

**nca-architecture**

7 Court Farm Barns, Medcroft Road, Tackley OX5 3AL  
Tel : 01869 226610

Design & Access Statement in support of  
Proposed Development at  
EP Barrus Industrial Unit,  
Launton Road,  
Bicester,  
OX26 4UR

February 2023

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## Introduction

This design and access statement has been prepared by NCA in support of the full planning application for the construction of a new detached storage building, extension of the existing industrial unit, in conjunction with a recladding of the existing building and associated landscaping. An array of PV panels to the existing roof are also proposed.



Fig 01 – Aerial Photo of the site.

## Site Description

The application site is located on the Bicester industrial estate, located at the junction between Launton Road and Granville Way, though it is principally accessed from Glen Way. The existing building is set behind an open car parking area and is finished in red brick, steel framed windows with a shallow corrugated metal roof. A brown metal cladding band is located at the wall and roof junction.

The existing building has been extended previously with the addition of a Rubb type warehouse building at the northern extents, which addresses the delivery yard. At the Southern building extents lies an external service yard.

The existing building is two storey in height, and by virtue of its footprint has distinctive horizontal emphasis. The rhythm of fenestration on the principal façade addressing Launton Road makes the main building entrance difficult to distinguish.



Fig 02 – Photo of existing building.



Fig 03 – Existing Delivery Yard.



Fig 04 – Existing Rubb Type Building.



Fig 05 – Existing Service Yard.

## The Proposal

The proposal is split into two principal aspects – the extensions / new additions to afford additional warehouse storage, new primary entrance & reception, and larger external service yard, alongside a comprehensive re-design of the building envelope. These are described in greater detail below:

### Building additions / extensions

- It is proposed to construct a new Rubb type warehouse building adjacent to , and accessed from the existing delivery yard. This will be located on an area of grass located towards the Northern site extents, and will also require the removal of 4 no. trees. The loss of these trees will be mitigated through the planting of replacement trees along the main site frontage, addressing Launton Road. This new warehouse building will match the existing Rubb type extension which was added to the main building in 2013. It is important to add that under this planning consent (12/01650/F) a second warehouse building was consented, though never built, adjacent to where the current proposed building is to be sited.
- The existing building entrance, though recessed into the primary elevation, is unclear and it is not immediately obvious. In addition, the existing reception area is quite dark and not particularly welcoming or pleasant to work in. The provision of a new extension and entrance to the building frontage is considered to address these issues, and has been designed to complement the cladding exercise (described in more detail below). The new reception area includes a waiting area for visitors, before they are admitted access to the main building, and will be set back from areas of hardstanding by soft landscaping.
- The existing service yard is to be extended through the partial demolition of the existing boundary wall which encloses it. A new enclosure is to be constructed within the car park area to increase available space, whilst maintaining separation and security of the service area. The footpath adjacent to the existing side and site boundary is to be extended to link it to the pedestrian footpaths which bisect the car park.



Fig 06 – Proposed Location of new Warehouse.



Fig 07 – Existing building entrance.

## Building Recladding

- The aspirations behind the building cladding aspect of the proposal is to improve the appearance of the building, and to create an operational HQ that is commensurate with the Barras brand. In addition, a lifting or improvement to the building is considered to assist in skill retention and recruitment, providing staff and workers with a workplace that they can be proud of.
- The design introduces a timber effect cladding plinth below the ground floor window cill, which steps up to meet the existing metal clad eaves banding to bookend the building at its corners. The upper floor windows are to be enlarged by reducing the depth of the spandrel panel which will improve the natural daylighting of office spaces at FF level, and introduce a greater degree of verticality to the fenestration. An anthracite grey cladding panel infills between the timber effect boarding to introduce varied texture and contrast.



Fig 08 – Example of Timber effect cladding.

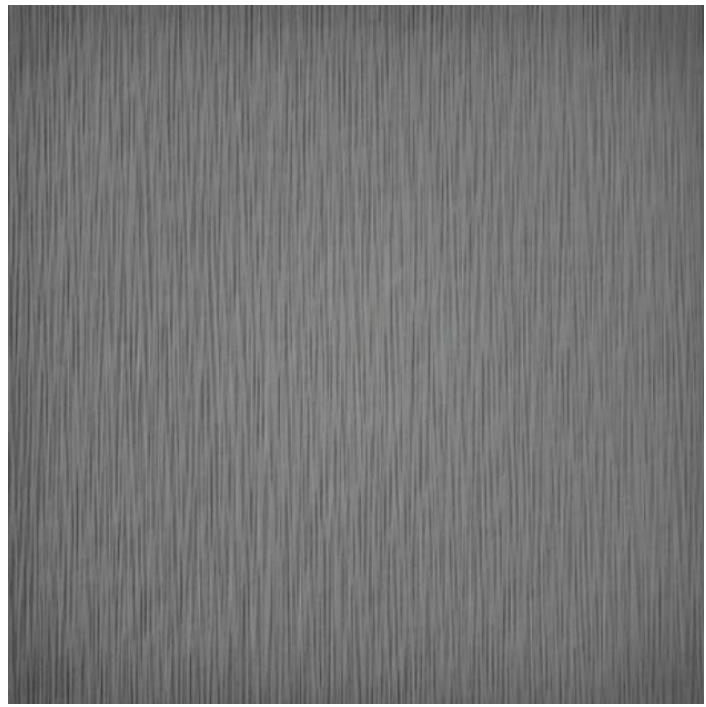


Fig 09 – Proposed Cladding panel.

## Access and Parking

Vehicular and pedestrian access to the application site remain largely unchanged, however an emergency access point from Granville Way is relocated to accommodate the enlarged service yard. The number of parking spaces is reduced from 127 to 114, however this is not considered to result in a shortfall of available parking, by virtue of a change in work patterns following Covid-19. 8 spaces are additionally provided with EV charging points, and there is no reduction in disabled parking bays.