



# Land To The South of Banbury Rise, Banbury

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## DESIGN CODE

**PEGASUS**  
GROUP

**BLOOR HOMES**

**“The creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities...”**

(Para. 126, NPPF 2021)

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# 01 Introduction & Background

## **THE VISION AT LAND SOUTH OF BANBURY RISE**

- 1.1 The overarching vision at Land South of Banbury Rise is to create a development which builds upon the established principles of Banbury Rise to the north. The site offers the opportunity to continue the gradual transition from the urban fringe and the open countryside.
- 1.2 An extension to the Banbury Rise community of 250 new dwellings, public open space including sustainable urban drainage and children's play area. Dwelling design responds positively to the existing character of the surrounding area and celebrates Banbury's architecture as well as respecting its setting along the rural edge by drawing on design influences from the adjacent Banbury Rise development. A landscape-led approach to place making ensures built form and landscape features work in harmony and are the defining character to the public realm. Clear vehicular, pedestrian and cyclist routes are provided within a legible and permeable movement network.
- 1.3 The Vision at Land South of Banbury Rise seeks to continue the best elements of Banbury Rise to create an attractive and readily accessible framework to establish a vibrant and sustainable settlement and community.



## PLANNING BACKGROUND

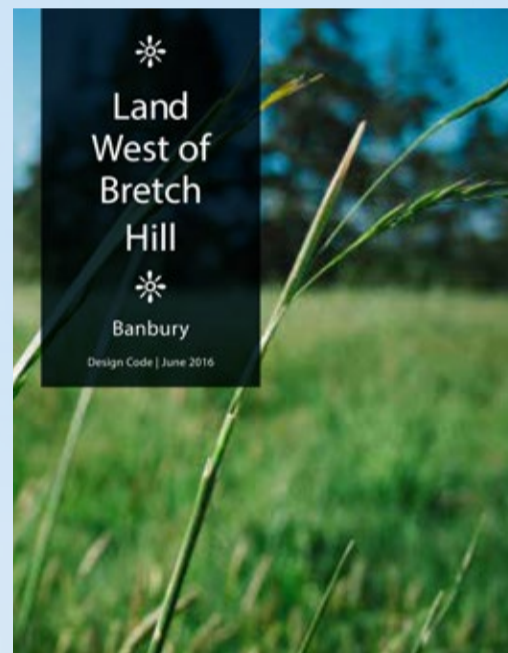
- 1.4 This Design Code has been prepared by Pegasus Urban Design on behalf of Bloor Homes, pursuant to condition 24 set out in the Outline Planning Application (22/O2101/OUT) which was granted permission in September 2023.
- 1.5 The description of the Outline Planning Application reads:  
*“Outline planning application for a residential development comprising up to 250 dwellings (with up to 30% affordable housing), public open space, landscaping and associated supporting infrastructure. Means of vehicular access to be determined via Edinburgh Way, with additional pedestrian and cycle connections via Dover Avenue and Balmoral Avenue. Emergency access provision also via Balmoral Avenue. All other matters reserved.”*
- 1.6 As part of the Outline Planning Application and subsequent approval the pre-commencement condition 24 states that:  
*“No reserved matters applications shall be made or development commenced until a Design Code for the site has been produced in accordance with Condition 25 and following consultation with the Local Planning Authority and other stakeholders, and has been submitted to and approved in writing by the Local Planning Authority. The development shall thereafter be carried out in accordance with the approved Design Code.”*
- 1.7 Following discussions with Cherwell District Council during the reserved matters pre-application advice process, it has been agreed that Bloor Homes will prepare a single document (this Design Code) that will include the necessary and appropriate level of detail to address condition 25, which will be prepared in tandem with the Reserved Matters Pre-Application. This design code has been informed by 3no. pre-application advice meetings with the district council and supports the reserved matters proposal.
- 1.8 This Design Code and the design principles established within it have been formed having due regard to the approved Masterplan, Parameter Plans, Design and Access Statement and other plans and documents that were approved at the outline stage. Further to this, the design principles established as part of the adjacent Banbury Rise development specifically Phase 3 as agreed with the district council, are an important element in this design code.



AERIAL SITE LOCATION PLAN

## **BANBURY RISE (NORTH OF THE SITE)**

- 1.9 Bloor Homes submitted an outline application for the construction of up to 400 residential dwellings (including extra care), employment, open space, access and associated infrastructure which was approved in March 2016 (ref: 13/00444/OUT).
- 1.10 This application site lies to the south of the Banbury Rise development. Banbury Rise is split into three phases. Phases 1 and 2 are built and phase 3 is nearing completion. This application site is to be designed as a continuation of Banbury Rise, following similar design principles. Further detail on Banbury Rise can be found later in this document.
- 1.11 A Design Code was approved to fulfil Condition 6 of the Outline Planning Permission (13/00444/OUT) relating to Banbury Rise (Phases 1, 2 and 3) located to the north of the site. Following this, a full planning application (ref 17/00189/F) was approved relating to parcels on the central and southern parcels of the site. This was also supported by a Design Code.
- 1.12 These codes provided detailed design guidance that facilitated the delivery of high quality design at Banbury Rise.



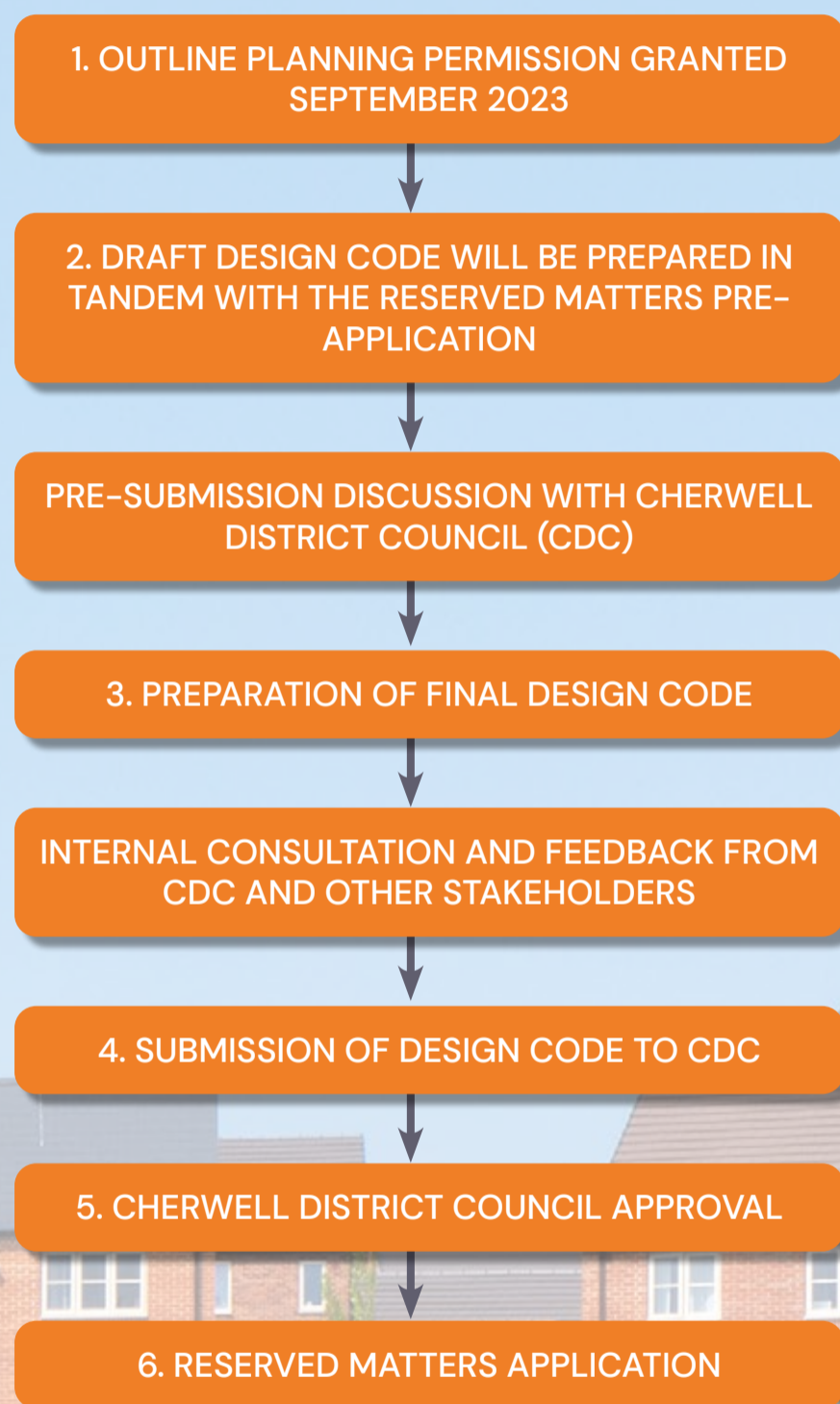
OUTLINE DESIGN CODE

## **PURPOSE OF THE DESIGN CODE**

- 1.13 During the Reserved Matters Pre-Application advice process, Cherwell District Council and Bloor Homes have discussed the requirements for the Design Code to guide the development at Land South of Banbury Rise. The objective of producing the Design Code is to provide a clear framework for development, that is supported by all parties, to ensure the quality of design is achieved without over complicating the process.
- 1.14 The Design Code is intended to:
- Establish a long-term vision, and a design-led framework for the site;
  - Build upon the work established in the outline and full planning application, and the design and access statement for the site;
  - Provide a level of certainty to the landowner, Local Authority, Developer and the community;
  - Provide a clear guide for developers working on individual development parcels; and
  - Set the context for more detailed design work required at the Reserved Matters stage.
- 1.15 The Reserved Matters application for Land South of Banbury Rise will be submitted in accordance with the Code. However, variations away from the Code may be considered acceptable in exceptional circumstances where design or site constraints dictate a more suitable alternative approach.

## **DESIGN CODING PROCESS**

- 1.16 Design Codes are a form of detail design guidance that establishes the two and three dimensional design elements of a particular development. The flow diagram presented below illustrates the process undertaken in conjunction with Cherwell District Council, in the preparation of the Reserved Matters Pre-Application and this Design Code.





RED LINE CONTEXT PLAN



# 02. Outline Masterplan, Design And Access Statement & Landscape Strategy

2.1 The following plans and documents form part of the Outline Planning Permission and shape the detailed design of the site. The key design principles established are carried through this Design Code and subsequent Reserved Matters applications.

- P20-1853\_04C Indicative Masterplan (July 2022)
- P20-1853\_6B Banbury Rise DAS (July 2022)
- 10511-FPCR-XX-XX-DR-L-0001-PO1 01-LANDSCAPE STRATEGY D (June 2022)

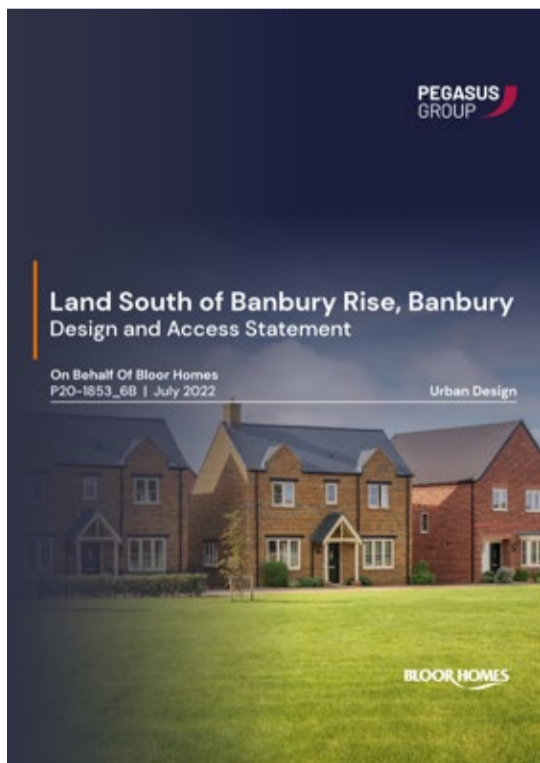
2.2 The Indicative Masterplan highlights the broad development parcels within the existing setting and combines other principles such as key links and the primary access point. In addition, the illustrative masterplan also shows the location of the other land uses including the public open space, attenuation basins and Local Equipped Area of Play.

2.3 The Design and Access Statement explains how the proposed development is a suitable response to the site and its setting and demonstrates that it can be adequately accessed by future residents. The document assesses the existing context and sets out the broad design principles and presents the design proposals including the uses, built form, movement and public open space.

2.4 The Landscape Strategy illustrates the new planting proposals, which enhances the site's connection within the wider green infrastructure setting and provides new habitat opportunities. This plan also highlights the natural continuation of the western public open space from Banbury Rise to the north of the site extending the Linear Park.



LANDSCAPE STRATEGY PLAN



P20-1853\_6B DAS



KEY			
	SITE LOCATION		PRIMARY VEHICULAR ACCESS
	EXISTING VEGETATION		PEDESTRIAN/CYCLIST ACCESS
	PUBLIC RIGHT OF WAY		PEDESTRIAN/CYCLIST/EMERGENCY ONLY ACCESS
	PROPOSED FOOT & POTENTIAL CYCLE LINKS		PRELIMINARY DRAINAGE LOCATIONS
	INDICATIVE PLANTING		HOUSING (up to 250 Dwellings)
	LOCALLY EQUIPPED AREA FOR PLAY		POTENTIAL FUTURE LINK TO WITHYCOMBE FARM (RESIDENTIAL TRAFFIC ONLY)
			WODAL SQUARE

APPROVED ILLUSTRATIVE MASTERPLAN



**KEY**

	RESIDENTIAL BUILT FORM		EXISTING PUBLIC RIGHT OF WAY		PRELIMINARY DRAINAGE LOCATIONS
	PRIMARY VEHICULAR ACCESS		PROPOSED FOOT & POTENTIAL CYCLE LINKS		EMPLOYMENT LAND
	POTENTIAL FUTURE LINK TO WITHYCOMBE FARM (RESIDENTIAL TRAFFIC ONLY)		NODAL SQUARE		
	PEDESTRIAN/CYCLIST ACCESS		LOCAL EQUIPPED AREA OF PLAY		
	PEDESTRIAN/CYCLIST/EMERGENCY ONLY ACCESS		INDICATIVE PLANTING		

COMBINED MASTERPLAN SHOWING PHASES 1, 2 AND 3 OF BANBURY RISE AND THIS APPLICATION TO THE SOUTH

# 03. Analysis & Assessment

3.1 This section provides a summary of the assessment of the site and its surroundings that has been undertaken.

## **SITE'S IMMEDIATE CONTEXT**

3.2 The site comprises circa 14.09 hectares of land on the western edge of Banbury, this includes the access running through the recently built Banbury Rise development to the north of the site. The site is bound by residential development to the east, Banbury Rise development to the north and open fields to the south and west. Withycombe Farm is located to the west of the site.

3.3 The consent of Banbury Rise sets the precedent for development in this location and it has been demonstrated that the site is reasonably located to access local services and public transport opportunities.

## **CHARACTER INFORMED BY CONTEXT**

3.4 The identification of defining urban design principles from the existing development of Banbury Rise provides useful design cues that can be reinterpreted and incorporated within modern development.

3.5 The assessment of Banbury Rise has highlighted that each phase within the development have similar design principles, therefore providing a consistent character that would naturally continue south into this site. These have provided key features that will help guide this Design Code and subsequent Reserved Matters application.





PRIVATE DRIVES LOCATED ON THE EDGE OF THE DEVELOPMENT.



HIGHER PERCENTAGE OF RECONSTITUTED IRONSTONE FRONTING ONTO THE OPEN SPACE, CREATING A SOFT TRANSITION TO THE SURROUNDING COUNTRYSIDE.



DWELLINGS FRONTING OUT ONTO THE OPEN SPACE FEATURE GABLES AND A MIX OF MATERIALS TO BREAK UP THE STREET SCENE.



DWELLINGS TURN CORNERS TO PROVIDE SURVEILLANCE.

PHOTOGRAPHS OF THE BANBURY RISE DEVELOPMENT

PHASE 1

PHASE 2

PHASE 3



## BANBURY RISE

- 3.7 Bloor Homes' Banbury Rise development is nearing completion and is located to the north of the site. This scheme is a LABC award-winning residential-led development. Phases 1 and 2 are built and phase 3 is currently under construction.
- 3.8 As illustrated on the Character Area Analysis Plan opposite, various design principles have been identified. Typically, these design principles help to positively influence and inform the design approach.
- 3.9 Three character areas have been chosen to study which demonstrates distinct style of urban form and building details of the newly built development to the north of the site.

Eastern Edge	
Built form	Mix of terraced, semi-detached and detached dwellings.
Architectural detail	Simple vernacular with flat facades, projecting porches, pitched roofs and occasional hipped roofs.
Building line	Consistent with occasional set back behind parking.
Materials	Predominantly red brick with dark grey and brown roof tiles.
Building heights	Generally 2 storey with occasional 3 storey in key locations. 3 storey apartments are located on the eastern edge.
Main Street	
Built form	Predominantly detached dwellings with the occasional semi-detached and terrace dwellings.
Architectural detail	Generally flat facades, projecting porches, gablets, pitched roofs, occasional hipped roofs, dormer windows, brick corbelling to the eaves and contrasting brick banding.
Building line	Consistent with occasional and sometimes significant set backs.
Materials	Predominantly buff brick with dark grey and brown roof tiles. Occasional use of render at corner accent locations. Occasional Reconstituted Ironstone.
Building heights	Predominantly 2 and 2.5 storey.
Rural Edge	
Built form	Predominantly detached or larger semi-detached.
Architectural detail	Variation in flat and protruding facades, bay windows, brick banding and pitched porches. Occasional use of tudor boarding.
Building line	Varied set back with varied size front gardens.
Materials	Red and mixed red brick with dark grey and brown roofs. Occasional Reconstituted Ironstone.
Building heights	Maximum 2 storey.



## BANBURY RISE ANALYSIS

- 3.10 As agreed with Cherwell District Council, Phase 3 has been analysed in detail as this part of the Banbury Rise development is located immediately to the north of the site. This phase comprises 137 houses and serves as the only vehicular access into Phase 4.
- 3.11 The entrance to Phase 3 is accessed from Bailey Road. Wilson Road is the main street leading into the development and then turns into Longley Crescent. Continuous built form fronts onto the main street with a green square situated along the main street to add interest and aid way-finding.
- 3.12 The design and materials of the development have been informed by the Design Code for this Phase.



BANBURY RISE SET ANALYSIS PLAN

## ANALYSIS OF SET BACK DISTANCES

- 3.13 The development in Phase 3 has a varied set back distance giving the opportunity for soft landscaping along the streets. Buildings are set back from the street allowing for front gardens with tree planting. The buildings loosely follow the road and therefore the buildings define the space rather than the road.



BANBURY RISE SET BACKS PLAN

## ANALYSIS OF EXISTING BUILT FORM

3.14 Along the main street, the majority of the built form is semi-detached or detached with parking to the side or to the front of houses. The ridge and eaves heights are generally similar with subtle changes along the street due to 2.5 storey dwellings. This reflects the subtle change in ridge and eaves height found in Banbury Rise





CONCEPT PLAN TAKEN FROM OUTLINE DESIGN AND ACCESS STATEMENT



# 04. Outline DAS Design Principles

## **HERITAGE & VISUAL SENSITIVITIES**

The development must respect the Grade II listed 'Withycombe Farmhouse' which adjoins the site. The development will require careful consideration to ensure it is not overly prominent visually in the context of Withycombe Farm. Existing natural screening along the western edge of the development will soften the visual apartness of the development in this context. This will be enhanced as part of the proposals.

Consideration to be had of the site topography and the highest point to the south of the site.

## **GREEN INFRASTRUCTURE**

The development must consider the existing landscape and enhance the ecology of the site. The development should be a natural continuation of the western green corridor from Banbury Rise to the north of the site. Existing established vegetation should be retained and enhanced where possible.

## **RELATIONSHIP TO BRETCH HILL AND BANBURY RISE**

The development should respect the privacy and amenity of the existing properties to the north and east of the site. Consideration to be had to the linear park to the north of the site and to continue the pedestrian connections throughout the site to allow existing residents to access the new development and its amenities.

## **ACCESS**

Designs shall maintain and enhance existing pedestrian routes to ensure easy and convenient access to the adjacent communities and surrounding landscape. To the north west, a Public Right of Way crosses the site and this has been retained in the proposals.

## **MOVEMENT**

It is important to establish a clear movement hierarchy to provide simple movement around the site. Key routes will follow desire lines to allow for ease of movement throughout the site and more direct access to local facilities.

## **DEVELOPMENT BLOCKS**

The development blocks shall be designed to allow for good surveillance to surrounding streets and open space as well as providing private amenity spaces for dwellings.



**KEY**

- APPLICATION BOUNDARY
- LISTED BUILDINGS AT WITHYCOMBE FARM

**CHARACTER AREAS**

- CA1: MAIN STREET
- CA2: CORE HOUSING
- CA3: RURAL EDGE

**LEGIBILITY & PLACEMAKING**

- MANDATORY EDGE
- ADVISORY EDGE
- GREEN FOCAL SPACE
- CORNER ACCENTS
- VIEWS MAXIMISED TO LANDSCAPE FEATURES

**MOVEMENT**

- MAIN VEHICULAR ACCESS VIA PHASE 3 BANBURY RISE
- POTENTIAL LINK
- EXISTING PUBLIC RIGHT OF WAY
- INDICATIVE PEDESTRIAN ROUTE
- PEDESTRIAN CONNECTION TO DOVER AVENUE & WIDER COUNTRYSIDE
- EMERGENCY ACCESS ONTO BALMORAL AVENUE

**GREEN INFRASTRUCTURE**

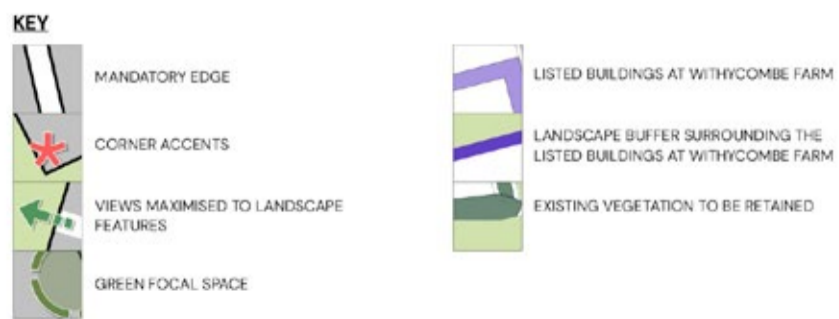
- OPEN SPACE
- INDICATIVE LOCATION OF LOCAL EQUIPPED PLAY AREA (LEAP)
- EXISTING VEGETATION TO BE RETAINED
- INDICATIVE LOCATION OF ATTENUATION BASINS
- ARCHAEOLOGICAL NO BUILD AREA

# 05. Overarching Regulating Plan

- 5.1 This is a two-dimensional plan that simplifies the Design Code principles. It displays factors that may affect the design development and highlights the features of the site to be focused on. It therefore sets out the design code development parameters, which include:
- Proposed land uses;
  - Building heights;
  - Key frontages;
  - Key focal spaces;
  - Key building locations
  - Corner Accents
  - Character areas; and
  - Landscaping/open space typology areas.
- 5.2 Further design coded elements that are not shown on the Regulating Plan but set development parameters within this document include:
- Density;
  - Street/Movement hierarchy (vehicular and pedestrian);
  - Building typologies;
  - Housing mix and affordable housing design principles;
  - Architectural style and appropriate detailing;
  - Building materials and use of colour;
  - Boundary treatments; and
  - Public realm design.

## UNITS OF CHARACTER

- 5.3 The new built environment is coded by dividing the following units of placemaking into different components:
- General Urban Design Principles
  - Character Area Guidance
  - Street Typologies
  - Building Form Guidance
  - Public Realm



## **GENERAL URBAN DESIGN PRINCIPLES**

5.4 The following section sets out and establishes general urban design principles that are to be used to inform the detailed design of the Reserved Matters Application, alongside the above-mentioned units of character. The plan opposite visually represents the General Urban Design Principles.

### **KEY OUTWARD FACING FRONTAGES**

5.5 Key outward facing frontages will be particularly prominent and critical to the appearance of the development. Particular attention should be paid to the massing, materials and architectural detailing of buildings framing key open spaces and streets, to ensure these buildings have frontages that would contribute towards creating a unique and memorable experience of distinctive quality and character.

5.6 Designs for these frontages should be approached using the composite street elevations, where emphasis is given to the contextual surrounding and the nature of the public realm to be created.

## **LEGIBILITY AND WAYFINDING**

5.7 Key buildings, nodal points and a clear hierarchy of routes and intersections are considered to increase the legibility of the development. Legibility refers to the degree to which people can understand and identify with the built environment. Building and layout design, planting and views will be utilised to form visual focal points and create identifiable routes.

### **NODAL POINTS**

5.8 Nodal points are located within the development to help provide variety and diversity in the public realm, and aid the creation of a well-balanced development. Visual links between nodal points tie the proposals together, ensuring that the development is easy to navigate. The visual linking of the built environment with the open space and landscape features further enhances a verdant residential character, through the visual in heritage of open space and landscape.

### **CORNER ACCENTS**

5.9 Prominent corners of the development are key to aiding legibility and way-finding and should provide animation and surveillance to the street, with dual fronted dwellings facing both sides of the public realm.

**Key principles:**

- Variation in materials;
- Corner turner buildings will provide surveillance to both sides of the junction;
- Buildings to provide active frontage to the street; and
- Set back from the adoptable highway to allow for landscaping and/or suitable boundary treatment.

5.10 Corner Accents identified on the Urban Design Principles Plan frame the key views identified, however the precise number and location of these are subject to detailed design during any Reserved Matters Application.



EXAMPLE OF A CORNER ACCENT DWELLING

### **URBAN FORM AND MORPHOLOGY**

- 5.11 The way that buildings relate to one another is one of the most important aspects that can be used to define an areas character. The proportion, massing, shape and layout of buildings are important elements of character. Other cues such as defining building lines, eaves heights, ridge heights, alongside the rhythm/spacing between buildings will be important in establishing formal or informal character areas.
- 5.12 The key aspect of urban morphology will therefore be addressed for each character area and include:
- Urban form (relationship of buildings to one another);
  - Building typology (terrace, detached etc);
  - Density (generally higher in development core and lower where transition to wider landscape);
  - Building lines (consistent or varied);
  - Height/Enclosure;
  - Roofscape (roof form, consistent or varied eaves/ridge heights);
  - Scale and proportion of the buildings and its fenestration (important for both urban form and detail);
  - Building details;
  - Building materials;
  - Landscape Design; and
  - Parking.

### **DEVELOPMENT BLOCK PRINCIPLES**

- 5.13 Broad block structures have been established within the outline planning permission, its related plans and documents and by the Regulatory Plan. The following parameters and principles can be established to inform the detailed design:
- Dwellings within the blocks will form back-to-back relationships, thereby enclosing rear gardens;
  - The exterior of the blocks will be defined by the fronts of dwellings and exposed side elevations must be active; and
  - On plot parking will be provided, in the form of detached/integral garages or hard surfaced parking areas.



## DENSITY

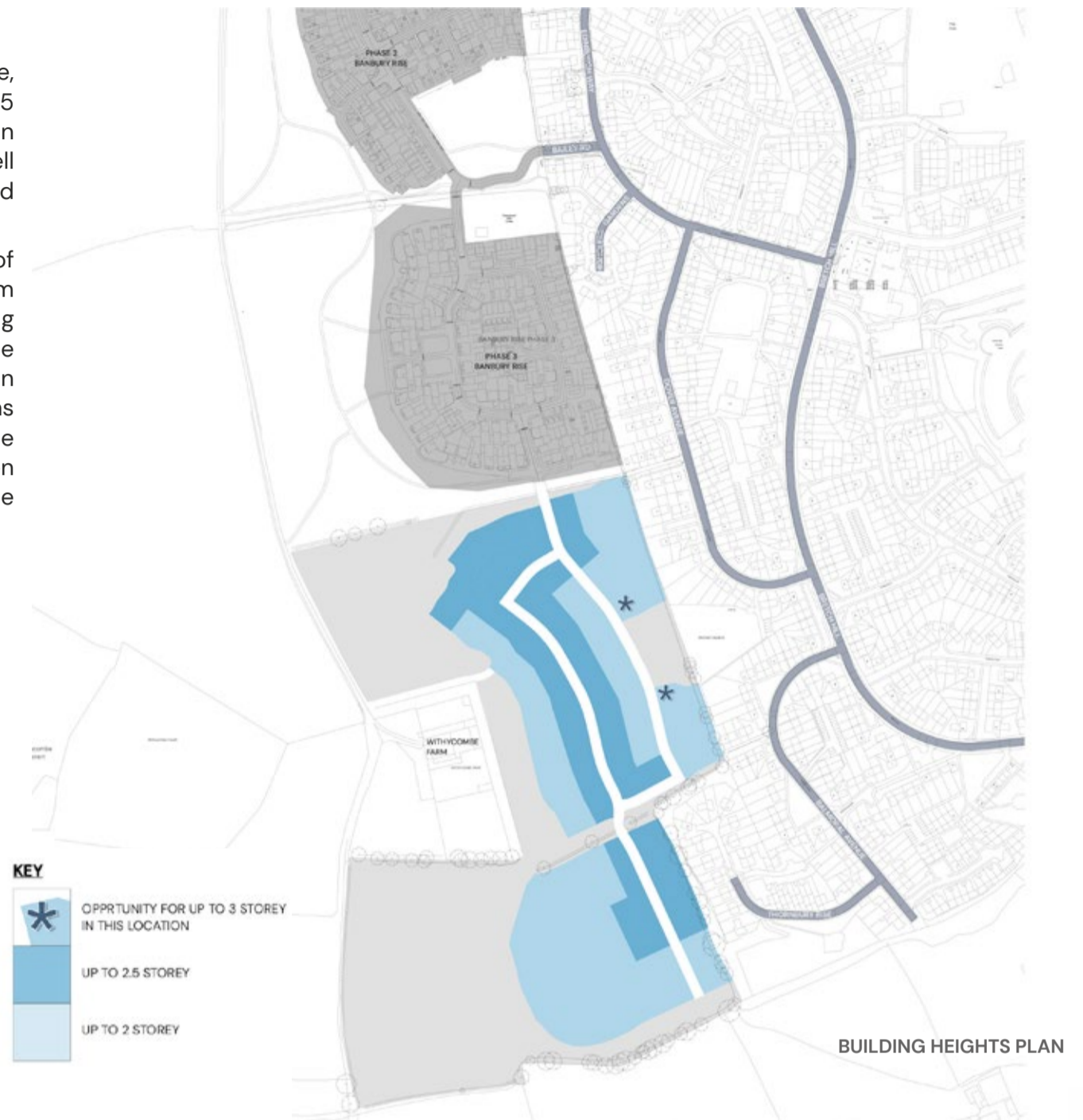
- 5.14 A variety of densities are proposed through the site, averaging 35 dwellings per hectare (dph).
- 5.15 Higher density development will be located along the Main Street, a continuation of Banbury Rise to the north. The use of higher densities in these locations will help reinforce the connection of the proposed development to the recent development to the north of the site, and aid in legibility and wayfinding of the site, helping to define key routes through the proposals.
- 5.16 Lower density development will be located where it would be appropriate to enhance the more rural and informal character of the proposals, such as the southern and western edges.
- 5.17 Medium density will generally be located towards the eastern side of the site and areas of the site which doesn't benefit from being either high or low density.

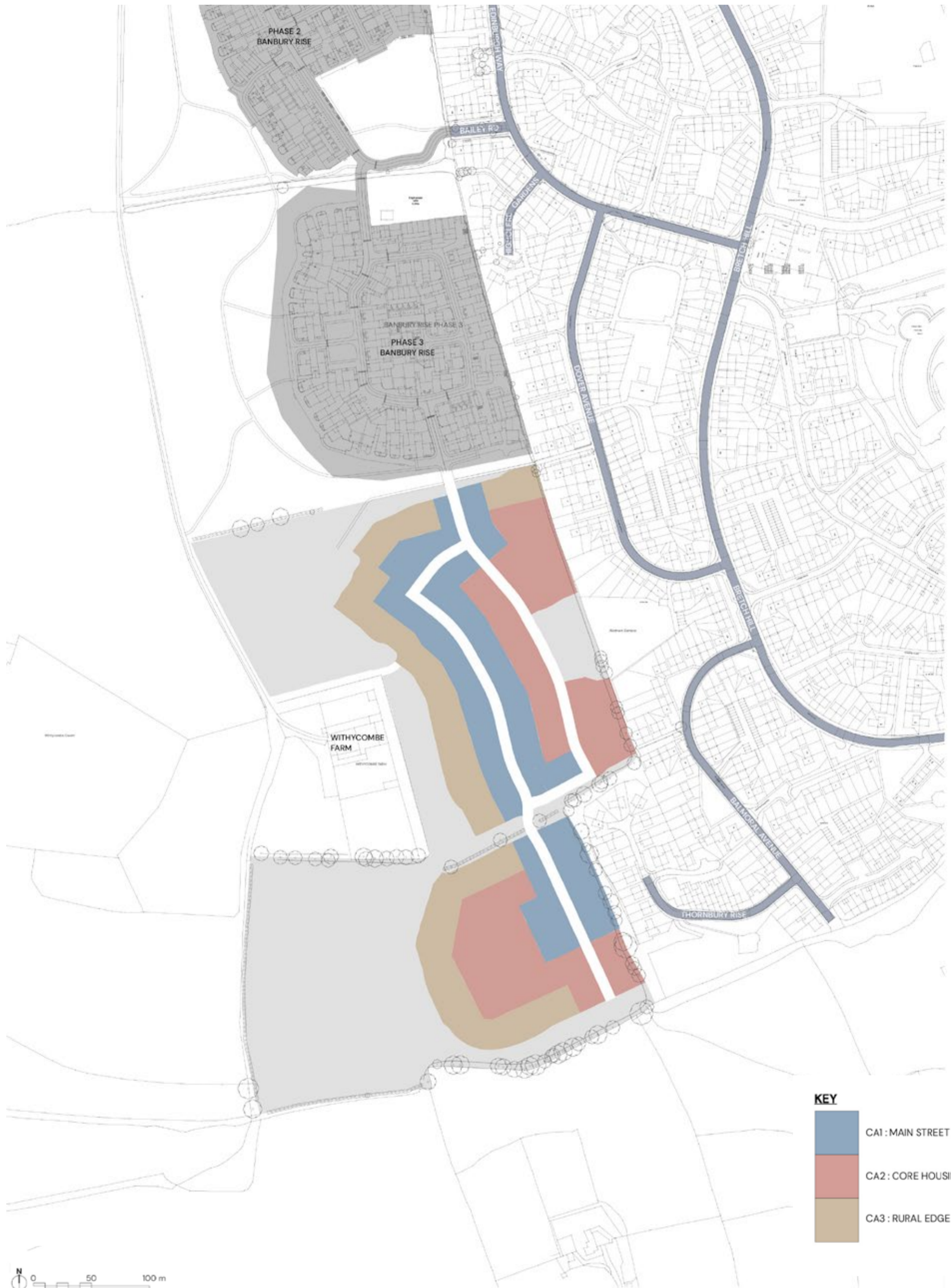




## BUILDING HEIGHTS

- 5.18 Building heights will vary across the site, ranging from approximately 5m to 11.5 m. This range allows for bungalows in more sensitive areas of the site as well as 3 storey apartments to articulate and create interest within the street scene.
- 5.19 The site will predominantly consist of development up to 2 storey (up to 9m ridge height) reflecting the surrounding built form of Banbury. However, the use of increased storey heights can aid in legibility and wayfinding, as such areas of up to 3 storey development will be located adjacent to the eastern open space to define key frontages along the primary route.





**KEY**

	CA1 : MAIN STREET
	CA2 : CORE HOUSING
	CA3 : RURAL EDGE

CHARACTER AREAS PLAN

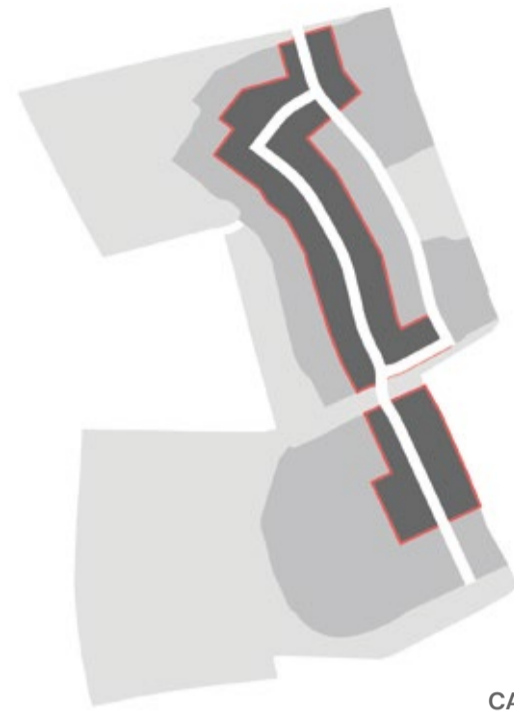
## 06. Character Area Coding

- 6.1 Character Areas are a useful way of helping assimilate the design proposals within its surroundings, whilst providing a continuity of themes across the development. The site has been split into three distinct character areas, each with a clearly defined character relating to the site's context and surroundings. This is a refinement to the character areas set out in the Outline Design and Access Statement and are a natural continuation of the character areas within the Banbury Rise development to the north of the site. The following pages describe how the character areas should be designed in such a way to help create a varied and diverse townscape.
- 6.2 The character areas are detailed below as follows:
- CA1: The Main Street
  - CA2: Core Housing
  - CA3: The Rural Edge
- 6.3 Each character area is defined via a range of design components including building typology, height, enclosure, building detail, building material and other key elements that shape the overall appearance of the identified character areas.
- 6.4 Further information regarding the proposed character areas is set out later in this section of the Design Code.



## CA1: THE MAIN STREET

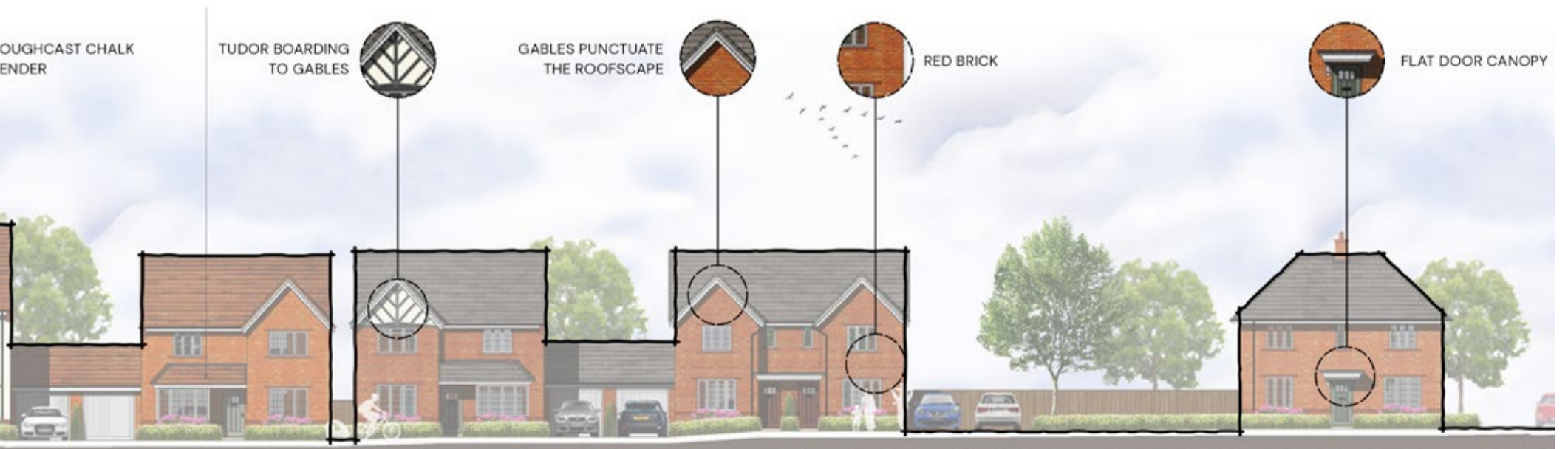
- 6.5 The Main Street character area covers the primary route that runs through the scheme and is characterised by the most formal development. It's main objective is to confirm the Primary Street and it's function through the site. The character of The Main Street will draw from the existing character found within The Main Street character area in Phase 3 Banbury Rise. The character will link with Banbury Rise to create a cohesive scheme with variation in materials, street trees and boundary treatments create variation in the street scene.
- 6.6 The use of consistent building lines will aid a cohesive character and help define a more formal character. However, groups of frontage parking could be incorporated, to provide a break in the building line and to allow for tree planting in between parking spaces. Private spaces will be clearly demarked from the public realm through the incorporation of soft landscaping.
- 6.7 Dwellings will range between 2 storey and 3 storey with 2.5 and 3 storey being located around key spaces and to dwellings terminating key views. Consistency in plot widths and boundary openings to the street are encouraged, aiding in rhythm and repetition within the street scene. Consistent methods of architectural detailing are to be incorporated.
- 6.8 The accompanying table, plans and imagery address the design components required for this character area.



CA LOCATION PLAN

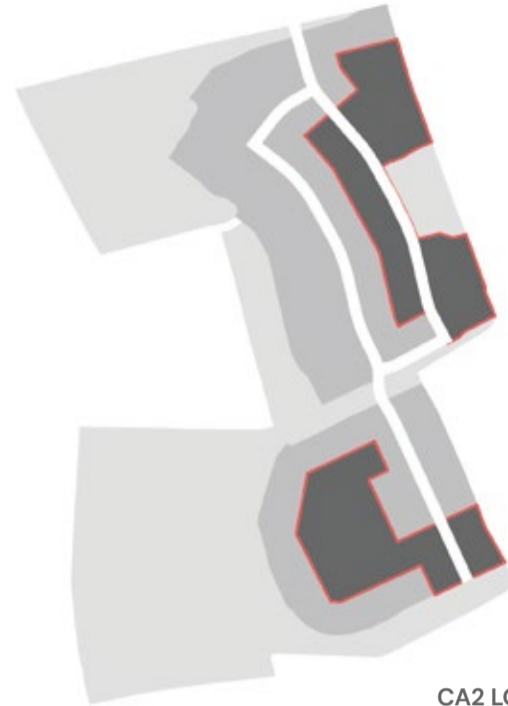
The Main Street	
<b>Form and Massing</b>	<ul style="list-style-type: none"> <li>• Simple vernacular.</li> <li>• Flat facades.</li> <li>• Prominent gables.</li> <li>• Pitched/flat door canopy.</li> </ul>
<b>Building and Roof Lines</b>	<ul style="list-style-type: none"> <li>• Predominantly consistent building line with occasional significant variation.</li> <li>• Variation in ridge lines due to topography and differences in building heights and hipped roofs.</li> </ul>
<b>Set-back and Verges</b>	<ul style="list-style-type: none"> <li>• Varies between 1.5m – 7m.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>• Predominantly Red brick with occasional chalk render.</li> <li>• Grey or Brown roof tile.</li> </ul>
<b>Parking</b>	<ul style="list-style-type: none"> <li>• Allocated parking will be on plot, either to the side of dwellings or to the front.</li> </ul>
<b>Character of Street and Public Realm</b>	<ul style="list-style-type: none"> <li>• Consistent character featuring porches, bay windows and dormers.</li> <li>• Formal and informal key spaces with a strong landscape emphasis.</li> <li>• Frontage parking is broken up by landscaped strips and/or tree planting.</li> </ul>
<b>Continuity of Frontage</b>	<ul style="list-style-type: none"> <li>• Predominantly continuous frontage.</li> </ul>
<b>Building Type</b>	<ul style="list-style-type: none"> <li>• A mix of terraced, semi-detached and detached dwellings.</li> </ul>
<b>Building Heights</b>	<ul style="list-style-type: none"> <li>• Predominantly 2 storey with occasional 2.5 storey and key locations.</li> </ul>
<b>Density</b>	<ul style="list-style-type: none"> <li>• Medium density</li> </ul>





## CA2: CORE HOUSING

- 6.9 The Core Housing character area covers the development area between CA1 and the existing Bretch Hill and will provide a transition between the more formal character of CA1 and the existing residential area. The character of this character area will draw from the Core Housing character area in Phase 3 Banbury Rise to compliment the adjacent residential area whilst also providing a cohesive development.
- 6.10 Dwellings in this character area will generally be served from secondary streets or tertiary streets. The building line will typically be consistent and will define the boundary of the public realm. Dwellings will typically be 2 storey in height with occasional 2.5 storeys at key locations and set back behind private amenity space which will incorporate soft landscaping where possible.
- 6.11 The accompanying table, plans and imagery address the design components required for this character area.



CA2 LOCATION PLAN

Core Housing	
<b>Form and Massing</b>	<ul style="list-style-type: none"> <li>• Simple vernacular.</li> <li>• Flat facades.</li> <li>• Prominent gables.</li> <li>• Pitched roof porches.</li> </ul>
<b>Building and Roof Lines</b>	<ul style="list-style-type: none"> <li>• Predominantly consistent building line with occasional variation.</li> <li>• Variation in ridge lines due to topography and building heights.</li> </ul>
<b>Set-back and Verges</b>	<ul style="list-style-type: none"> <li>• Varies between 1m – 8m.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>• Predominantly Red brick.</li> <li>• Materials will be consistent within groupings.</li> <li>• Typically dark grey roof tiles with occasional brown roof tile.</li> </ul>
<b>Parking</b>	<ul style="list-style-type: none"> <li>• Allocated parking will be on plot, either to the side of dwellings or to the front.</li> </ul>
<b>Character of Street and Public Realm</b>	<ul style="list-style-type: none"> <li>• Consistent character featuring porches, bay windows and dormers.</li> <li>• Typically informal grassed incidental spaces.</li> </ul>
<b>Continuity of Frontage</b>	<ul style="list-style-type: none"> <li>• A mix of continuous and discontinuous frontage.</li> </ul>
<b>Building Type</b>	<ul style="list-style-type: none"> <li>• A mix of terraced, semi-detached, detached and apartments.</li> </ul>
<b>Building Heights</b>	<ul style="list-style-type: none"> <li>• Predominantly 2 storey with occasional 2.5 and 3 storey.</li> </ul>
<b>Density</b>	<ul style="list-style-type: none"> <li>• High Density</li> </ul>





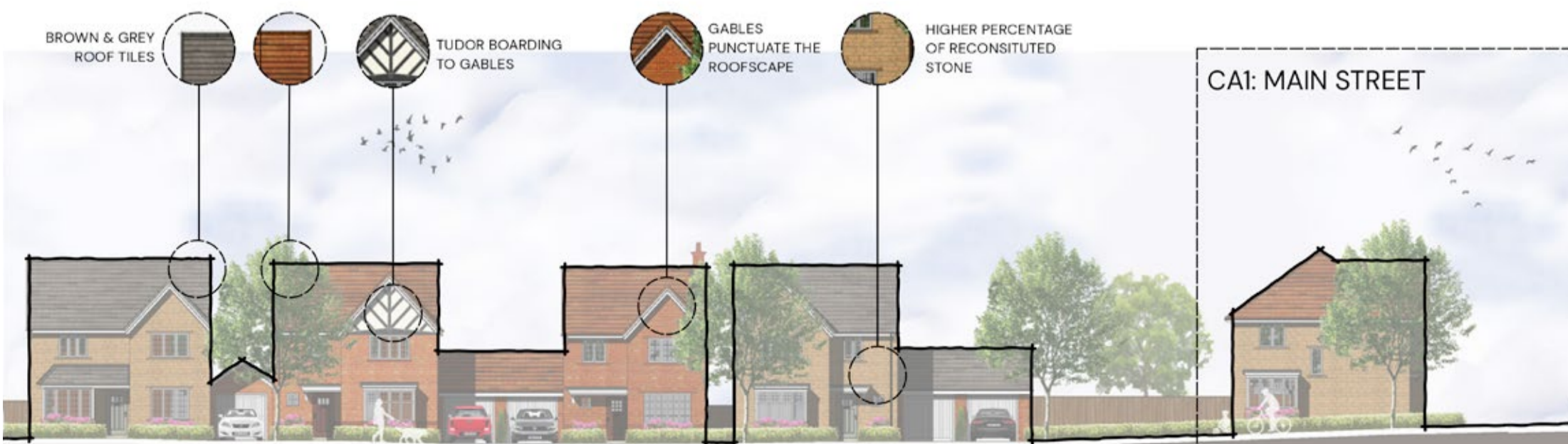
### CA3: THE RURAL EDGE

- 6.12 The Rural Edge character area forms the northern, western and southern edges of the development and will continue the western edge to Banbury created by Banbury Rise. Views out to the surrounding countryside are a defining feature of this character area, that will draw the green environment into the built form. As such, the architectural approach will be less formal than the other character areas. The character of the Rural Edge has been inspired by the western edge of Banbury Rise to the north of the site.
- 6.13 Dwellings in this character area will generally be larger, detached and served by private drives, thus creating a more irregular street pattern to aid in the creation of a more informal character. Dwellings within this area will typically be formed by a lower density arrangement of buildings with a less formal relationship with the public realm, suggesting a more rural feel with a strong presence of soft landscaping and the incorporation of estate railings to define the public realm.
- 6.14 The accompanying table, plans and imagery address the design components required for this character area.



CA LOCATION PLAN

The Rural Edge	
<b>Form and Massing</b>	<ul style="list-style-type: none"> <li>• Simple vernacular.</li> <li>• Flat facades.</li> <li>• Prominent gables.</li> <li>• Pitched roof porches.</li> </ul>
<b>Building and Roof Lines</b>	<ul style="list-style-type: none"> <li>• Fluid building lines, irregular in shape, helping to create a loose appearance.</li> <li>• Variation in ridge lines due to topography and building heights.</li> </ul>
<b>Set-back and Verges</b>	<ul style="list-style-type: none"> <li>• Varies between 1m – 8m.</li> <li>• Fronting onto open space edge.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>• Red brick or reconstituted stone with occasional black tudor boarding.</li> <li>• A mixture of dark grey and brown roof tiles.</li> </ul>
<b>Parking</b>	<ul style="list-style-type: none"> <li>• Allocated parking will be on plot, predominantly to the side of dwellings, with occasional frontage parking.</li> </ul>
<b>Character of Street and Public Realm</b>	<ul style="list-style-type: none"> <li>• Consistent character featuring porches and dormers.</li> <li>• Block paved shared surfaces and private drives will create a soft low key edge to the open space.</li> <li>• Trees within public and private space will soften the development.</li> </ul>
<b>Continuity of Frontage</b>	<ul style="list-style-type: none"> <li>• Typically a discontinuous frontage.</li> </ul>
<b>Building Type</b>	<ul style="list-style-type: none"> <li>• Predominantly detached dwellings.</li> </ul>
<b>Boundary Treatment</b>	<ul style="list-style-type: none"> <li>• Hedge planting.</li> </ul>
<b>Building Heights</b>	<ul style="list-style-type: none"> <li>• Predominantly 2 storey.</li> </ul>
<b>Density</b>	<ul style="list-style-type: none"> <li>• Low Density.</li> </ul>







## BUILDING FORM GUIDANCE

### STREET SCENE OVERVIEW

6.15 Architectural elements within each building must relate to the requirements of the overall street scene. In particular, all parts of buildings visible from the public realm must be considered as complete architectural compositions, where they collectively form the street scene and impact of the public realm. Guidance includes:

- Create obvious main frontages: street frontages are required to be active. Within residential areas activeness equates to movement at building entrances and visibility through fenestration. Blank facades to any street frontage undermine this principle;
- Treat visible gable end elevations as part of the street scene: windows should be provided to principal elevations, and amended to suit an end/side condition as necessary; and
- Dwellings should be orientated to ensure that living space fronts onto street.

### BUILDING DETAILS

6.16 The materials and details will vary in difference areas of the development. The proposal is for a relatively simple palette of materials to be established that will vary according to the character area and condition.






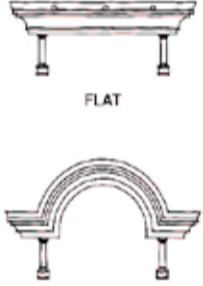

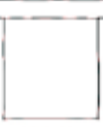





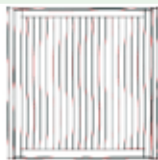
6.17 Details considered include:

- Building details (window arrangement and proportions, balconies etc);
- Building materials for roofs and facades; and
- Scale and proportion of the building and its fenestration (for both urban form and detail).

### ARCHITECTURAL DESIGN

6.18 The aim for the development proposals is to create a varied, identifiable character through modulation of structural form, rather than rely upon superficial decoration in isolation. The architectural design approach encourages details that include:

- Eave depths wide enough to allow shading and modelling on walls: well projected eaves can provide both strong definition of the structures with light and shadow on the façade providing visual interest (rather than arbitrary decoration);
- Simple projections of structure such as bay windows to achieve modulation and shading; and
- Door and window reveals are encouraged (approximately 30-50mm) to give a sense of depth to openings in the elevations, emphasising the relationship of solid and void.

GABLE DETAILS	WINDOW STYLE	WINDOW HEADERS	DETAILING	DOOR STYLE	DOOR CANOPIES
 <p>TUDOR BOARDING (TO OCCASIONAL PLOTS)</p>	 <p>8 MODULE CASEMENT</p>	 <p>SOLDIER BRICK</p>	 <p>BRICK/STONE QUOINS</p>	 <p>PANELLED DOOR WITH GLAZING</p>	 <p>FLAT</p> <p>ARCHED</p>
 <p>BOXED EAVES</p>		 <p>STONE</p>			
BARGEBOARD/SOFFIT/ FASCIA	WINDOW COLOUR	WINDOW CILLS	FLAT BAY WINDOW	DOOR COLOUR	GARAGE DOORS
 <p>WHITE</p>	 <p>WHITE</p>	 <p>STONE</p>	 <p>FLAT BAY WINDOW</p>	 <p>BLACK GREY RED GREEN</p>	 <p>VI</p>

## MATERIALS

6.19 Preference should be given to a limited palette of materials. The range of facing materials used in existing buildings in Banbury Rise, its residential suburbs and the surrounding area are relatively similar to one another and should be the basis for the selection of finishes within the new development. In general:

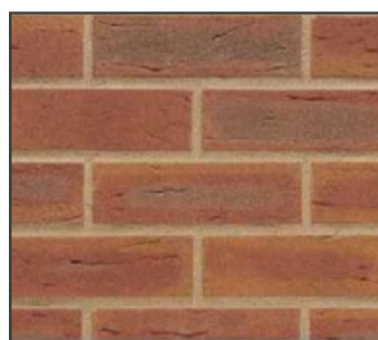
- 3 – 4 finishes should be the maximum in a single elevational composition;
- Materials should not be deployed just for the reasons of variety, but used to express the geometry of the building design, for example to projecting elements, at breaks in the elevation etc; and
- Where buildings are intended as a focus or marker within the development proposals their main architectural elements (i.e. entrances, projecting elements) should be emphasised to create a feature.

## FENESTRATION

6.20 Within each building or group, the main architectural elements form a hierarchy of parts, which should reflect the relative importance of their functions. This applies particularly to the composition of windows and doors within an elevation, making a link between the internal functions of the building and its external environment, including:

- Emphasising entrances – the entrance is the most important part of the front elevation and requires more than just a door to express its significance. Setbacks, recesses, canopies and steps in the façade can all modulate the elevation to emphasise and provide shelter to the entrance;
- Define windows in principal rooms – principal rooms such as lounges and main bedrooms may warrant larger or more prominent windows than other functions like kitchens and bathrooms;
- Arrange windows for comfortable surveillance – this is particularly important at entrances so that occupants have views over entrance paths and doors, and can be achieved through distinctive details such as corner windows and projecting bays; and

6.21 The scale and proportion of windows should be considered in relation to the façade composition. The way this is done will depend on the window type and their vertical and horizontal orientation.



BRICK TYPE 1



BRICK TYPE 1  
WITH CHALK RENDER



BRICK TYPE 1  
WITH BLACK TUDOR BOARDING



RECONSTITUTED  
STONE



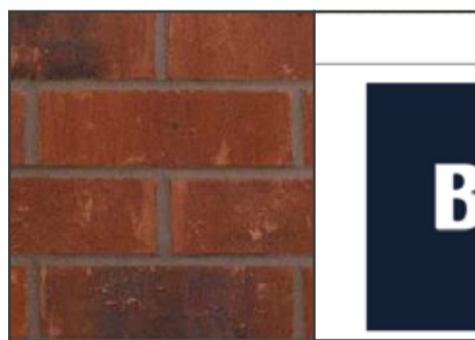
DETAIL BRICK  
STONE



BRICK TYPE 2



BRICK TYPE 2  
WITH CHALK RENDER



BRICK TYPE 2  
WITH BLACK TUDOR BOARDING



GREY ROOF TILE



BRICK TYPE 3



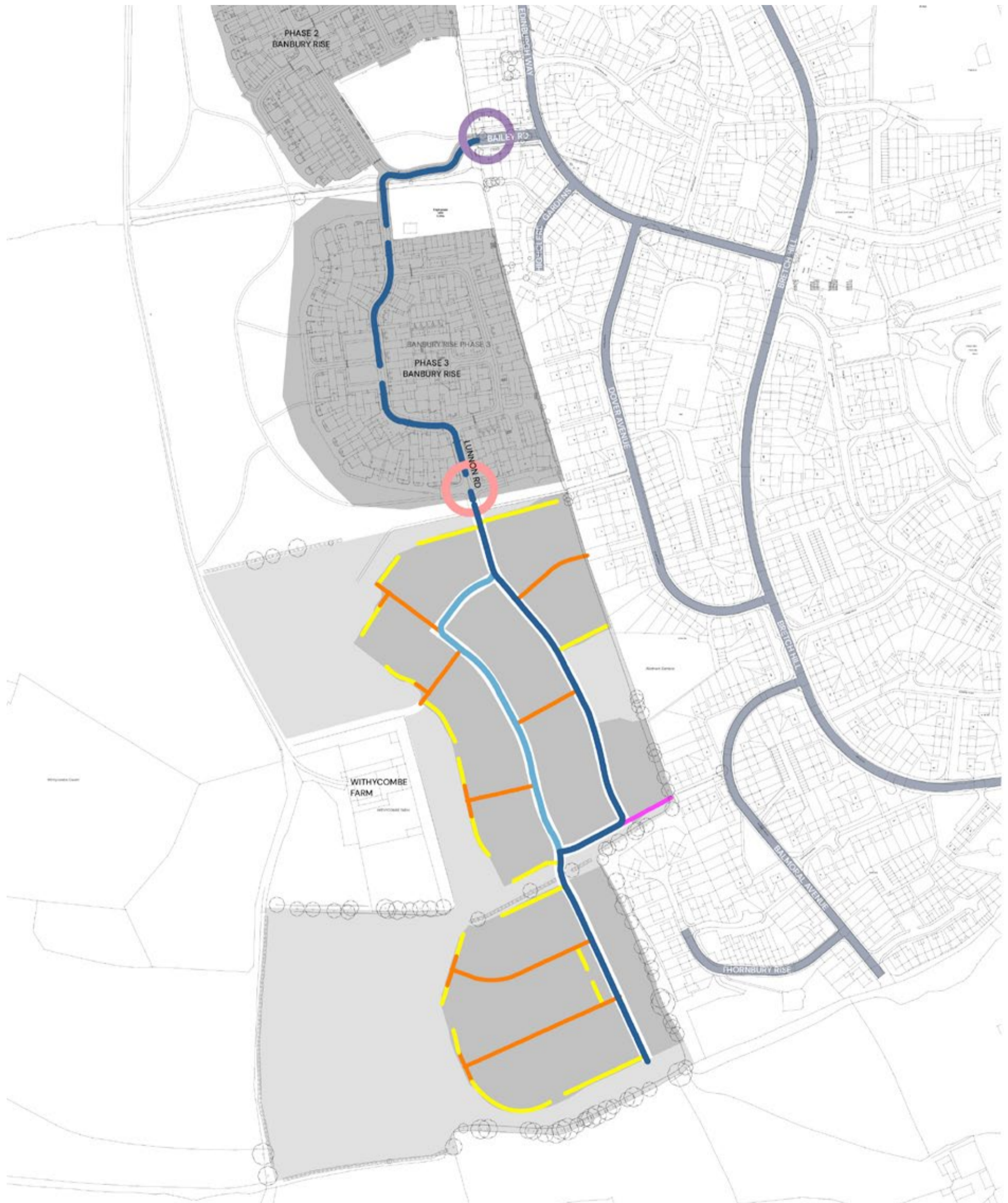
BRICK TYPE 3  
WITH CHALK RENDER



BRICK TYPE 3  
WITH BLACK TUDOR BOARDING



BROWN ROOF TILE



**KEY**

- |   |   |   |                  |   |                                       |
|---|---|---|------------------|---|---------------------------------------|
|  | EXISTING HIGHWAY NETWORK                                      |  | PRIMARY STREET A |  | EMERGENCY ACCESS ONTO BALMORAL AVENUE |
|  | VEHICULAR ACCESS POINT FROM BAILEY ROAD                       |  | PRIMARY STREET B |  | SECONDARY STREET                      |
|  | EXISTING PRIMARY STREET THROUGH PHASE 3 OF BANBURY RISE       |  | PRIVATE DRIVE    |   |                                       |
|  | VEHICULAR ACCESS POINT INTO THE APPLICATION POINT VIA PHASE 3 |   |                  |   |                                       |

STREET TYPOLOGY PLAN

# 07. Movement Coding

- 7.1 To enable the creation of a legible and permeable development, that is not dominated by a repetitive road layout, it is important to establish a street hierarchy that clearly defines the movement parameters for the development.
- 7.2 Streets and open spaces will cross different character areas and will be important in providing continuity across the development. Streets will be designed as key aspects of the public space, the nature and form of which will vary according to their connectivity, location within the development proposals and function.
- 7.3 The key aspects defining each Street Typology are:
- Scale and setting of the street;
  - Movement network designed to be pedestrian and cyclist friendly to maximise sustainable forms of transport, this relates to both the overall street hierarchy and the detailed design of the spaces;
  - Parking strategies depending upon the location, density and building typology;
  - Engineering requirements; and
  - Materials and details that coordinate and have a level of consistency across the phase.
- 7.4 The street typology code does not code every highway detail within the scheme; however it instructs the technical specifications for all street typologies within the phase, in order to give certainty to designers over the acceptability of street components, whilst allowing flexibility in the articulation of development parcels, within different variations of the same themes.

## MOVEMENT AND CIRCULATION

- 7.5 As part of the outline application the scheme includes the creation of a new vehicular access point from Lunnon Road in the northern Banbury Rise development. Further to this, the following are proposed:
- An emergency access onto Balmoral Avenue located to the east of the site;
  - Creation of pedestrian and cycle routes through the open space within the site;
  - A new pedestrian/cycle connection between the scheme and Dover Avenue to the east of the site; and
  - Potential future link to Withycombe Farm (residential traffic only).
- 7.6 This network of streets has been further broken down into three different street typologies. The location of each of these typologies, along with proposed pedestrian routes are indicated on the proposed street typology plan presented opposite. The details of the street typologies are set out on the following pages along with a summary table of each street type.

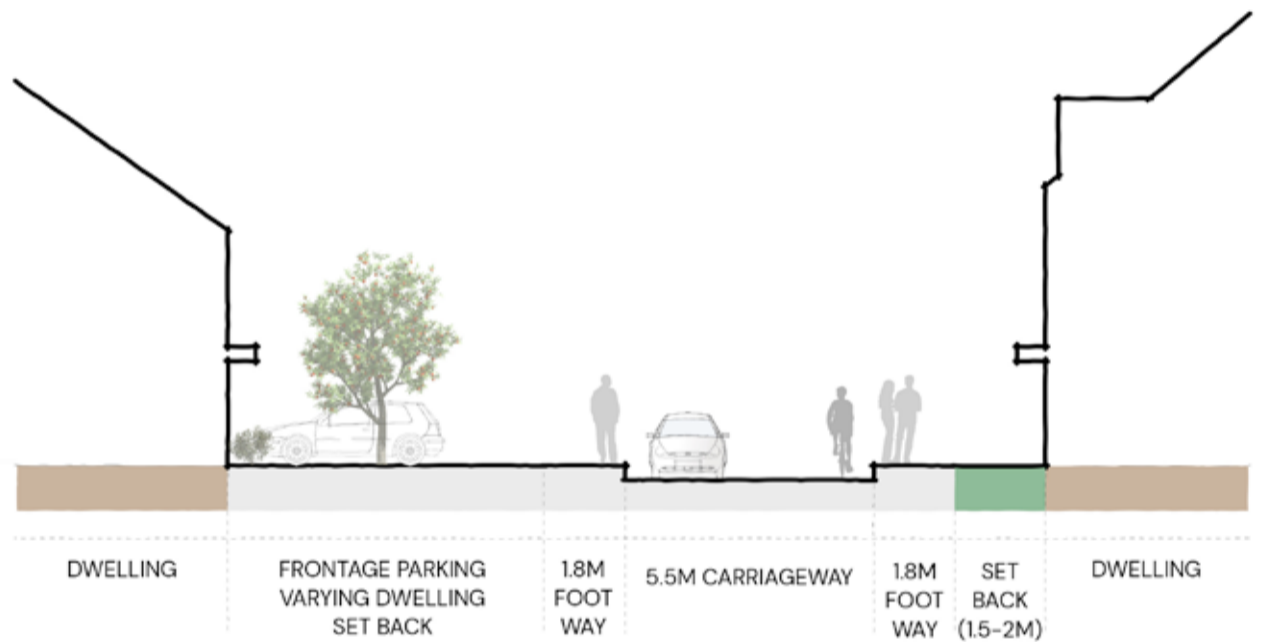
	Primary Street	Secondary Street	Tertiary / Green Edge
Design Speed	20 mph	20 mph	20 mph
Maximum No. Properties Served	400	200 link or loop / 100 cul-de-sac	50
Typical Carriageway Width	5.5m	5m	Shared surface: 4.5m
Footway	Min. 1.8m either side of carriageway	Min. 1.8m on one side of carriageway	Within shared surface
Cycleway	On Road	On Road	On Road
Verge	Incidental verges	Incidental verges	1m grassed service margin
Direct Access To Properties	Yes	Yes	Yes
Forward Visibility	22m stopping sight distance	12–20m stopping sight distance	N/A
Junction Sightlines	Min. 2.4m x 25m	Min. 2.4m x 25m	N/A
Junction Spacing	Defined by junction visibility requirements	Defined by junction visibility requirements	N/A
Junction Radii	Min. 6m (unless otherwise determined by tracking)	4.5–6m (unless otherwise determined by tracking)	N/A
Swept Paths	Refuse vehicles	Refuse vehicles	Refuse vehicles
Street Lighting	Rear of footway	Rear of footway	In service strip
Services	In footway	In footway	In service strip
Traffic Calming Options	TBA	TBA	TBA
Carriageway Surfacing	Asphalt	Asphalt	Asphalt
Footway Surfacing	Asphalt	Asphalt	Asphalt
Kerbing Material	Concrete	Concrete	Concrete
Boundary Treatment	Low hedges	Low hedges / shrub planting	Low hedges / shrub planting

**PRIMARY STREET**

- 7.7 The Primary Street will provide primary movement through the centre of the site from Lunnon Road in the Banbury Rise development to the north of the site. The carriageway will be a sufficient width and design to carry all necessary vehicles, as it leads through the development.
- 7.8 The Primary Street will comprise two routes. Primary Street A runs past the eastern open space. Dwellings are generally set back from the street with parking to the front, providing opportunities for tree planting in between parking spaces creating a more verdant feel through the development.
- 7.9 Primary Street B runs centrally through the development with dwellings set back from adoptable highway providing the opportunity for landscaped front gardens and tree planting to soften the development.
- 7.10 Footways on both sides of the carriageway will allow for pedestrian links for ease of movement when travelling by foot.
- 7.11 Street lighting will be designed in conjunction with the trees to ensure safe and acceptable levels of lighting throughout the development.
- 7.12 Direct access to dwellings and private drives will be provided to ensure ease of access for all residents.



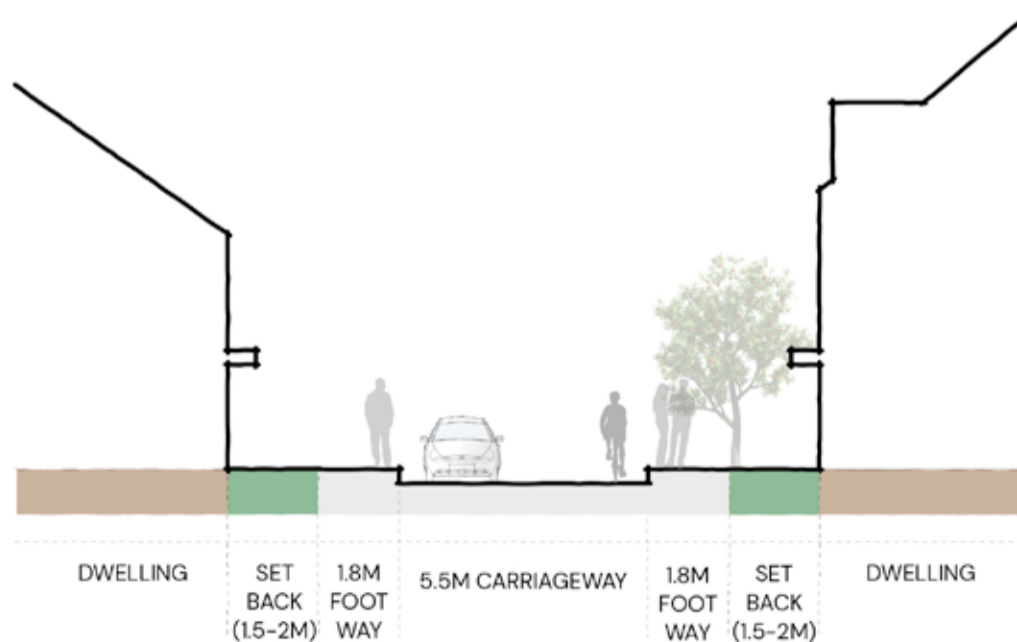
PRIMARY STREET A VIGNETTE



PRIMARY STREET A SECTION



PRIMARY STREET B VIGNETTE



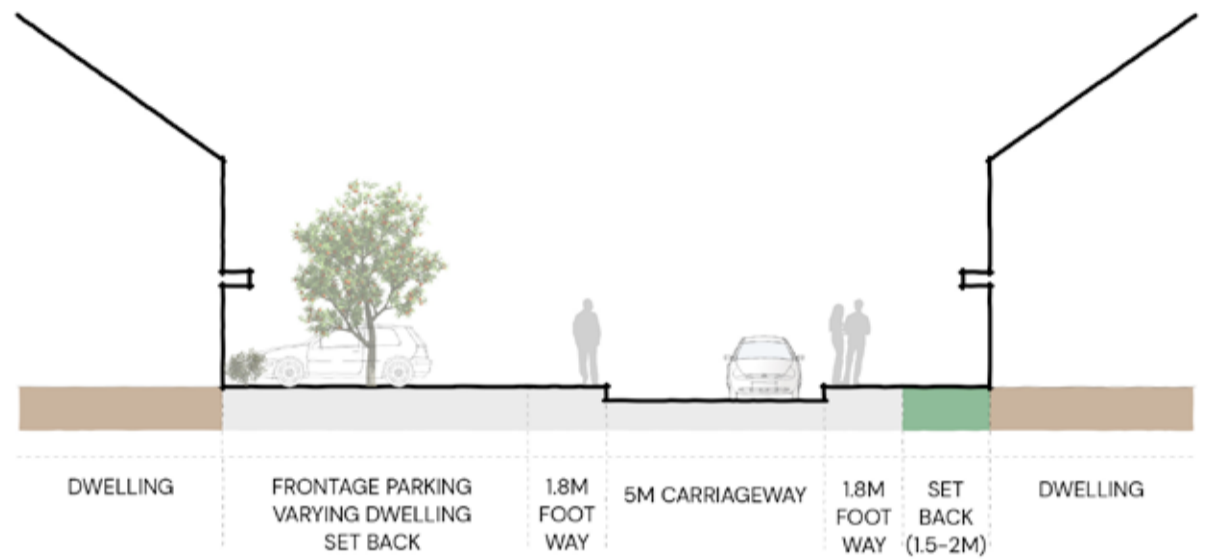
PRIMARY STREET B SECTION

**SECONDARY STREET**

- 7.13 Accessed from the Primary Street will be Secondary Streets that will provide links through the site to individual development parcels.
- 7.14 Footways on both sides of the carriageway will allow for pedestrian links for ease of movement when travelling by foot.
- 7.15 To soften these surroundings, landscaping could be incorporated along the street, particularly between parking spaces where dwellings are parking to the front.



SECONDARY STREET VIGNETTE



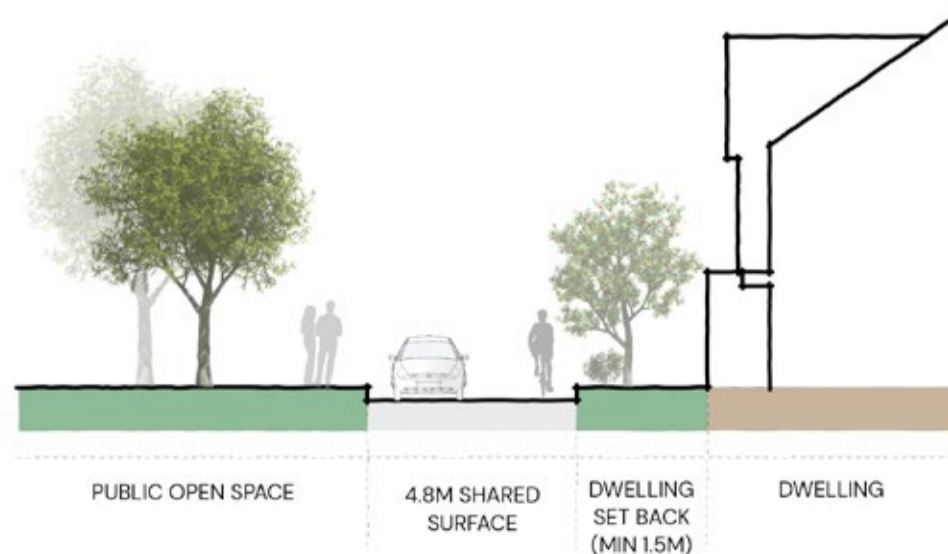
SECONDARY STREET SECTION

**TERTIARY/GREEN EDGE**

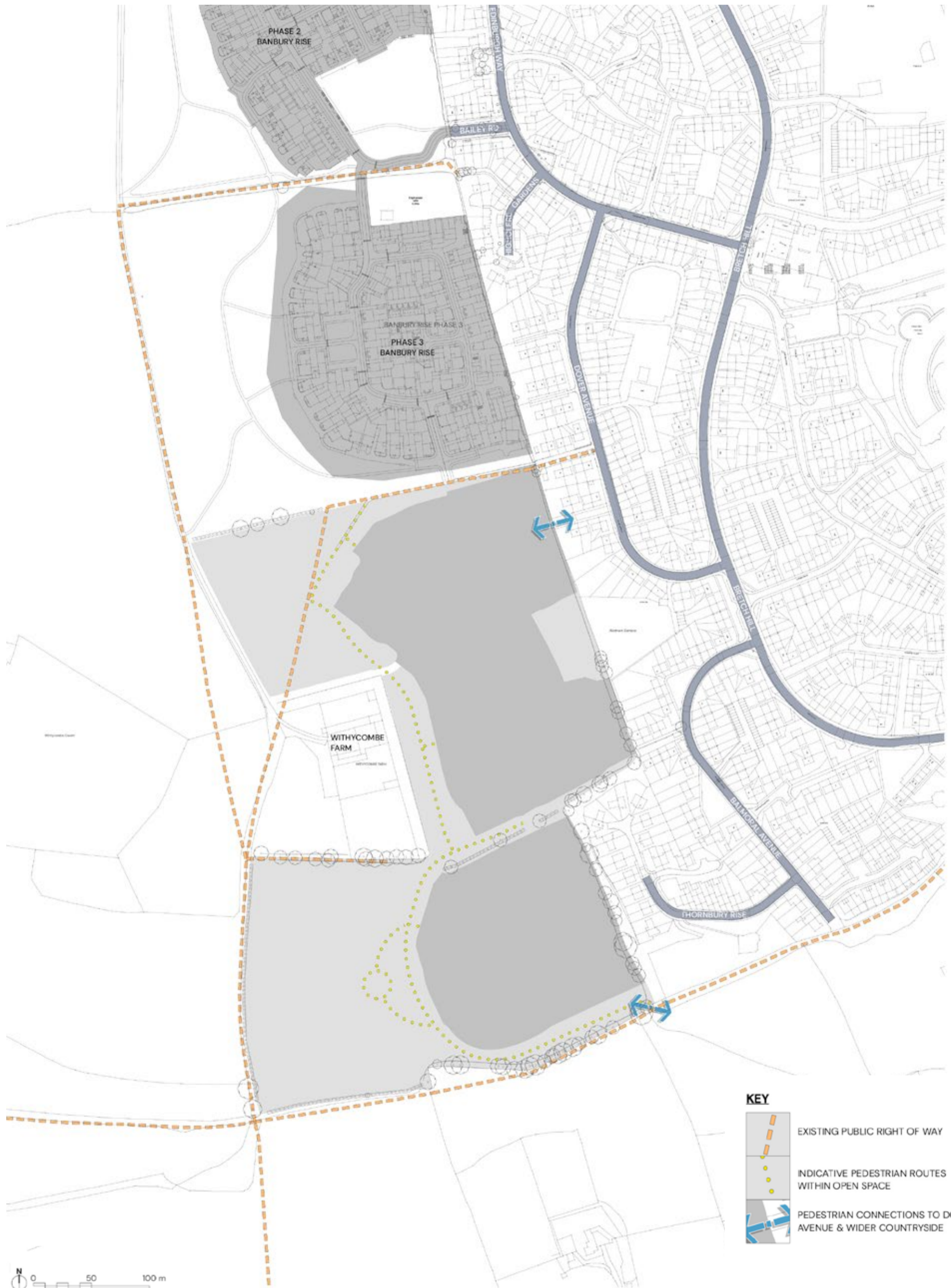
- 7.16 Tertiary/Green Edge streets will be less formal in character and allow for greater instances of social interaction within the street. The materials and boundary treatments on these streets will compliment the natural setting to the Rural Edge Character Area. The streets should be treated as shared surface to provide a more organic and casual environment on the western edge of the development.



TERTIARY STREET VIGNETTE



TERTIARY STREET SECTION



**KEY**

	EXISTING PUBLIC RIGHT OF WAY
	INDICATIVE PEDESTRIAN ROUTES WITHIN OPEN SPACE
	PEDESTRIAN CONNECTIONS TO DOVER AVENUE & WIDER COUNTRYSIDE

PEDESTRIAN MOVEMENT PLAN



## **PEDESTRIAN/CYCLIST NETWORK AND MOVEMENT**

- 7.17 Walking and cycling are the most sustainable forms of movement and are promoted throughout the development.
- 7.18 Some of the key pedestrian links from the development include:
- Links to scheme via the new access from Lunnon Road;
  - Links to the new Local Equipped Area of Play;
  - Links to the existing residential area of Bretch Hill and beyond;
  - Links to the existing public rights of way that run through and adjacent to the site; and
  - Recreational routes in and around the development.
- 7.19 Specific design codes have not been specified for these routes, but the following rules should be applied to all routes:
- Footways to be 2m or more in width;
  - Combined foot/cycleways to be minimum 3m in width;
  - Create direct barrier free routes;
  - Create attractive, well-lit and safe routes;
  - Create routes that can be used for everyone and consider the use of surface treatments to aid in orientation and legibility;
  - Make sure routes are overlooked by properties with good levels of natural surveillance; and
  - Self binding gravel is preferred as oppose to asphalt.

## **TRAFFIC CALMING**

- 7.20 The street typologies beyond The Primary Street are less likely to encourage higher speeds. However, subject to safety audits the secondary and tertiary streets have three traffic calming options:
- Horizontal deflection (either by left or right build out or by central pinch point);
  - Raised block paved tables at junctions (with gradual approach ramps); and
  - Changes in surface materials, particularly around key junctions.
- 7.21 The exact form and location of traffic calming features are subject to future Reserved Matters submissions.



## **VEHICLE & CYCLE PARKING**

7.22 Parking will be designed in line with current guidance:

- Street Design Guide (Design of Cycle and Car Parking).
- Oxfordshire Cycling Design Standards (Quantum of Cycle Parking).

7.23 Vehicle parking (including visitor) will follow the approved standards at Banbury Rise to the north:

- 1 bedroom dwelling - 1 space
- 2+ bedroom dwelling - 2 spaces



### **RESIDENTIAL PARKING STRATEGY**

- 7.24 Allocation parking will predominantly be provided on plot, within the curtilage, either to the front or side of dwellings with individual bays and/or garages set back from the building line to allow ease of access to dwellings.
- 7.25 Allocated residential parking will be provided at a minimum rate of 1 no. car space per 1 bedroom dwelling and 2 no. spaces for units with 2 or more bedrooms.
- 7.26 Vehicle/pedestrian visibility splays of 1.5m x 1.5m (from the back of the highway to the side of the driveway, assuming a 2.4m car width) should be incorporated where parking spaces abut the back edge of the footway or the highway boundary.
- 7.27 Where parking is provided on street (via parallel or perpendicular bays) it should be carefully designed with areas of landscaping and/or planting should be used to break up the appearance.
- 7.28 Where parking courts are utilised, they should be designed to include good levels of natural surveillance and opportunities for active overlooking.
- 7.29 Disabled parking will be provided in accordance with the appropriate standards.

### **VISITOR PARKING STRATEGY**

- 7.30 Unallocated visitor parking will be provided at the rate set out in the table above, as shown in Oxfordshire Parking Standards for New Residential Developments.
- 7.31 The approach to accommodating visitor parking will be informed by the following guiding principles:
- Visitor parking will typically be provided on plot for each dwelling adjacent to the allocated spaces; and
  - Additional on street spaces will be provided within key spaces, verges, widening of the street and on the edge of the western open space.

### **CYCLE PARKING**

- 7.32 As per Oxfordshire's Cycling Design Standards the minimum approved standards are:
- Resident cycle parking – 1 space for 1 bed unit, 2 spaces for larger units.
  - Visitor cycle parking – in addition to the above, 1 stand per 2 units where more than 4 units.
- 7.33 Where cycle parking is to be accommodated within garages then these should be of an appropriate size to ensure that there is room for both car and cycle parking. If a dwelling has no garage, secure enclosed cycle parking must be provided. This can be in a rear garden if there is direct access or at the front of the property if there is no direct garden access.
- 7.34 For apartments, secure cycle parking should be provided in a communal facility in close proximity to the building entrance and where it is well overlooked.

## 08. Lighting Strategy

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- 8.1 In most cases, the provision of lighting significantly increases people's sense of security and enhances the chance of criminal detection. Good lighting design plays an important part in creating a more comfortable and attractive environment that reduces the fear of crime and enhances community safety. The selection of lighting must be considered in a comprehensive manner to ensure that a common language of elements is maintained across the public realm and contributes towards a locally distinct environment. The use and placement of lighting should reflect the following principles:
- Ensure it is robust, high quality and durable, with hidden/recessed anti-vandalism fixings;
  - Heights of lighting columns in pedestrian areas should have a human scale;
  - Provide lighting along essential footpaths and cycleways;
  - Avoid unnecessary and excessive lighting levels;
  - Street trees must be sensitively located and ensure there is no conflict with highway lighting; and
  - Where possible, signage should be fixed to poles/posts such as lighting columns. Litter bins can be similar attached, provided that lighting columns have been designed to accommodate additional fittings. Overall, signage should be kept to a minimum and used only as required.

# 09 Recycling and Refuse Collection Strategy

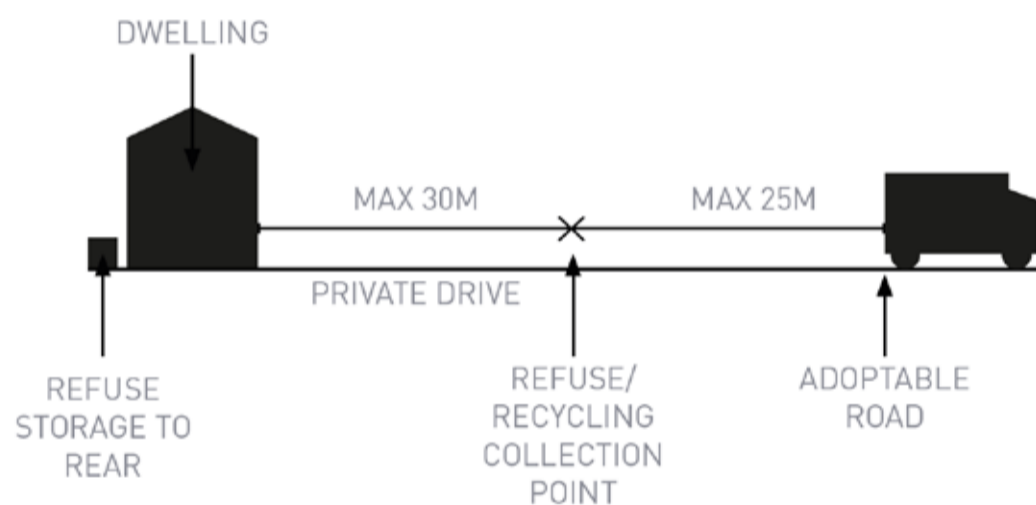
9.1 A refuse strategy will need to be prepared at the Reserved Matters stage to assess how the bins and recycling will be collected. A number of Bin Collection Points (BCP) will be required and will be positioned at a maximum distance of 30m from the furthest dwelling curtilage and positioned a maximum of 25m from the nearest adoptable road. This will ensure any future bin collection operate within the maximum bin carry distances.

## **DWELLING REFUSE**

9.2 Cherwell District Council provide one green non-recyclable waste bin, one blue recycling bin, one brown garden waste bin and a kitchen caddy. The bins are currently always emptied once a fortnight, however the kitchen caddy is emptied weekly along with one other bin.

## **APARTMENT REFUSE**

9.3 Residents of apartments will require access to communal bin stores. The amount of bins required will depend on the number of apartments within an apartment building. The bins are collected from the bin store are not generally moved to kerbside. Communal bin stores will require screened refuse stores, constructed in either brick or timber.



REFUSE & RECYCLING COLLECTION DIAGRAM





# 10. Landscape Principles

## TREE STRATEGY PLAN

10.1 Incorporating nature, particularly tree planting, within the streets is a key principle in the design of new developments. Trees along streets have been given priority in the latest edition of the NPPF. This section explains the principles, potential locations and species of trees along the Primary Street. Away from the Primary Street, trees will be provided responding to the NPPF requirement.

*“Trees make an important contribution to the character and quality of urban environments, and can also help mitigate and adapt to climate change. Planning policies and decisions should ensure that new streets are tree-lined (unless, in specific cases, there are clear, justifiable and compelling reasons why this would be inappropriate), that opportunities are taken to incorporate trees elsewhere in developments (such as parks and community orchards), that appropriate measures are in place to secure the long-term maintenance of newly-planted trees, and that existing trees are retained wherever possible. Applicants and local planning authorities should work with highways officers and tree officers to ensure that the right trees are planted in the right places, and solutions are found that are compatible with highways standards and the needs of different users.”*

*(Para 131, NPPF 2021)*

10.2 The following overarching principles are to be considered for street tree planting:

- Tree species are to be chosen to represent the scale of the streetscape and location within the street hierarchy. As the street hierarchy changes from its different arrangement the street tree planting shall also change in amount and scale to suit the streetscape. Primary routes will have more regular tree planting and the regularity, spacing, form and size will change in the secondary, tertiary and private drives.
- To plan for sustainability, risk of tree disease and longevity, tree species are to be mixed to ensure mass tree failure/loss is reduced. However, tree species shall be chosen to replicate form and shape to ensure the design intent is achieved.
- Trees to be on alternative sides of the street.
- Feature trees to be used to denote key locations and to help define key land uses and destinations. A change in tree species and form may also help define key points along the street to break long vistas, provide a navigation tool to aid way finding, or to help slow the pace of the street. These feature trees will typically be larger specimens in a different form to those used regularly along the streets.
- Clear stem trees should be specified to ensure visibility is afforded. All trees to have a minimum 2m minimum clear stem to ensure sightlines are respected. The canopy form and shape of street trees shall be considered to ensure safety and reduce the opportunity for overhanging branches affecting vehicles.
- Tree positions must be coordinated to avoid conflict with parking courts, driveways and junctions as well as lighting columns and utilities, with adequate unobstructed soil volume.



## FEATURE TREES

### ACER CAMPESTRE

#### Field Maple

- 10.3 This Acer has bushy, rounded head with small leaves that are of typical maple shape, though more rounded than some in the Acer family. The tree has a striking butter-yellow Autumn colour and when young the bark has a curious cork-like appearance, though as the tree matures this disappears. Grows to approximately 15m high.



### BETULA PENDULA

#### Silver Birch

- 10.4 A striking medium-sized deciduous tree with silver bark, spring catkins and pretty yellow Autumn foliage. When mature they can reach 30m in height forming a light canopy with elegant, drooping branches.



### PRUNUS AVIUM

#### Wild Cherry

- 10.5 A deciduous tree with pure white flowers in Spring, followed by small red/purple cherries in Summer. The chestnut coloured bark becomes silvery with age and leaves turn a rich orange/red in the Autumn. Grows to approximately 20m in height.



### LIRIODENDRON TULIPIFERA

#### Tulip Tree

- 10.6 An ornamental tree with distinctive foliage with four lobes and has yellow and green tulip shaped flowers. The unusual shaped foliage is bright green, turning yellow in Autumn. Grows to approximately 10m high with a 6m spread in 20 years.



## ORNAMENTAL HEDGEROWS

### PHOTINIA X FRASERI 'RED ROBIN'

#### Christmas Berry 'Red Robin'

- 10.7 Evergreen shrub or small tree grows to approximately 6m high with rich red young foliage which ages to leathery glossy green. In warm situations, clusters of small white flowers appear towards the end of May giving off a musky fragrance. Red fruits in Autumn, although rarely have time to ripen in the UK.



### ILEX AQUIFOLIUM 'J.C VAN TOL'

#### English Holly

- 10.8 This hardy native Holly has glossy dark green leaves that are almost spineless. The bright red berries appear on dark purple stems in Autumn and persist through the Winter months, providing a rich source of food for birds.



### PRUNUS LUSITANICA

#### Portuguese Laurel

- 10.9 This Laurel is a large evergreen shrub or small tree. It showcases dark-green, pointed, glossy leaves on striking deep-pink/maroon stems when mature, whilst younger plants can display lush green stems.



### ELAEAGNUS X EBBINGEI 'GILT EDGE'

#### Oleaster

- 10.10 An evergreen shrub, bearing leaves with contrasting dark green centres and gold-coloured margins. Inconspicuous silvery flowers, with an intense fragrance, appear in October, followed by red fruit.



### GRISELINIA LITTORALIS

#### New Zealand Broadleaf

- 10.11 An evergreen shrub native to New Zealand with light green oval leaves. It's flowers are small and unnoticeable, these are followed by purple fruits on female plants if they are fertilised. This shrub is a robust plant and able to survive strong winds.





## **STREET TREES**

### **CARPINUS BETULUS 'FRANS FONTAINE'**

#### **Upright Hornbeam**

- 10.12 A variety of hornbeam which maintains its tight pyramidal shape well into maturity. The foliage is bright green in Spring, turning gold and orange in the Autumn. Grows to approximately 22m.



### **AMELANCHIER X GRANDIFLORA 'ROBIN HILL'**

#### **Pink Juneberry Tree**

- 10.15 This tree has pink buds with pale pink flowers that open to white. The oval foliage is bronze as it emerges in Spring, turning darker green in the Summer and magnificent shades of Red and Orange in the Autumn. Grows to approximately 4m high with a spread of 3m in 20 years.



### **PRUNUS 'SNOW GOOSE'**

#### **Cherry Tree**

- 10.13 A narrow upright tree with large, white, single flowers displaying attractive tasselled stamens that appear before the leaves. The bright green leaves gain beautiful Autumn tints of bronze and red adding Autumn interest. Grows to approximately 4m high with a 3m spread in 20 years.



### **MALUS BACCATA 'STREET PARADE'**

#### **Crab Apple**

- 10.16 A Crab Apple tree which produces a mass of white flowers in the Spring. Produces small purple/red fruits by August and its green foliage turns to yellow in the Autumn. Grows to approximately 7m high.



### **ACER CAMPESTRE 'STREETWISE'**

#### **Field Maple**

- 10.14 This Acer has a neat, upright habit with brilliant yellow Autumn Colour. Grows to approximately 7m high with a 3m spread after 25 years.



### **SORBUS AUCUPARIA 'STREETWISE'**

#### **Mountain Ash**

- 10.17 A small-medium sized deciduous tree which provides interest across three seasons. It has a neat upright habit with a narrow canopy. Attractive white flowers appear in May and early June then fruits bright orange/red berries from July to November. Grows to approximately 7m high with a 3m spread after 25 years.





10.18 The landscape and public realm strategy for Land South of Banbury Rise is to create a diverse, engaging and ecologically and historically sympathetic landscape.

10.19 Through consideration to the surrounding landscape which includes previous phase of development, this scheme aims to be successfully integrated into the landscape providing for connected landscaped areas enhancing the emerging green network. Hedgerows on the western boundary of the site will be retained and enhanced where possible. This will be the location for the majority of the scheme's open space.

10.20 The landscape has been designed in such a way to:

- Mitigate the visual impact of the development through a structural native landscape including a tree and hedge buffer to the western boundary whilst celebrating some of the features in the landscape by offering views out.
- Provide recreational routes to the edge of the built development including opportunities for play, public art and orchard planting.
- Creation of wildflower meadows, scrub planting and improvements to the sites biodiversity to encourage habitat creation and provide habitat links to the wider surrounding area.
- Protect the sites potential heritage assets.
- Propose trees and shrubs for their form, colour and flower, providing structure and identifiable character to the landscaped areas, POS and plot frontages.
- Provide distinctive tree planting in different locations to aid wayfinding through and around the site.



### **POS OVERVIEW**

10.21 Throughout the scheme, existing trees and boundary hedgerows and grassland is to be retained where possible and managed in a way that ensures the existing landscape features are retained and managed as necessary.

10.22 Proposed new hedgerows to provide separation between housing and open space, small blocks of mixed planting, wildflower and understorey or wetland grassland to attenuation areas will be implemented, enabling a range of habitats to flourish within an area of amenity open space. Amenity grassland will be maintained in the more recreational areas around play areas and footpaths.

10.23 Trees will be utilised for their variation in structure, leaf form and foliage colour within open spaces and as focal features and softening of the built form, including an area of orchard fruit trees of local provenance. The species and number of proposed trees will be set out within the detailed landscaping proposals.

### **PLOT LANDSCAPING OVERVIEW**

10.24 Throughout the residential areas, tree species have been selected for their form, varying leaf colour and flower to enrich the character of the spaces, whilst offering shade and some wildlife links within the built areas.

10.25 Trees along roads will enhance the streetscape whilst aiding the softening of the built form. Clipped hedges and ornamental evergreen shrub planting will separate the public/private interface. Some single species native hedgerow planting can provide the transition between residential development to open spaces or the existing and wider landscape.

10.26 Garden frontages away from the spine road are less formal and consist of ornamental shrub planting and additional smaller trees of mixed species.

10.27 Car park bays will be partially screened by shrub planting or evergreen hedgerows to reduce the visual impact of the parking areas. Within the Lanes and private drive areas planting is less formal. Areas of lawn, ornamental shrub planting and flowering ornamental tree species will be proposed. This planting gives seasonal interest and provides a range of flowering shrubs enhancing biodiversity, thus giving interest and providing an attractive street scene.

10.28 As part of the submission, a range of biodiversity measures will be adopted to increase the ecological value of the development, the following measures will include:

- Use of native tree and shrub flora to attract wildlife through providing optimal and sub-optimal habitats.
- A variety of species are being used to ensure biosecurity for the scheme and future proofing the landscape design.
- Improvements to existing hedgerows and mixed planting block consisting of shrubs, some native trees and understorey of longer grass will support the existing structure.
- Some marginal planting to proposed wetland attenuation ponds will provide new opportunities for habitat creation.
- Whilst not strictly applicable to biodiversity enhancement, habitat provision for birds and bats, log piles within open spaces to attract invertebrates and ornamental species to plot frontages will be selected for their wildlife value qualities.

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