

## Non-residential uses: Education Campus (including Sports Pavillion)

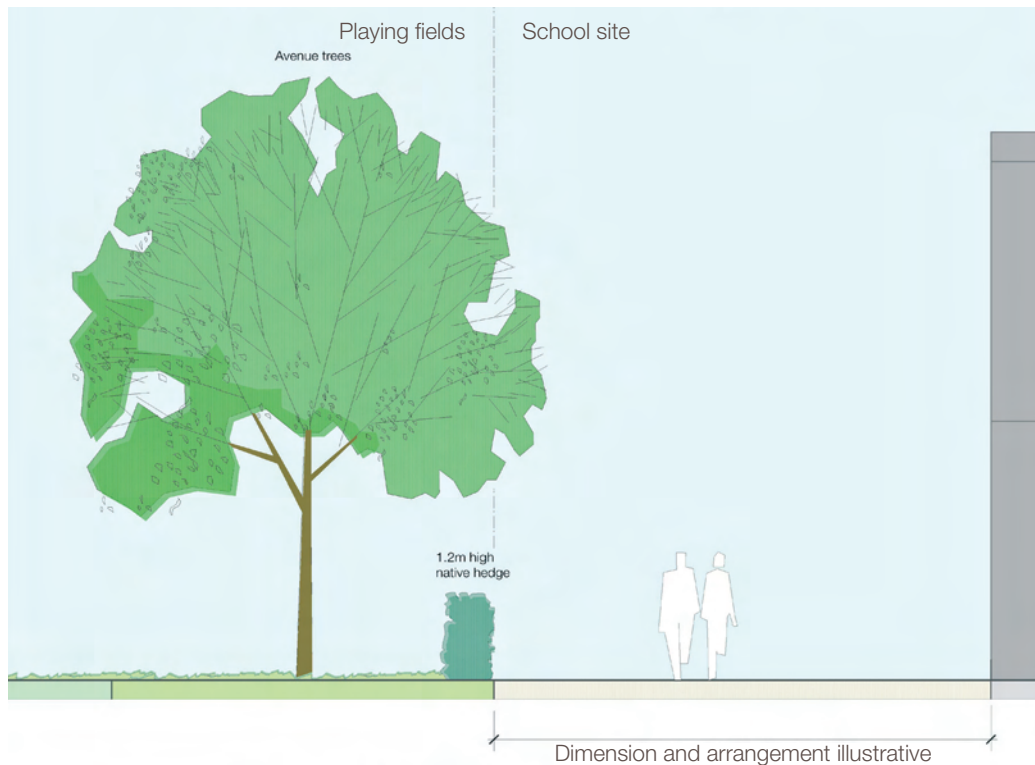


Figure 4.20 Section Z-Z, Preferred Option: The south-western boundary of the school overlooks playing fields which pupils will use. Preferably the school buildings will define this frontage with a subtle boundary treatment, as shown above.

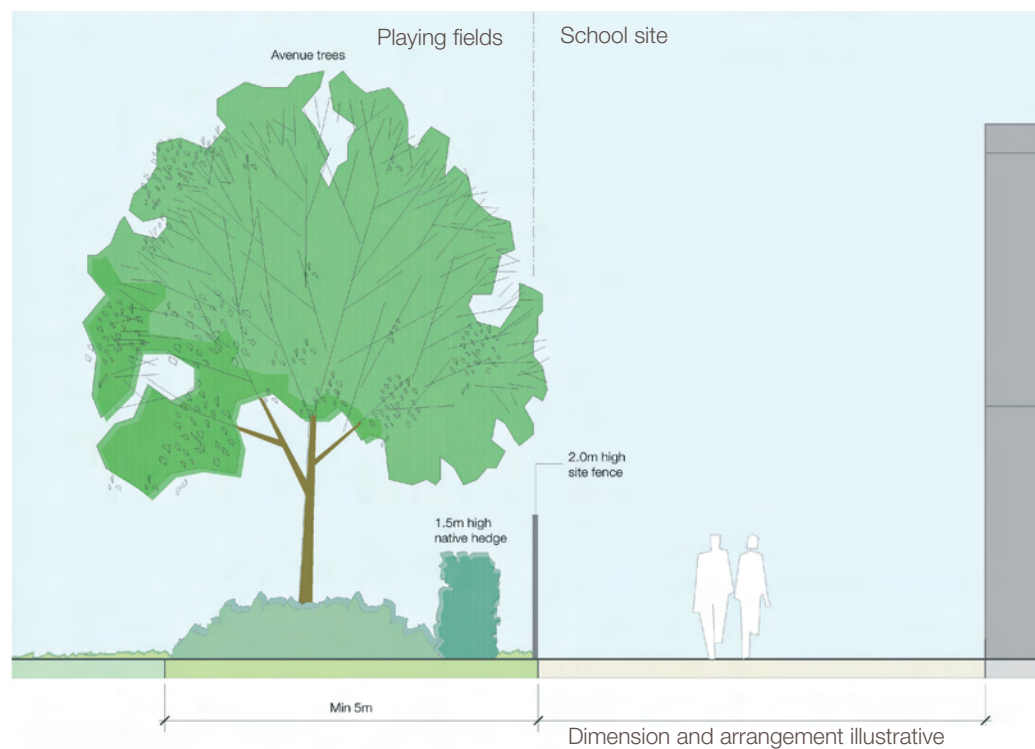


Figure 4.21 Section Z-Z, Security fence option: If a security fence for the south-western boundary is required it should be softened with planting, as shown above.

## Cycling

Cycling to and from the school will be encouraged. **Covered and secure cycle parking will be provided within the Education Campus site.**

## South-West frontage

The Education Campus has a main frontage facing south-west. This frontage is most prominent in views, with a major landmark opportunity at the western corner – combining with the main entrance to the school – and another overlooking the A41.

**This frontage must:**

- **be directly and positively addressed by buildings**
- **read as the principal elevation of a key civic building within Kingsmere**
- **have an independence of form, design and materials (from surrounding buildings), in order to signal the nature of the use**
- **be designed to maximise active frontage in and around the main entrance**

**The main entrance of the building (at least) must be set close to the site entrance** to ensure proper surveillance of that area, as well as a welcoming and legible approach for visitors.

## Non-residential uses: Education Campus (including Sports Pavillion)

### Greenway frontage

The Education Campus also has a frontage to the public footpath greenway that adjoins the north-western boundary of the site. The greenway is a broad landscape corridor overlooked on its northern side by housing within the Whitelands character area. The greenway will be a busy pedestrian route on school days and a quiet leisure route at other times.

It is important that the school buildings positively address this frontage architecturally. It should also provide natural surveillance via ground and upper floor windows. Pedestrian gates at either end of this frontage will facilitate activity here on school days.

### A41 frontage

The south-eastern frontage of the Education Campus overlooks the A41 as it enters Bicester. This frontage will be largely screened by planting and the school buildings will be of primary importance only in longer views. The massing of the building, and particularly its roof form and skyline profile, must be considered carefully.

### Boundary treatments

**Where necessary the site frontages will be secured by a continuous 2m black or green coloured weldmesh fencing system with lockable personnel and vehicular gates.**

The south-west boundary may be secured by buildings fronting the open space. Where weldmesh fencing is proposed along the south-west boundary, it must be used in combination with landscape planting to ensure an attractive development edge. Figures 4.20 and 4.21 illustrate appropriate landscape treatments along this edge.

The south-eastern and north-western boundary treatments are illustrated in Appendix 1, Figures App1.12 and 1.13

### Planting and public realm

**All planting must accord with the Whitelands character area.** Refer to pages 118-121. Hard landscape materials must be chosen to complement the materials used in adjacent character areas.

### Sports Pavilion

MANDATORY	<b>m</b>
Site size	<b>0.35ha</b>
Vehicular Access	<b>From the Primary Street.</b>
Vehicle parking	<b>Car and coach parking on site.</b>
Cycling	<b>Covered and secure cycle parking provided on site.</b>
Architecture	<b>Maximum building height of 7m.</b>

#### Site

**The location of the Sports Pavilion site is shown on the Land Use Proposals plan and is mandatory.** The sports pavilion and its surroundings are likely to be owned and managed by Cherwell District Council.

**The Sports Pavillion site will be 0.35ha.**

#### Vehicular access

**Vehicular ingress and egress to the Pavilion car park will only be available from the primary street immediately to the east.**

#### Pedestrian access

Pedestrian access to the site will be predominantly via the Primary Street. Other pedestrian access arrangements from within the public open space will be provided.

#### Vehicle parking

**Car and coach parking must be provided adjacent to the pavilion.** Parking will be controlled by the Council so as to minimise use by those using neighbouring facilities.

#### Cycling

Cycling to and from the pavilion will be encouraged. **Covered and secure cycle parking will be provided adjacent to the pavilion.**

#### Recycling

**The Sports Pavillion car park offers a potential location for the main public recycling centre for Kingsmere.**

## Non-residential uses: Education Campus (including Sports Pavillion)

### Architecture

The Education Campus must be of the highest quality design to reflect its importance as a principal civic building within Kingsmere. Quality will be assessed in terms of functionality, innovation and robustness (in both the short and long term), as well as in the standard of materials and detailing used.

On the main frontage the building must sit comfortably within the landscape in distant views, whilst making a bold and confident statement in views close to the main entrance. **The overall height and architectural scale of the building must relate to both long and short views, but must not exceed 11.5m.**

This will be reflected in the overall massing and form of the building as well as in the sub-division and detailing of elevations. A taller landmark element must be a key part of the building design at, or near, the main school entrance.

**The part of the building viewed by those approaching Bicester by vehicle from the south must also act as a landmark.**

As a landmark civic building it is expected that the Education Campus will perform to the highest sustainability standards. This is likely to be reflected in the form, layout and architecture of the building, including for instance rooftop cooling vents.

### Materials

**The main wall and roof materials for the Education Campus must harmonise with the materials and colours used in the Whitelands character area.** These include render, red or buff brick and natural and reconstituted stone. In view of its civic status, contrasting or accent colours may also be used, as may high quality modern cladding materials

### Architecture / materials

The Sports Pavilion is an important landmark building on a prominent part of the site. It is a civic building which will be used by a wide cross-section of the community. As such the building must be of the highest quality design. Quality will be assessed in terms of functionality, innovation and robustness (in both the short and long term), as well as in the standard of materials and detailing used.

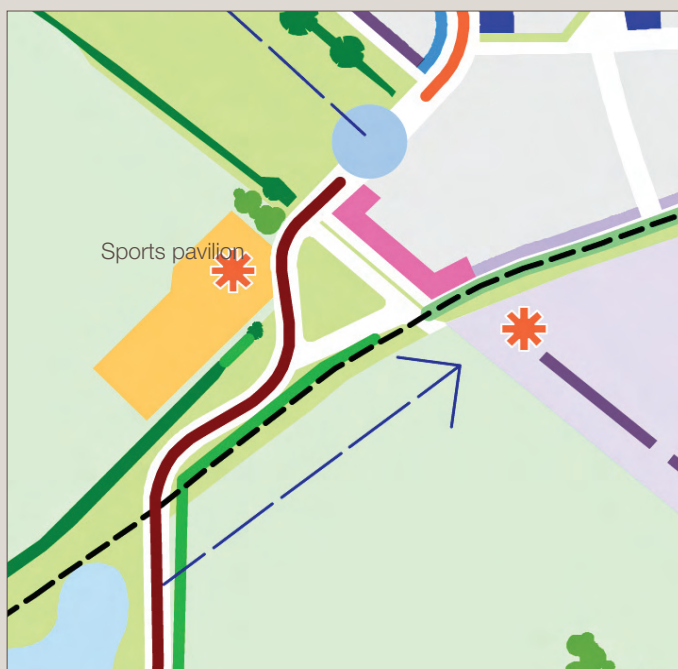
The building may be located anywhere on the site (shown orange in Figure 4.22) It may be up to two storeys (7m maximum) in height but should be designed to sit horizontally within the landscape. The building will principally face the adjoining sports pitches (providing a sheltered viewing point for spectators). It must also have a clearly expressed main entrance which faces the car park and vehicular site entrance.

### Security

As a stand alone building some distance from its neighbours the Sports Pavilion may be vulnerable to vandalism or other anti-social behaviour. It is therefore vital that the building design process addresses the range of possible attacks on the pavilion in an holistic way.

A creative design approach should be used to look at ways in which the architecture, landscape and management of the building can combine to protect it. This will include maximising visibility from the adjoining primary street, drawing footpaths past the building, and providing active security lighting and cctv. The managers of the building may also explore the benefits of sharing responsibility for the pavilion with a residents group from Kingsmere.

The use of solid roller shutters to windows and doors must be avoided.



**Figure 4.22** The Sports pavilion will be a key landmark building as it occupies a prominent location within the southern gateway for Kingsmere. (Detail of Parameters Plan, figure 3.22)

## Health Village

## Health Village



MANDATORY	<i>m</i>
Site size	<b>2.69ha, including 1.0 ha for a community hospital and 0.4ha for a GP surgery</b>
Vehicular Access	<b>From the Primary and Secondary street network only. Main site entrance at south-west corner.</b>
Pedestrian access	<b>On all public frontages.</b>
Cycling	<b>Covered and secure cycle parking provided on site.</b>
Boundary treatments	<b>1.1m tall railings only in front of street buildings. 2m tall railings between street buildings. Landscaped margin along public open space frontage. Petrol station boundary secured by 2.0m weldmesh.</b>
Planting	<b>As Pingle Brook character area.</b>
Architecture	<b>Tallest buildings located closest to Oxford Road frontage and in site core. Maximum building height of 14.5m.</b>
Materials	<b>Must harmonise in colour and tone with Pingle Brook character area.</b>

## Site

**The location, size and shape of the Health Village site is shown on the Land Use Proposals plan and is mandatory.**

**The site will be 2.69 hectares and will provide for a community hospital (1.0 hectares) and GP surgery (0.4 hectares).** The remainder may include other health care facilities and / or an extra care elderly nursing home (and / or general employment uses as an alternative use).

## Pedestrian access

**Pedestrian access to the site must be available on all public frontages** to encourage walking and provide street activity. Particular emphasis will be placed on entrances on the Oxford Road frontage and on the public right of way on the northern boundary as these will link best to the town centre. **Pedestrian signage for the Health Village will be provided from the junction of Middleton Stoney Road and Oxford Road, from the local centre to Kingsmere, and from the A41 gateway.**

## Vehicular access

**Vehicle routes to and from the site must originate at the A41 gateway and use the shortest possible route utilising primary and secondary streets only.**

Should a more direct route from the A41 be seen as desirable, and is supported by the Highway authority an additional planning application will be required. At the time this code was adopted, such a direct route was not supported by the Highway authority.

**The main site entrance must be on the secondary street on the southern boundary, as close as possible to the western boundary of the health village site.** This site entrance will be the principal entrance used by refuse vehicles, ambulances, visitor cars and heavy delivery vehicles. Other lesser entrances may be provided elsewhere on the secondary street or western boundary. (See Figure 4.24) **Vehicle signage for the Health Village will begin at the A41 gateway and will also be provided from the local centre.**

## Cycling

Cycling to and from the Health Village will be encouraged. **Covered and secure cycle parking will be provided on the site,** accessible from the A41 frontage, secondary street and public right of way / cycleway

## Vehicle parking

The number of car, van and lorry parking spaces provided for the Hotel and employment uses will be as agreed with the local authority. The number provided will relate to a balance between minimising traffic movements, avoiding on-street parking and promoting alternative modes of transport and will be based on the parking standards adopted at the time of the outline permission for Kingsmere.

In general terms, vehicle parking for the zone will be provided inside an outer ring of buildings defining the perimeter of the zone. This will provide some security for vehicles, allow siting of the perimeter buildings close to the external street and reduce the visual impact of the parking. Vehicle parking will be controlled by the site owner.

Parking areas will be largely screened from views from outside the zone by buildings (on street frontages) or planting (on the A41).

## Drainage

**The surface water drainage system must be independent of the main network and provide surface water alluviation and storage within the plot, suitable for 100 year plus climate change events.**



Non-residential uses: Health Village

- Application boundary
- Health Village
- Employment
- Existing trees / copses / hedgerows
- Existing or diverted watercourses
- Proposed structural planting
- Proposed attenuation ponds / swales
- Informal open space
- Formal open space
- Rights of way
- Primary Street (urban)
- Secondary Street
- Pedestrian – cycle only through street
- Secondary frontage
- Rural edge frontage
- Public open space / green corridors frontage
- Employment frontage
- Private boundary secured behind development
- \* Landmark buildings
- Marker buildings



**Figure 4.23** The Health Village occupies a prime location adjoining routes to the town centre. It has key frontages to Pingle Brook open space, Oxford Road and the internal Secondary Street. (Detail of Parameters Plan, figure 3.22)

- Health Village boundary
- Vehicle ingress
- Vehicle egress
- Main pedestrian / cycle entrances
- Weldmesh fence (2m high) and gates
- X Pingle Brook frontage (see text)
- Y Street frontage (see text)
- Z Oxford Road frontage (see text)



**Figure 4.24** Access to the Health Village for vehicles will be via a secondary street. Pedestrians and cyclists will have entrances from all directions.

## Non-residential uses: Health Village

### Frontages

The Health Village has three key frontages, to the south (due to main vehicle access), north (due to the open space and public right of way) east (due to Oxford Road), and a lesser street frontage to the west. All four frontages must be positively addressed by the public elevations of buildings.

The Oxford Road frontage (Z in figure 4.24) is modest in length but important because it overlooks a key and historic route to the town centre. Buildings here will be part of a new series of buildings beside this road, including the new Education Campus and employment area, all of which will be partly contained behind boundary planting.

A strong architectural consistency between these buildings is not considered necessary because of their different functions, nor is it especially practical. In common with the new buildings to the south, the Health Village buildings on this frontage will be confident and contemporary buildings indicating significant new uses in this part of Bicester. Whilst no vehicular access will be provided to this frontage a prominent pedestrian entrance must be accommodated.

The northern frontage (eastern side) overlooks Pingle Brook public open space (X in figure 4.24) and adjoins Pingle Brook residential character area. It is highly accessible by those on foot, either from Oxford Road or the town centre, as well as being prominent in views across the open space. Health Village buildings on this frontage should be designed with regard for neighbouring buildings in the Pingle Brook character area. This must be expressed in terms of the landscape setting and relative heights, and could include the use of similar materials.

The northern frontage (western side) overlooks the principal pedestrian route between Kingsmere and the town centre. This frontage (part of Y in Figure 4.24) should be treated as a street with a high degree of enclosure and consistency.

The southern frontage (aside from that part adjoining the petrol station) is the main vehicular access to the Health Village and will therefore be a prominent arrival point. This frontage (part of Y in Figure 4.24) forms part of a street and Health Village buildings must be designed to provide street enclosure and natural surveillance. Buildings here must employ scale and materials that can be compatible with domestic buildings elsewhere in the street. The western frontage forms one side of a street, the other half of which is residential. This will be a minor street and few Health Village vehicles will use it for access. Buildings on this frontage must be modestly scaled in order to support a residential character for the street.

### Architecture

The Health Village site is sufficiently large, and with such a range of potential building types, that more than one architectural approach is desirable.

**The key architectural requirement for the site is that the frontages are handled in the manner outlined above. Some frontages will require street architecture (Y in figure 4.24) whilst others, principally that overlooking the open space (X in Figure 4.24), may be more broken and independent.**

**Building heights on the site may extend to four storeys (14.5m). This must however be used exceptionally, perhaps on landmark corners or on the**

### Oxford Road frontage.

Buildings on the western and southern frontages should be lower in order to sit comfortably with their residential neighbours.

The Health Village will partly comprise buildings and uses for the general public which will therefore have civic significance. These buildings and areas must be of the highest quality, assessed in terms of functionality, innovation and robustness (in both the short and long term), as well as in the standard of materials and detailing used.

### Materials

**Wall materials for the Health Village must harmonise with those specified for the Pingle Brook character area**, which are red and buff brick or painted render, particularly overlooking the public open space and on the western edge of the site. Stone may also be used. Given the potential for contemporary design on the site, especially on the Oxford Road frontage, **materials such as steel, glass and timber will be acceptable.**

Roof materials may be slate or tile, or may be metal on flat or mono-pitch forms.

### Boundary treatments

**All public realm frontages, except that to Pingle Brook open space, must be principally defined by the public elevations of buildings**, including entrance doors and clear glazed windows. Buildings will be set close to the street pavement (maximum distance 1.5m) and may be set behind low (1.1m maximum) area railings. On the public open space frontage, buildings will be set back behind a landscaped margin.

**Gaps between frontage buildings will be secured by tall (2.0m maximum) railings and gates.**

**The boundary with the existing petrol station must be secured by 2.0m weldmesh fencing** preventing public access between the sites and access from Oxford Road along the boundary.

### Planting and public realm

**All planting should accord with the Pingle Brook character area** (which the Health Village abuts) and all hard materials must be chosen to complement those used in that character area.

Non-residential uses: Health Village



Public buildings and areas of civic significance in the Health Village must be of the highest quality, assessed in terms of functionality, innovation and robustness (in both the short and long term), as well as in the standard of materials and detailing used.



- Application boundary
  - 2 storeys\*
  - 2 to 2.5 storeys\*\*
  - Employment (max 14.5m)
  - Health Village (max 14.5m)
  - ⋯ Max 10m zone
- \* within this area 5% may be 1 storey (but only on third tier frontages) and 20% may be 2.5 storey. In addition 3 storey buildings may be permissible as landmark or marker buildings
- \*\* within this area 10% may be 3 storey (for landmark or marker buildings)

Extract from Building Heights plan showing vicinity of the Health Village – see Figure 3.21 (section 3)

## Employment Zone

## Employment Zone



MANDATORY	<i>m</i>
Site size	<b>3.0ha, comprising 1.0ha for a hotel site and 2.0ha for employment purposes.</b>
Vehicular Access	<b>From the Primary Street only. One entrance shared between hotel and employment.</b>
Pedestrian access	<b>From the Primary Street to the west and the Side Street to the north.</b>
Cycling	<b>Covered and secure cycle parking provided on site.</b>
Vehicle parking	<b>Number in accordance with 2008 standards. Located within the block not on primary street frontages. Management scheme to be submitted to LPA with any Reserved Matters submission.</b>
Boundary treatments	<b>Landscape planting screen to A41 boundary. 1.1m tall railings only in front of buildings on Primary Street. 2m tall railings between all street frontage buildings. 3.0m landscaped margin along Primary and Side Street.</b>
Planting	<b>As Urban Village character area.</b>
Architecture	<b>Hotel provides landmark element at A41 gateway. Tallest buildings located close to A41. Maximum building height of 14.5m.</b>
Materials	<b>Modern cladding systems with some brick, render and timber on residential streets.</b>

## Nature of uses

**The employment zone combines a 1.0 hectare site for an hotel and a 2.0 hectare site for general employment uses.**

## Site

**The location, size and shape of the Employment Zone site is shown on the Land Use Proposals plan and is mandatory.**

**The total site area will be 3.0 hectares.**

## Vehicular access

**Vehicle routes to and from the zone must originate at the A41 (eastern) gateway to Kingsmere and use the shortest possible route so as to minimise any impact on residential amenity.**

**Vehicular ingress and egress will be from the primary street on the western boundary of the employment zone, as close as possible to the A41.** One or two entrances may be provided. The entrances will be used by staff, visitors, deliveries and refuse.

**No other vehicular entrances will be provided to the Employment Zone.**

**Vehicle signage for the Hotel and employment uses will begin at the A41 gateway and will also be provided from the local centre.**

## Pedestrian access

**Pedestrian access to the site and direct access to buildings must be available from the Primary street to the west and Side street to the north** to encourage walking and provide street activity. **Pedestrian signage for the Hotel and employment uses will be provided from the junction of Middleton Stoney Road and Oxford Road, from the local centre to Kingsmere and from the A41 gateway.**

## Cycling

Cycling to and from the Hotel and employment uses will be encouraged. **Covered and secure cycle parking will be provided for the Hotel and employment buildings,** accessible from the primary street to the west and side street to the north.