

16.145.03

13 / 02 / 2023

Design and Access Statement

For

Planning Application for reserved matters under the Outline Planning Approval 19/00128/HYBRID

At

Frontier Park

Land adjacent to the M40

Junction 11

Banbury

OX16 3AD

1.0 Introduction

This statement accompanies a planning application for reserved matters under the Outline Planning Approval 19/00128/HYBRID for the site adjacent to the M40 at J11, Banbury.

The application seeks detailed approval for Unit C [FP180].

The application forms part of the wider Frontier Park development which has consent for B1, B2 and B8 uses which are currently constructed to the northern and central areas of the site. The site has allocated employment uses of B1(a) Offices (now Use Class E), B2 General Industrial, and B8 Storage and Distribution uses, with the intention of delivering economic and employment growth and investment supplemented by local infrastructure.

This statement should be read in conjunction with the following drawings by Campbell Driver Partnership [Architects]:

Application Drawings

- 16.145.003b – OS plan
- 16.145.03.902e – warehouse site plan
- 16.145.03.903c – warehouse plans
- 16.145.03.904c – warehouse elevations

2.0 Planning Policy

The National Planning Policy Framework (NPPF) sets out the governments planning policy for England and how it should be applied. The NPPF requires great importance to be attached to the design of the built environment and good design is a key aspect of sustainable development; the NPPF also advises that design is indivisible from good planning and should contribute positively to making places better for people. The NPPF also states that planning should not tend to oppose architectural styles and particular tastes and should not stifle innovation. In preparing the scheme for the proposed development full regard to the NPPF has been had in respect of design.

The application conforms to the NPPF as the proposal seeks to promote economic development and facilitate sustainable economic growth by building an employment focused development, in a highly accessible location that will promote employment opportunities.

Regard has also been had to design policies contained within the Cherwell Local Plan Part 1 2015 and in particular Policy Banbury 15 Employment land northeast of junction 11 and the specific design and place making principles of Policy ESD15.

3.0 Proposed Use

The planning application site is located at Junction 11 of the M40, northeast of Banbury town centre. The site is situated immediately adjacent to the M40 and bounded by the A361 to the east. The site is identified as a Strategic Employment Site in the Cherwell Local Plan.

The proposal seeks to further develop the southern areas of this strategic site creating employment opportunities through a mixture of use classes.

Given the transport links and proximity to the town of Banbury this will deliver a sustainable development as envisaged in Cherwell's Local Plan.



Figure 1. Site wide masterplan

4.0 Amount

The application site represents about 2.9 hectares of the site-wide Frontier Park site.

The warehouse floor area is 15,840 m² [170,000 sq.ft] together with associated offices at first and second floor level of 930 m² [10,000 sq.ft].

The site contains 78 car parking spaces including 6 designated accessible spaces and 2 designated EV charging points/spaces [with an allocation and infrastructure for further future EV charging points/spaces].

5.0 Layout

The layout of the site is influenced through the requirements and practicalities of the building use while noting the visual implications and necessities of the unit.

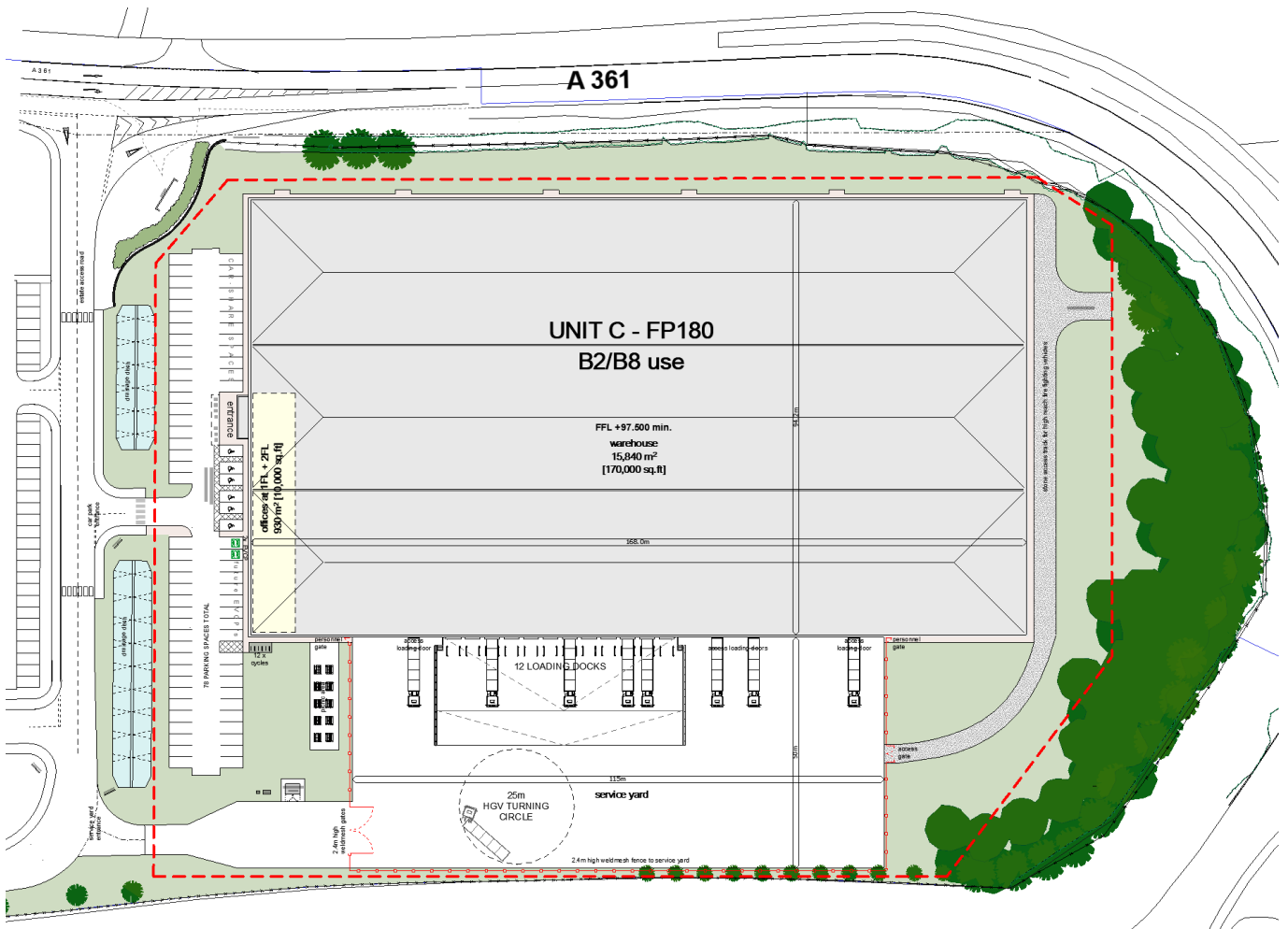


Figure 2. Proposed Site Plan

7.0 Appearance and Scale

The appearance of the proposed building has been designed to match / blend with the development as a whole, thereby providing a holistic and consistent appearance across Frontier park allowing it to be read as a single development.

The building marries with the other existing industrial buildings to north. The graduating grey tones of cladding are used to break up the tone and mass of the principal elevations and will appear to make the uppermost part of the elevations seem lighter. The main entrance is highlighted by contrasting full height curtain walling forming a projecting entrance accentuating the ground floor entrance area.

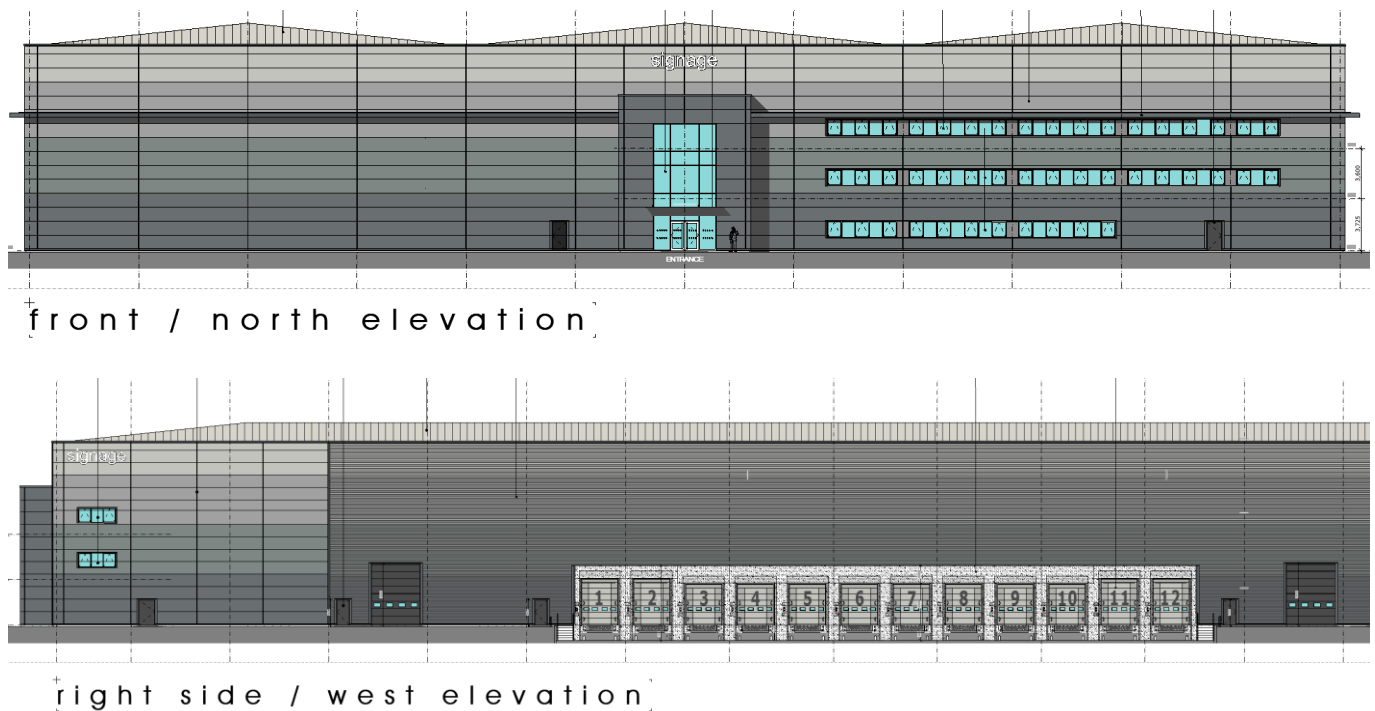


Figure 3. Typical elevations

8.0 Landscaping

Reference should be made to the Landscape scheme prepared by the Landscape Architects. Their proposals seek to create a naturalised landscape setting defining edges and boundaries and routes through the site by the use of quality planting and external materials.

In addition they have also undertaken a landscape visual impact assessment to identify the impact of the proposed development in the wider landscape setting.

As part of the client's commitment to sustainability, ecology assessments have also been undertaken which will inform the BREEAM Assessment and have been reinforced by a landscape proposal that supports and enhances bio-diversity.

9.0 Sustainable Design principles

Monte Blackburn Ltd is committed to ensuring the development employs best practice sustainable design principles and to this end the project will be registered as a BREEAM project with the aim of achieving 'Very Good.'

BREEAM project ratings review all measures, from ecology enhancement, sustainable drainage provision, life cycle costing to carbon efficient energy performance.

Best practice sustainable design principles will be achieved through the prioritisation of passive design principles including:

- Maximising levels of insulation to the external fabric
- Optimising levels of air tightness with the new structure
- Good practice junction detailing to reduce thermal bridging

In addition, particular consideration will be given to the use of efficient lighting systems and heating and cooling provision to ensure the most carbon and cost-efficient system possible with energy efficient solutions/renewable energy sources fully assessed at detailed design stage.

Energy efficiency and renewable energy sources will be fully explored with the aim of achieving a minimum of 10% enhanced performance on current Building Regulations.

10 Access

Access to the application site is from the A361 to the east of the site via an access road already formed to serve the ongoing development of Phases 1 & 2 of Frontier Park.

Inclusive access is achieved in all the buildings by the provision of level thresholds, accessible lifts where required and accessible amenities.

Accessible car parking is positioned adjacent to the main entrance to the building.

The site has an incorporated pedestrian link from the near bus stops for ease of commute and to promote sustainability within the site. These footpaths are continued through the site to allow safe passage to each building and / or area with appropriate crossings at vehicular interaction points.