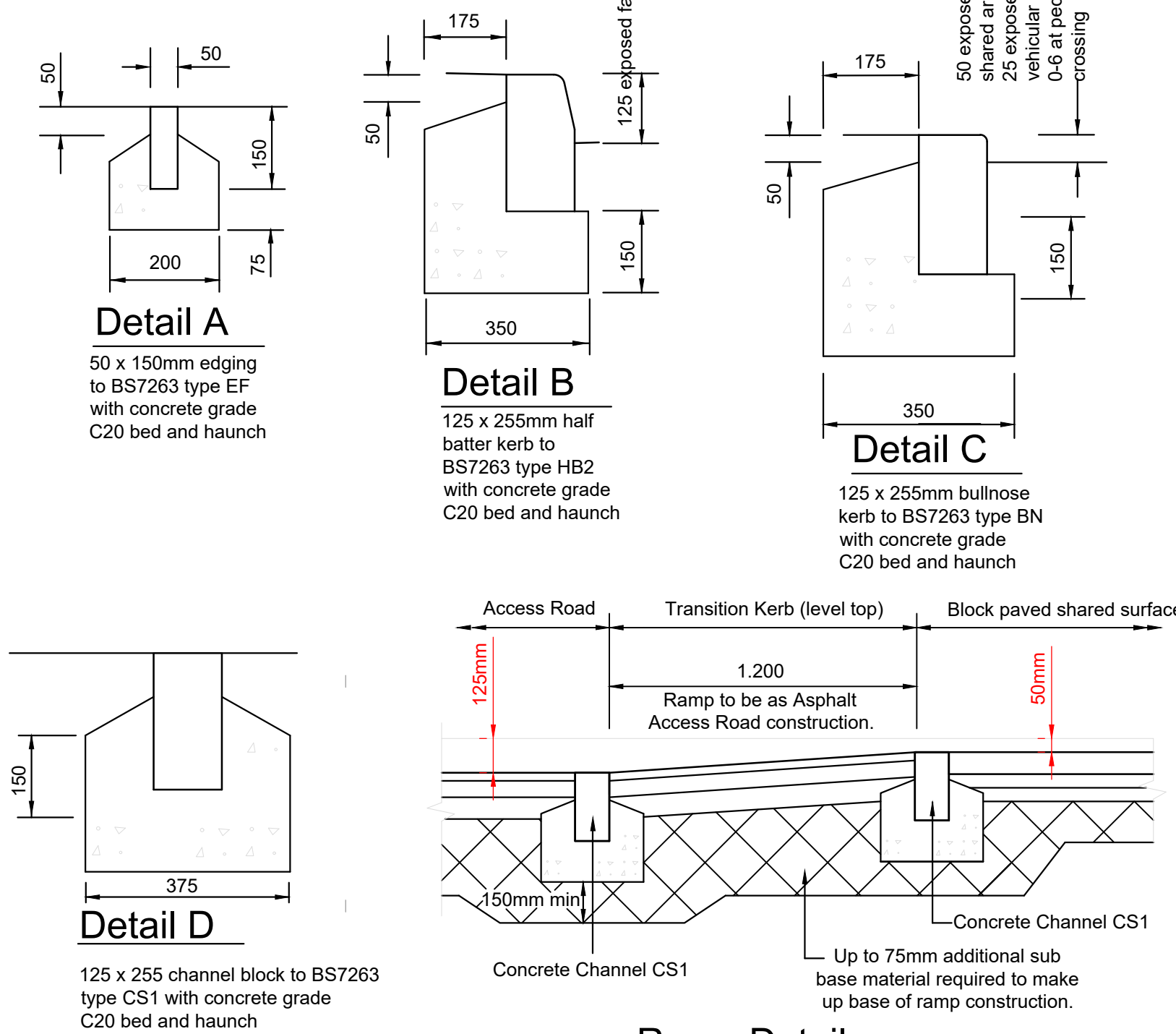
- Notes:
- No carriageway with a CBR of less than 2.5% should be commenced without reporting to the designer and developer.
 - All soft spots to be recorded and removed and back filled with 6F1 & 6F2 material and re-proof tested as required.
 - Ground stabilisation works at formation levels will be required for CBR values less than 2.5%.
 - Where subgrade is frost susceptible, minimum construction thickness to be 450mm
 - Extent of carriageway replacement or remedial work to be confirmed, subject to developer and Highway Authority confirmation.
 - In-situ CBR testing required at 30m centres



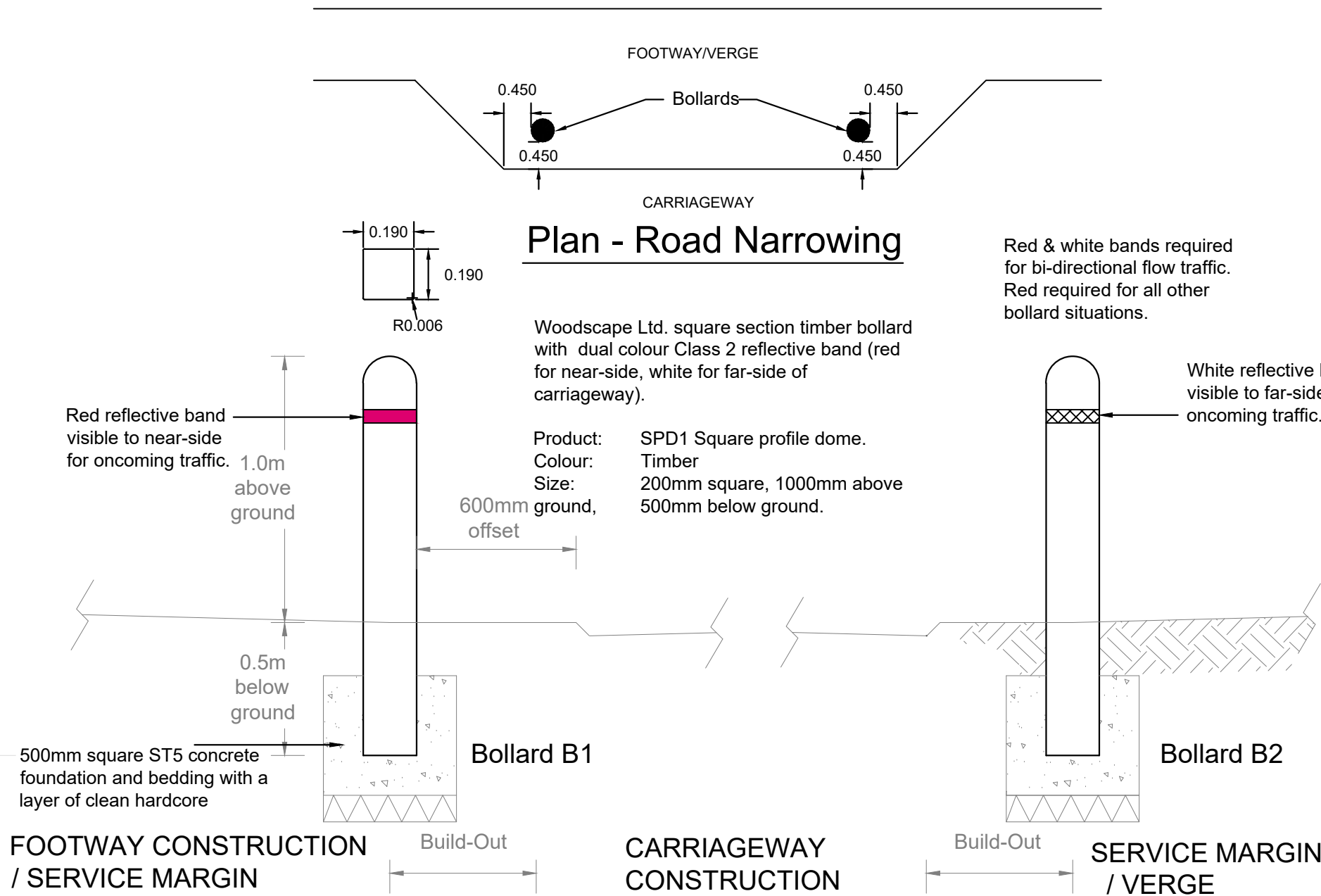
Detail of Rumble Strip

Note: Rumble strip to end 200mm off kerb line to maintain channel for drainage both sides. Bitmac road construction to form 200mm channels.

Remaining Access Road Bitmac Construction



Ramp Detail

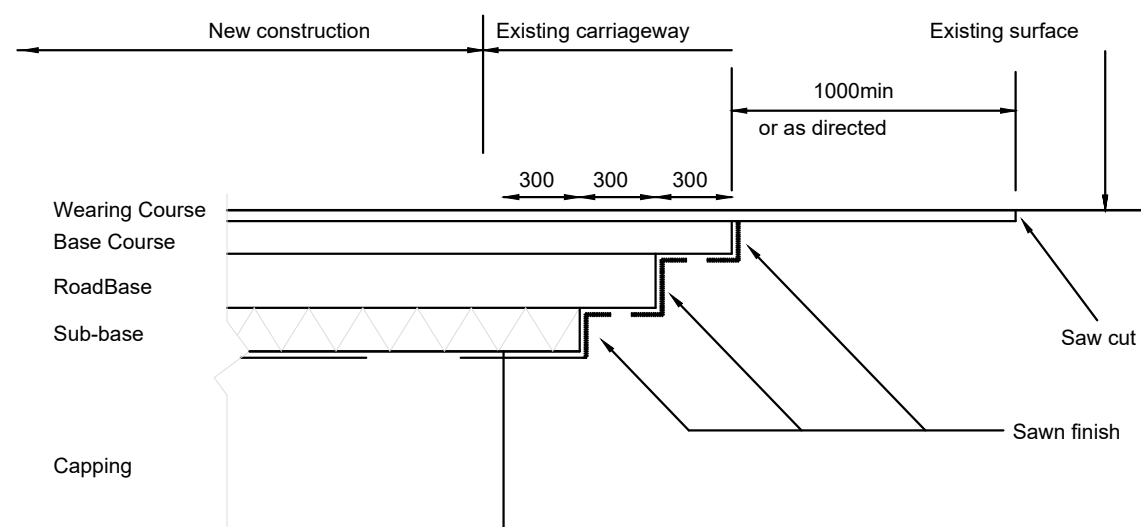


Reflective Bollards at Road Narrowing Detail

Highway Boundary Marker Notes

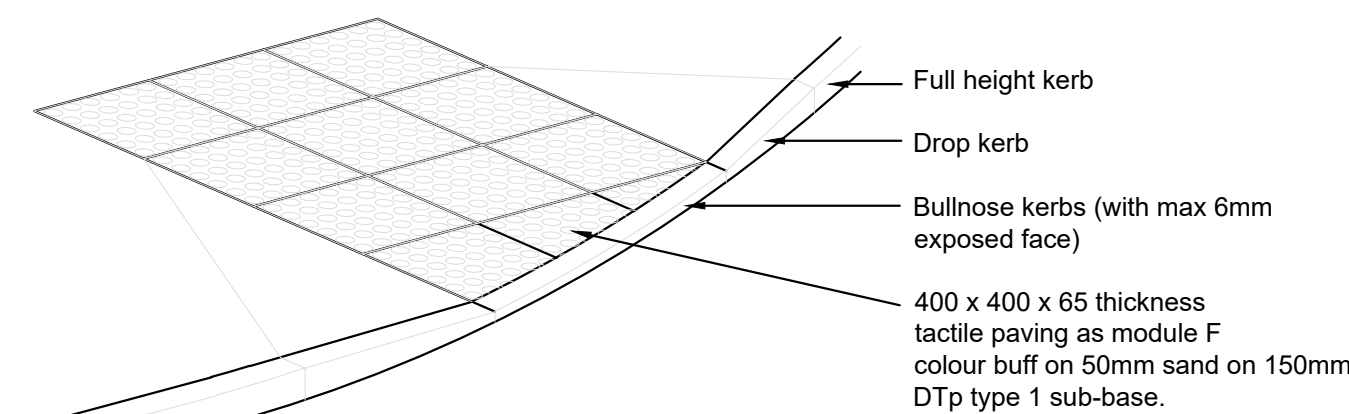
- A. Within grassed areas the limit of adoptable highway is to be indicated on site by means of concrete highway boundary markers positioned at intervals of one every 5m.
- B. The markers are to be concreted in position in grade ST4 concrete so that the top is flush with the verge in accordance with the manufacturers recommendations.
- C. Where the highway limits are not readily identifiable in ungrassed areas i.e. where the highway boundary meets with a private drive and there is no change in surface material, a marker should be placed in a central position on the highway boundary.

Highway Boundary Markers



Tie-In (where applicable)

Note: Tactile paviors are to be aligned with the direction of crossing. The crossing normally comprises nine uncut tactile paviors in three rows with cut tactile paviors laid to rear of kerb as required.



In Line Uncontrolled Pedestrian Crossing

For further information see Guidance on the use of Tactile Paving Surfaces (DETR 1998)

The Contractor is to check and verify in conjunction with the Architects details all setting out points, building and site dimensions, levels and sewer invert levels at connection points and ensure that they are fully conversant with the contents and of requirements the site investigation report before work starts. The Contractor is to comply in all respects with current building legislation, British Standard Specifications, Building Regulations etc, whether or not specifically stated on this drawing.

This drawing is not intended to show details of ground conditions or ground contaminants. Each area of ground relied upon to support any structure depicted (including drainage) must be investigated by the Contractor any areas of formation for soil structures which do not accord with the anticipated conditions as described in the site investigation report are to be immediately notified to the Engineer, where applicable. Any suspect fluid ground or ground contaminants on or within the ground should be further investigated by a suitable expert. Any earthworks shown indicate typical slopes for guidance only and should be investigated further by a suitable geotechnical expert.

Where existing trees are shown to be retained they are to be subject to a full Arboricultural inspection for safety. All trees are to be planted so as to ensure they are a minimum of 5 metres from buildings and 3 metres from drainage and services, where applicable. A foundation is to be provided to accommodate the proposed tree planting, where applicable.

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GENERAL NOTES

- This drawing is to be read in conjunction with relevant architectural and engineering drawings.
- Levels indicated in blocks are Finished floor levels and are 150mm above adjacent finished ground levels unless otherwise shown.
- Levels of the existing road at the point of tie-in with proposed site road must be checked prior to commencement of works.
- Any discrepancies between the details shown and actual on site conditions to be reported immediately to the engineer prior to commencement of works.

ADOPTABLE ROADS AND SEWERS

- Roads, footways and parking bays which form part of the highway to be adopted under Section 38 of the Highways Act 1980 shall comply with the requirements of the Adopting Authority.
- Sewers to be adopted under Section 104 of the Water Industries Act 1991 shall comply with the Water Authorities Association "Sewers for Adoption 6th Edition" with any amendments specified by the Adopting Water Authority.
- All pipes to be used in adoptable sewerage shall be either clayware to BS EN 205 or concrete to BS EN 1916 and BS 5911: Part 1 with Class S bedding unless otherwise stated. With approval of the Adopting Authority solid wall concentric external rib reinforced UPVC pipes complying with the relevant provisions of BS EN 13476 may be used.
- Where cover to a pipe is more than 1200mm under adoptable carriageway the trench shall be filled to formation of the carriageway with well compacted DTP Type 1 material.
- Where cover to a pipe is less than 1200mm under adoptable carriageway it shall be provided with concrete protection in accordance with the specification of the adopting authority and back filled to formation of the carriageway with well compacted DTP Type 1 material. Where concrete bed and surround is specified flexibility of joints is to be maintained by using compressible bitumen impregnated fibreboard at each pipe joint.
- All existing drainage invert levels, diameters and locations are to be checked by the Contractor prior to the commencement of any proposed drainage work. Any difference between actual and drawn details is to be reported to the Engineer immediately.
- Positions of existing services/statutory undertakers apparatus adjacent to or crossing proposed sewers is to be checked by the Contractor prior to starting work.
- All work to be carried out to the Highway Agency Specification for Highways & the relevant completed appendices.

CONSTRUCTION

Please note while these drawings may be used for tender purposes, drawings are subject to Oxfordshire County Council approval as part of ongoing consultations and design check. Amendments may therefore be requested

F	Block paving detail amended, bullnose kerb detail amended & rumble strip detail amended.	24.10.16	LJ
E	Wooden trip rail fence detail surrounding pond added to construction details.	23.09.16	LJ
D	Bitmac road and footway construction, ramp & rumble strip detail and block paved road kerb detail amended to suit 11/7/2016 OCC comments.	29.07.16	JB
C	Road construction thickness amended to previous revision whilst maintaining sub-base specification.	16.06.16	LJ
B	OCC Standard sub-base & capping layer table added & road construction thickness of 3% CBR added.	14.06.16	LJ
A	Drawing status changed to 'For Construction'	28.08.15	OB
-	First Issue	15.07.15	JB

Rev.	Description	Date	By
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Client	
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Project	Cotefield Farm Bodicote
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Title	Construction Details Highways
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Scale	NTS	Drawn	CF
Date	July 15	Checked	JB
File	15031/dwgs/civils/current	Drawing	15031 / 300 F